



PLANNING COMMISSION

AGENDA

CITY OF NOVI

Regular Meeting

October 30, 2019 7:00 PM

Council Chambers | Novi Civic Center

45175 W. Ten Mile (248) 347-0475

COMMISSION MEMBERS: Anthony, Avdoulos, Ferrell, Gronachan, Lynch, Maday, Pehrson

The Planning Commission wishes to advise the general public that all remarks shall be limited to three minutes per person during both the Public Hearing and Audience Participation portions of the meeting. Petitioners' presentations shall be limited to ten minutes.

No person, other than a Commission member, shall address an issue for public hearing following the closing of that public hearing by the Chairperson (except during Audience Participation).

The above participation policy is outlined in Sections 3.4 and 3.8 of the Planning Commission By-Laws and Rules of Procedure.

CALL TO ORDER

ROLL CALL

PLEDGE OF ALLEGIANCE

APPROVAL OF AGENDA

AUDIENCE PARTICIPATION

CORRESPONDENCE

COMMITTEE REPORTS

CITY PLANNER REPORT

CONSENT AGENDA - REMOVALS AND APPROVALS

1. SLI MEDICAL **DETAILS**

Approval at the request SLI Medical for Planning Commission's approval of minor changes to the revised site plan and the landscape plan to accommodate truck maneuvering. The applicant previously received Planning Commission approval for site plan and storm water management plan.

PUBLIC HEARINGS

1. KEFORD COLLISION AND TOWING JSP 18-31 **DETAILS**

Public hearing at the request of Keford Collision & Towing for Planning Commission's approval of the Preliminary site plan, Wetland permit, and Storm water management plan. The subject property is currently zoned I-2 (general industrial) with an associated planned rezoning overlay (PRO) agreement. The subject property is approximately 7.61 acres and is located on the south side of Grand River Avenue between Taft Road and Novi Road (Section 15). The applicant proposes to use the existing larger building for an auto body collision repair shop and related offices, along with an accessory use of car rental services. No

particular subtenants for the second existing building have been identified yet. The plan proposes an enclosed yard of up to 160 spaces in the rear yard for storage of towed vehicles.

2. TEXT AMENDMENT 18.290– UPDATES TO STANDARDS FOR HOTEL DEVELOPMENTS [DETAILS](#)

Public Hearing for Planning Commission's recommendation to the City Council for an ordinance to amend the City of Novi Zoning Ordinance at various sections, in order to bring ordinance language up to date, and update standards for minimum parking, open space and loading requirements for Hotel developments.

3. TEXT AMENDMENT 18.288 – UPDATES TO THE B-2 AND B-3 ZONING DISTRICTS [DETAILS](#)

Public Hearing for Planning Commission's recommendation to the City Council for an ordinance to amend the City of Novi Zoning Ordinance at various sections, in order to update the uses permitted as of right and the uses permitted as special land uses in the B-2, Community Business District and B-3, General Business District, and various other modifications.

MATTERS FOR CONSIDERATION

1. **APPROVAL OF THE SEPTEMBER 25, 2019 PLANNING COMMISSION MINUTES** [DETAILS](#)
2. **APPROVAL OF THE OCTOBER 16, 2019 PLANNING COMMISSION MINUTES** [DETAILS](#)

CONSENT AGENDA REMOVALS FOR COMMISSION ACTION

SUPPLEMENTAL ISSUES

AUDIENCE PARTICIPATION

ADJOURNMENT

SCHEDULED AND ANTICIPATED MEETINGS

TUES	11/12/19	CITY COUNCIL MEETING	7:00 PM
WED	11/13/19	PLANNING COMMISSION	7:00 PM
TUES	11/19/19	ZONING BOARD OF APPEALS	7:00 PM
MON	11/25/19	CITY COUNCIL MEETING	7:00 PM
MON	12/09/19	CITY COUNCIL MEETING	7:00 PM
TUES	12/10/19	ZONING BOARD OF APPEALS	7:00 PM
WED	12/11/19	PLANNING COMMISSION	7:00 PM
MON	12/16/19	CITY COUNCIL MEETING	7:00 PM

NOTICE: Full plan sets available to view at the Community Development Department.

People with disabilities needing accommodations for effective participation in this meeting should contact the City Clerk at 248.347.0456 at least seven business days in advance of the meeting. An attempt will be made to make reasonable accommodations.

MEMORANDUM



TO: MEMBERS OF THE PLANNING COMMISSION
THRU: BARBARA MCBETH, *A/CP*, CITY PLANNER
FROM: SRI RAVALI KOMARAGIRI, PLANNER
SUBJECT: JSP 18-74 SLI MEDICAL OFFICE BUILDING
DATE: OCTOBER 25, 2019

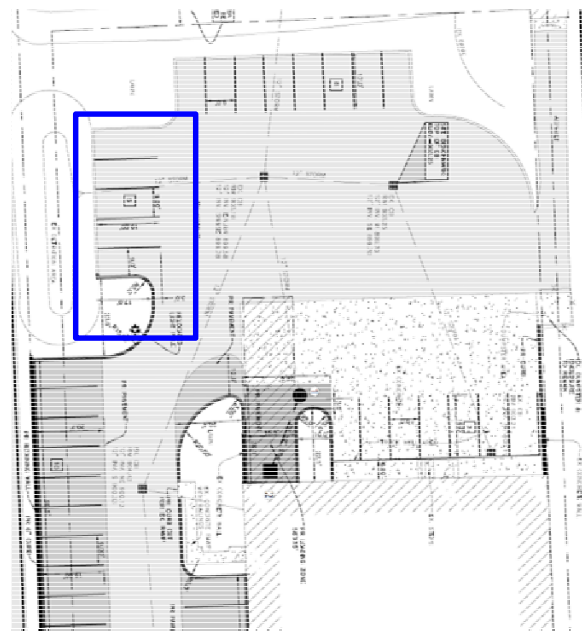
The purpose of this memo is to provide some background information on the applicant's request for approval of a revised site layout. The Planning Commission approved the Preliminary Site Plan and the storm water management plan at their June 12, 2019 meeting. Action summary is listed at the end of this letter.

The approval motion required the applicant to submit a truck circulation diagram as part of the final site plan. In order to address this comment, the applicant noted that the removal of proposed landscape islands is warranted to allow reasonable space for truck maneuvering. The current revised plan no longer proposes end islands north of the building enclosed on the red box in the image below. The parking spaces are relocated to west as indicated in the blue box. The plan as currently proposed meets the Ordinance requirements and previously approved waivers, but it is slightly different from the site layout from the Planning Commission approval.

PREVIOUS APPROVED LAYOUT



CURRENT REVISED LAYOUT



The revisions resulted in loss of green space in two locations:

- a. The loss of two end islands resulted in reduction of green space by approximately 600 square feet of landscaped area and the removal of four proposed trees.
- b. The curb south of the loading area is now pulled back to enable safe truck turning movements, but it resulted in the loss of green space.

However, the applicant has closely worked with our landscape architect and addressed the entire comments listed in the review letters. All reviews are recommending approval at this time to move forward to electronic stamping sets.

The Planning Commission is asked to approve the revised site plan, supported by staff to allow the applicant to move forward with the site plan approval process. The following motion is suggested for approval

Approval – Revised Preliminary Site Plan

In the matter of request of SLI Medical for JSP 18-74 SLI Medical Office Building, motion to approve the Revised Preliminary Site Plan based on and subject to the following:

1. The findings of compliance with Ordinance standards in the staff and consultant review letters and the remaining items listed in those letters being addressed on the electronic stamping set submittal; and

(This motion is made because the plan is otherwise in compliance with Article 3, Article 4, and Article 5 of the Zoning Ordinance and all other applicable provisions of the Ordinance.)

JUNE 12, 2019 PLANNING COMMISSION MEETING ACTION SUMMARY

The Planning Commission approved the Preliminary Site Plan and the storm water management plan at their June 12, 2019 meeting based on the following motion:

In the matter of request of SLI Medical for JSP 18-74 SLI Medical Office Building, motion to approve the Preliminary Site Plan based on and subject to the following:

1. The applicant shall revise the layout to provide a minimum of 75 parking spaces including the minimum required barrier free spaces at the time of final site plan;
2. **The applicant shall provide a truck circulation diagram as noted in the Traffic review letter at the time of final site plan;**
3. Traffic waiver to allow Parking study in lieu of performing a Traffic Impact study because the change in number of trips from existing use is not significant, which is hereby granted;
4. The applicant shall provide a revised landscape plan, subject to approval of the City's Landscape Architect, that complies with the following landscape waivers, at the time of final site plan approval:
 - c. Landscape waiver from Sec. 5.5.3.B.ii for lack of street trees along Grand River Avenue frontage due to conflicts with existing utilities in right-of-way, which is hereby granted;
 - d. Landscape waiver from Sec. 5.5.3.C.iv for not meeting the minimum required parking lot perimeter trees due to a lack of room between the parking and the west property line, provided that the applicant work with the City's landscape architect to maximize number of trees along other areas of the parking lot, which is hereby granted;
 - e. Landscape waiver from Sec. 5.5.3.B.ii for reduction in required greenbelt width along Grand River Avenue due to existing conditions which were previously approved, which is hereby granted;
 - f. Landscape waiver from Sec. 5.5.3.B.ii for lack of berm along Grand River Avenue, due to existing grading which is not changing, which is hereby granted;
 - g. Landscape waiver from Sec 5.5.3.D. for proposing less than 75% of the building foundation with landscaping, and locating some foundation area away from the building along the existing berm facing Grand River Avenue, which is hereby granted;
5. Zoning Board of Appeals variance from Section 5.2.12. for reduction of minimum required parking for the proposed use (122 spaces required, 75 spaces proposed);
6. The findings of compliance with Ordinance standards in the staff and consultant review letters and the remaining items listed in those letters being addressed on the Final Site Plan.

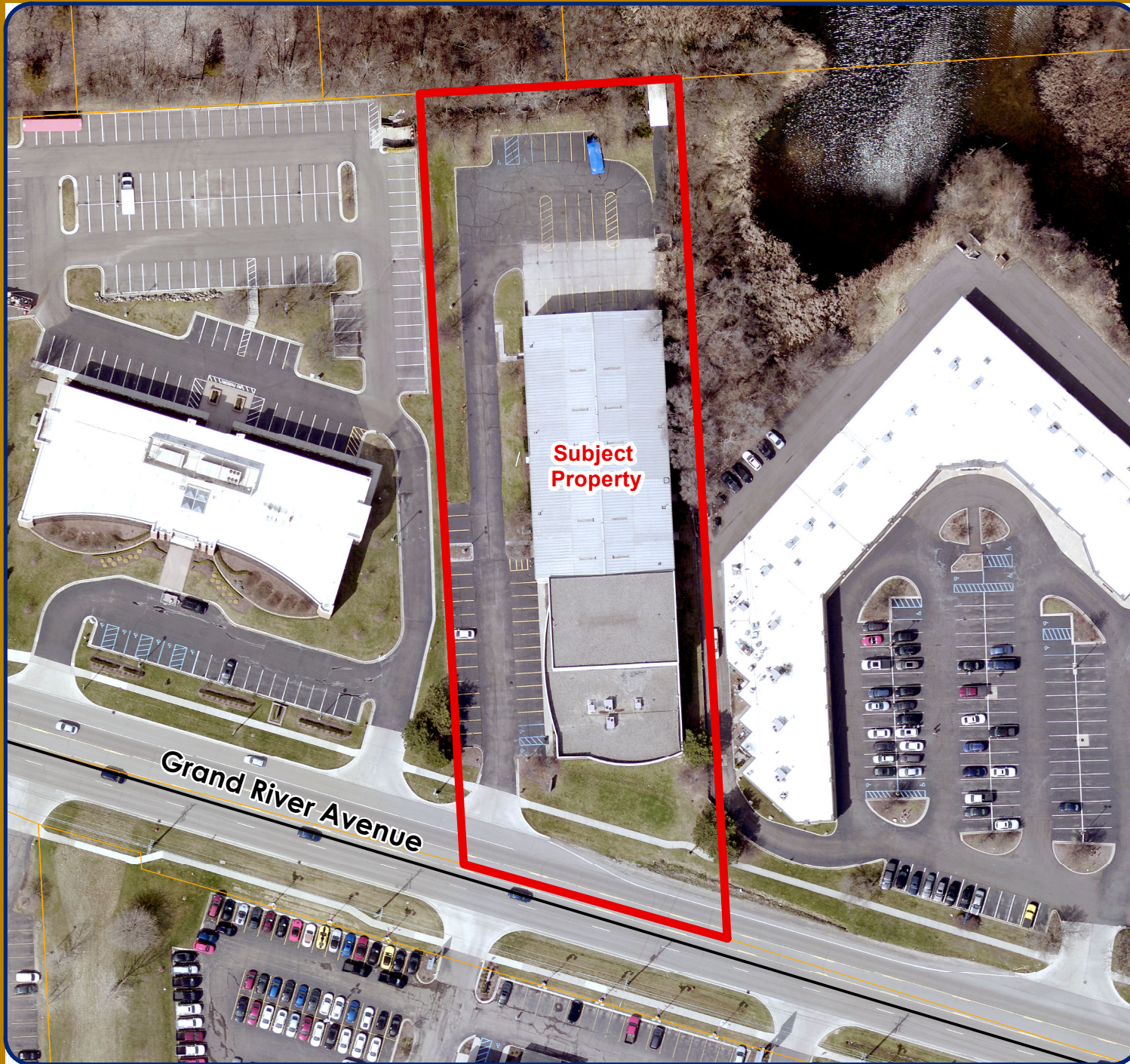
This motion is made because the plan is otherwise in compliance with Article 3, Article 4, and Article 5 of the Zoning Ordinance and all other applicable provisions of the Ordinance.

1. In the matter of request of SLI Medical for JSP 18-74 SLI Medical Office Building, motion to approve the Stormwater Management Plan based on and subject to the findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan. This motion is made because it otherwise in compliance with Chapter 11 of the Code of Ordinances and all other applicable provisions of the Ordinance.

MAPS
Location

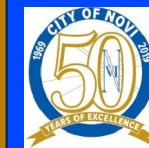
JSP 18-74 SLI MEDICAL OFFICE BUILDING

Location



LEGEND

 Sections



City of Novi

Dept. of Community Development
City Hall / Civic Center
45175 W Ten Mile Rd
Novi, MI 48375
cityofnovi.org

Map Author: Sri Komaragiri
Date: 06/07/19
JSP 18-74 SLI Medical Office Building
Version #: 1



1 inch = 98 feet



MAP INTERPRETATION NOTICE

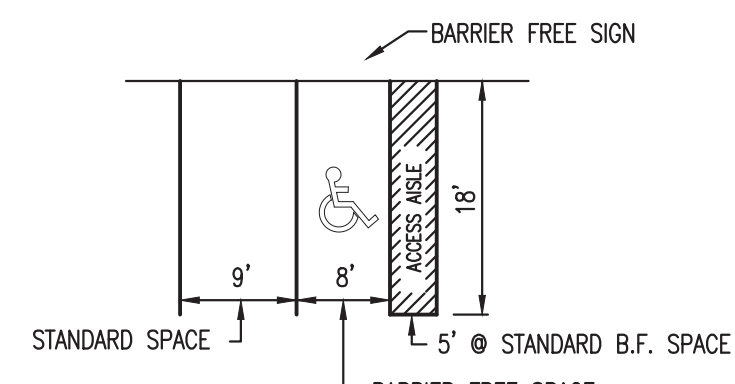
Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Please contact the City GIS Manager to confirm source and accuracy information related to this map.

PREVIOUSLY APPROVED SITE LAYOUT

REVISED SITE LAYOUT

NOTES:

- SIGNING SHALL BE PLACED 2 FEET FROM CURB OR SIDEWALK EDGE.
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- ALL TRAFFIC SIGNAGE WILL COMPLY WITH THE CURRENT MMUTCD STANDARDS.
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- TRAFFIC CONTROL SIGNS SHALL HAVE HIGH INTENSITY PRISMATIC (HIP) SHEETING TO MEET FHWA RETROREFLECTIVITY REQUIREMENTS.
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- ALL PAVEMENT MARKINGS SHALL BE 4" THICK AND YELLOW STRIPES TO MATCH EXISTING MARKINGS.
- THE LARGEST VEHICLE THAT WILL ACCESS THE SITE IS A WB-40 TRUCK.
- ALL WORK SHALL CONFORM TO THE CITY OF NOVI STANDARDS AND SPECIFICATIONS.
- SEE CITY OF NOVI DETAIL SHEETS FOR ALL PAVEMENT DETAILS.
- ALL BUSINESS, SERVICING OR PROCESSING, EXCEPT FOR OFF-STREET PARKING, LOADING/UNLOADING SHALL BE CONDUCTED WITHIN COMPLETELY ENCLOSED BUILDINGS.
- NO DEWATERING IS ANTICIPATED. IF THE NEED FOR DEWATERING IS ENCOUNTERED DURING CONSTRUCTION, A DEWATERING PLAN MUST BE SUBMITTED TO THE ENGINEERING DIVISION FOR REVIEW.
- NOT NEW LIGHTING IS PROPOSED FOR THIS SITE.



TYP. PARKING SPACE LAYOUT
NO SCALE

BICYCLE PARKING: ONE SPACE PER 20 EMPLOYEES ON THE MAXIMUM SHIF, MINIMUM 2 SPACES.
46 TOTAL EMPLOYEES
46/20=2.3 SPACES NEEDED, 2 SPACES REQUIRED
USE INVERTED "U" DESIGN FOR BIKE RACK
LOADING ZONE= 10 SF PER FRONT OF BUILDING
LOADING ZONE=84'X10'=840 SF REQUIRED
PROPOSED LOADING ZONE= 56'X15'= 840 SF
BARRIER FREE PARKING REQUIREMENTS
PHYSICAL THERAPY AREA=1,150 SF
1,150 SF/167= 6.88=7 PT SPACES
20% X 7=1.4 SPACES=2 ADA SPACES REQUIRED
75 TOTAL SPACES - 7 PT SPACES=68 SPACES
3 ADA SPACES REQUIRED
TOTAL ADA SPACES REQUIRED=5 SPACES

PARCEL DESCRIPTION

A PART OF THE NE 1/4 OF SECTION 23, T1N, R8E, CITY OF NOVI, OAKLAND COUNTY, MICHIGAN, MORE PARTICULARLY DESCRIBED AS: COMMENCING AT THE NORTH 1/4 CORNER OF SAID SECTION 23; THENCE N88°24'50"E 264.67 FEET ALONG THE NORTH LINE OF SAID SECTION 23 AN DTHE CENTERLINE OF ELEVEN MILE ROAD; THENCE S01°27'28"E 527.03 FEET TO THE POINT OF BEGINNING; THENCE N88°48'53"E 67.98 FEET; THENCE S00°45'39"E 585.77 FEET TO THE CENTERLINE OF GRANDE RIVER AVENUE; THENCE N71°30'41"W 185.36 FEET ALONG THE CENTERLINE OF GRANDE RIVER AVENUE; THENCE N00°45'39"W 523.36 FEET; THENCE N88°48'53"E 107.02 FEET TO THE POINT OF BEGINNING, CONTAINING 2.23 ACRES OF LAND, MORE OF LESS.

NOTE: THERE ARE NO WETLANDS LOCATED ON SITE PER THE NATIONAL WETLANDS INVENTORY MAPS

NOTE: NO FLOODPLAIN IS LOCATED ON SITE.

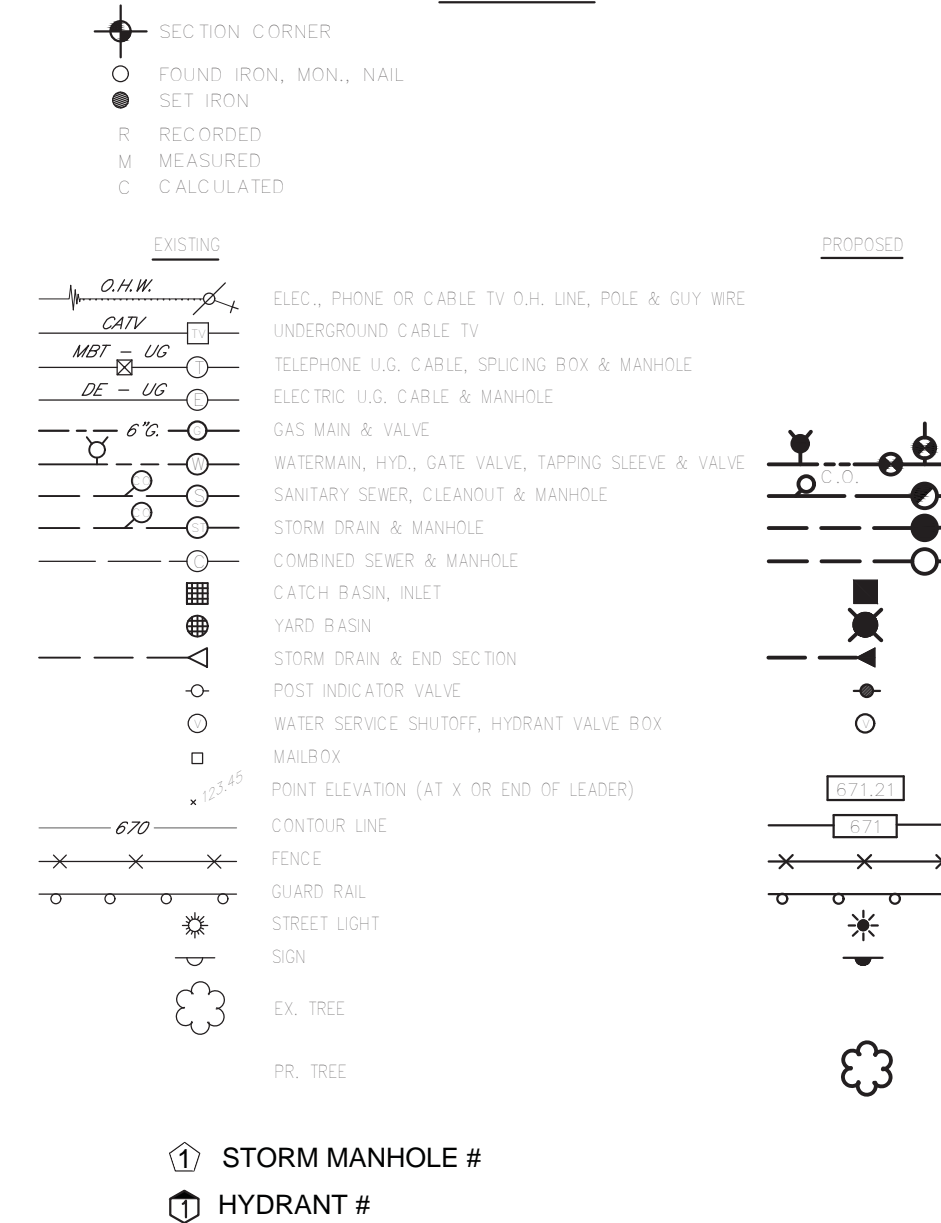
NOTE: NO REGULATED WOODLANDS ON SITE.

PARKING REQUIREMENTS:

1 SPACE PER 167 S.F. FLOOR AREA CLINIC
12,810 S.F. / 167 S.F. = 77 SPACES
1 SPACE PER 222 S.F. FLOOR AREA OFFICE
9,390 S.F. / 222 S.F. = 42 SPACES
1 SPACE PER EMPLOYEE = 3 SPACES
REQUIRED PARKING: 122 SPACES INCLUDING 5 ADA SPACES
EXISTING PARKING: 37 SPACES INCLUDING 4 ADA SPACES
PROPOSED PARKING: 38 SPACES INCLUDING 3 ADA SPACES
TOTAL ON-SITE PARKING: 75 SPACES INCLUDING 5 ADA SPACES

NOTE: A MINIMUM 64 PARKING SPACES ARE REQUIRED BASED ON A PARKING LOT STUDY. SEE PARKING NEEDS ASSESSMENT BY RICH & ASSOCIATES PARKING CONSULTANTS DATED 12/12/18

LEGEND



SITE BENCHMARKS (NAVD88):

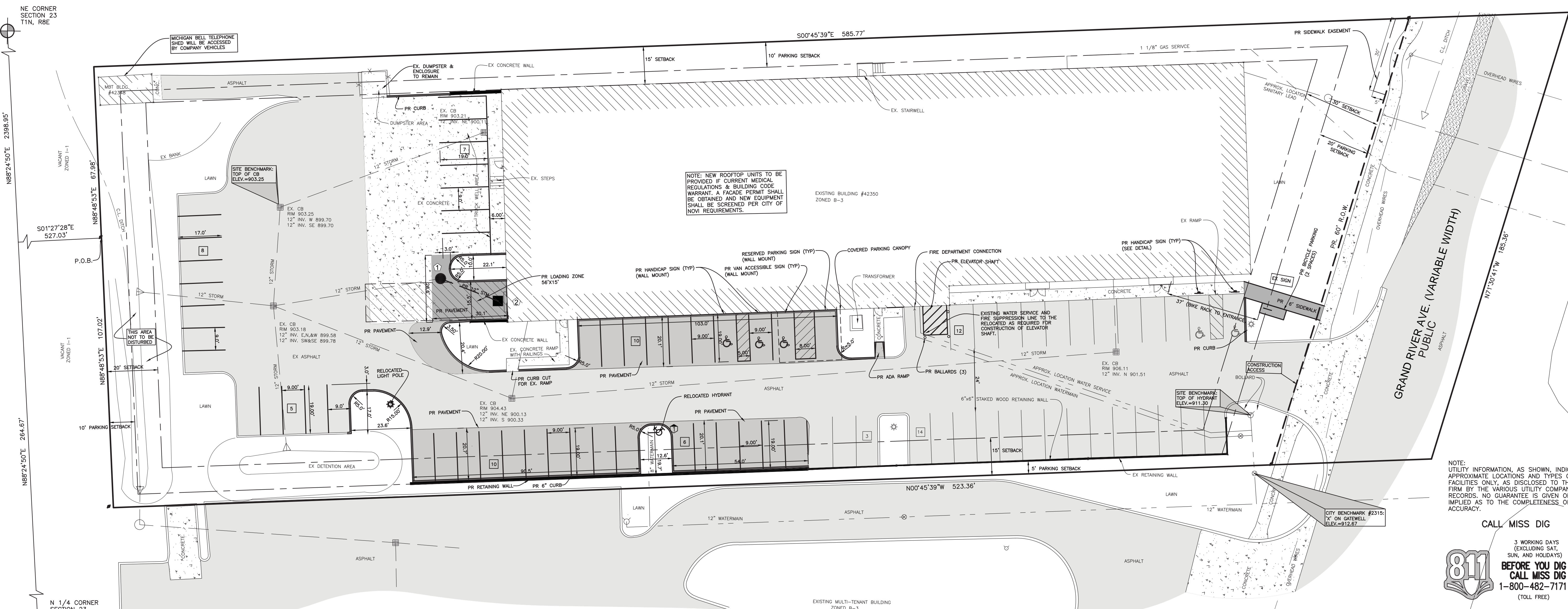
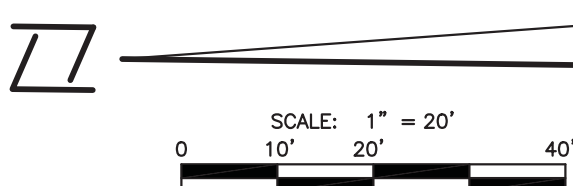
- TOP OF HYDRANT ON WEST SIDE OF SITE ENTRANCE
ELEV.: 911.30
 - TOP OF CATCHBASIN IN REAR OF BUILDING
ELEV.: 903.25
- CITY BENCHMARK #2315
"X" ON NORTH SIDE OF RIM OF GATEWELL LOCATED 5' NORTH OF SIDEWALK ON NORTH SIDE OF GRAND RIVER BETWEEN #42350 & #42400 GRAND RIVER.
ELEV.: 912.67

SITE DATA:

ADDRESS: 42350 GRAND RIVER AVENUE
PID: 22-23-226-001
ZONED: B-3, GENERAL BUSINESS
AREA: 2.23 ACRES

NOTE: TITLE WORK HAS NOT BEEN PROVIDED. FENN & ASSOCIATES, INC. IS RELIEVED OF ANY AND ALL LIABILITY RELATED TO ANY EASEMENTS OR ENCUMBRANCES AFFECTING THE PROPERTY.

NOTE: NO WETLANDS ARE SHOWN ON THE PROPERTY PER THE NATIONAL WETLAND INVENTORY MAPS AND THE CITY WETLAND MAPS.



NOTE: NEW ROOFTOP UNITS TO BE PROVIDED IF CURRENT MEDICAL REGULATIONS & BUILDING CODE WARRANT. A FACADE PERMIT SHALL BE OBTAINED AND NEW EQUIPMENT SHALL BE SCREENED PER CITY OF NOVI REQUIREMENTS.

NOTE: UTILITY INFORMATION, AS SHOWN, INDICATES APPROXIMATE LOCATIONS AND TYPES OF FACILITIES ONLY. AS DISCLOSED TO THIS FIRM BY THE VARIOUS UTILITY COMPANY'S RECORDS. NO GUARANTEE IS GIVEN OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY.

CALL MISS DIG
3 WORKING DAYS (EXCLUDING SAT, SUN, AND HOLIDAYS)
BEFORE YOU DIG CALL MISS DIG
1-800-482-7171
(TOLL FREE)

Fenn & Associates, Inc.
Land Surveying and Civil Engineering
14933 Commercial Drive, Shelby Township, MI 48315
Phone: 586-254-9577 Fax: 586-254-9020
www.fennsurveying.com

ENGINEERING SITE PLAN
SLI MEDICAL
PART OF THE NE 1/4 OF SECTION 23, T1N, R8E,
CITY OF NOVI, OAKLAND COUNTY, MICHIGAN

DISTRIBUTION	
●	PRELIMINARY
●	APPROVAL
○	CONSTRUCTION
○	AS-BUILT
REVISIONS	
DATE	REMARKS
2/27/19	PER CLIENT
5/8/19	PER CITY
6/7/19	PER CITY
6/24/19	ENG. DESIGN
10/14/19	PER CITY

SEAL

CLIENT
ARIS BERRIS
1072 EQUESTRIAN DRIVE
SOUTH LYON, MI 480178
248-308-2846

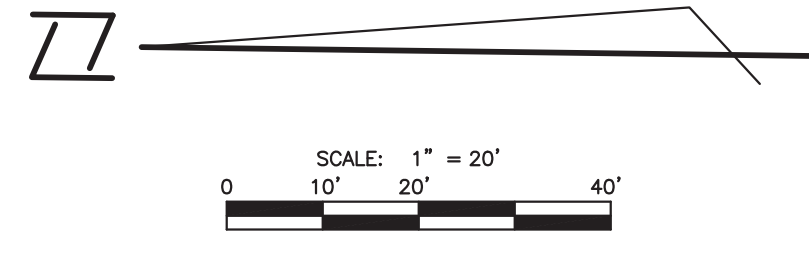
PROJECT NAME
SLI MEDICAL

PROJECT NUMBER
C18-033.06X

SHEET NAME
GENERAL PLAN

DRAWING SCALE: 1" = 20'
DATE: 07/08/19
PROJECT MANAGER: JSR, P.E.
DESIGNED BY: J.H.
DRAWN BY: J.H.
FIELD BY: BH, VA
CHECKED BY: TWDJSR, PE

SHEET NUMBER
2 OF 6

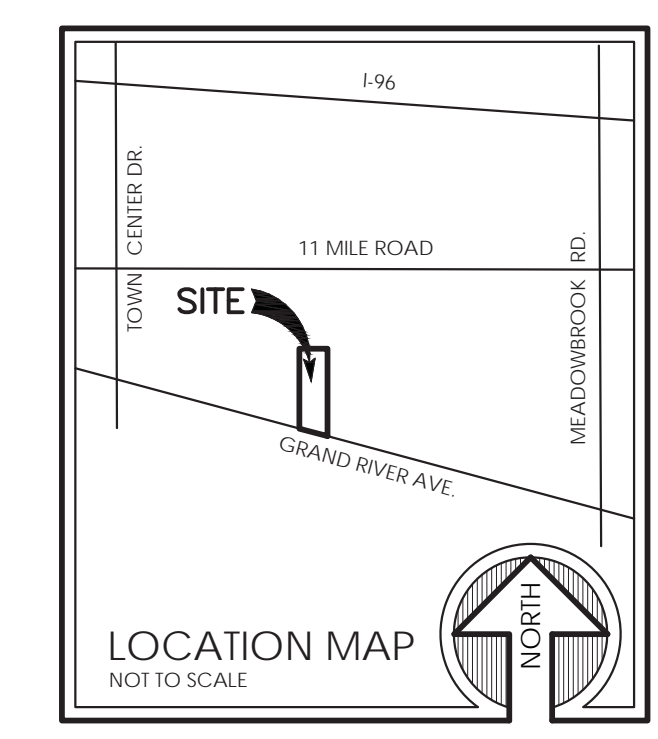


SITE DATA:
 ADDRESS: 42350 GRAND RIVER AVENUE
 ZONED: B-3, GENERAL BUSINESS
 AREA: 2.23 ACRES

SITE BENCHMARKS (NAVD88):
 1. TOP OF HYDRANT ON WEST SIDE OF SITE ENTRANCE ELEV.: 911.30
 2. TOP OF CATCHBASIN IN REAR OF BUILDING ELEV.: 903.25
CITY BENCHMARK #2315
 'X' ON NORTH SIDE OF RIM OF GATEWELL LOCATED 5' NORTH OF SIDEWALK ON NORTH SIDE OF GRAND RIVER BETWEEN #42350 & #42400 GRAND RIVER. ELEV.: 912.67

NOTE:
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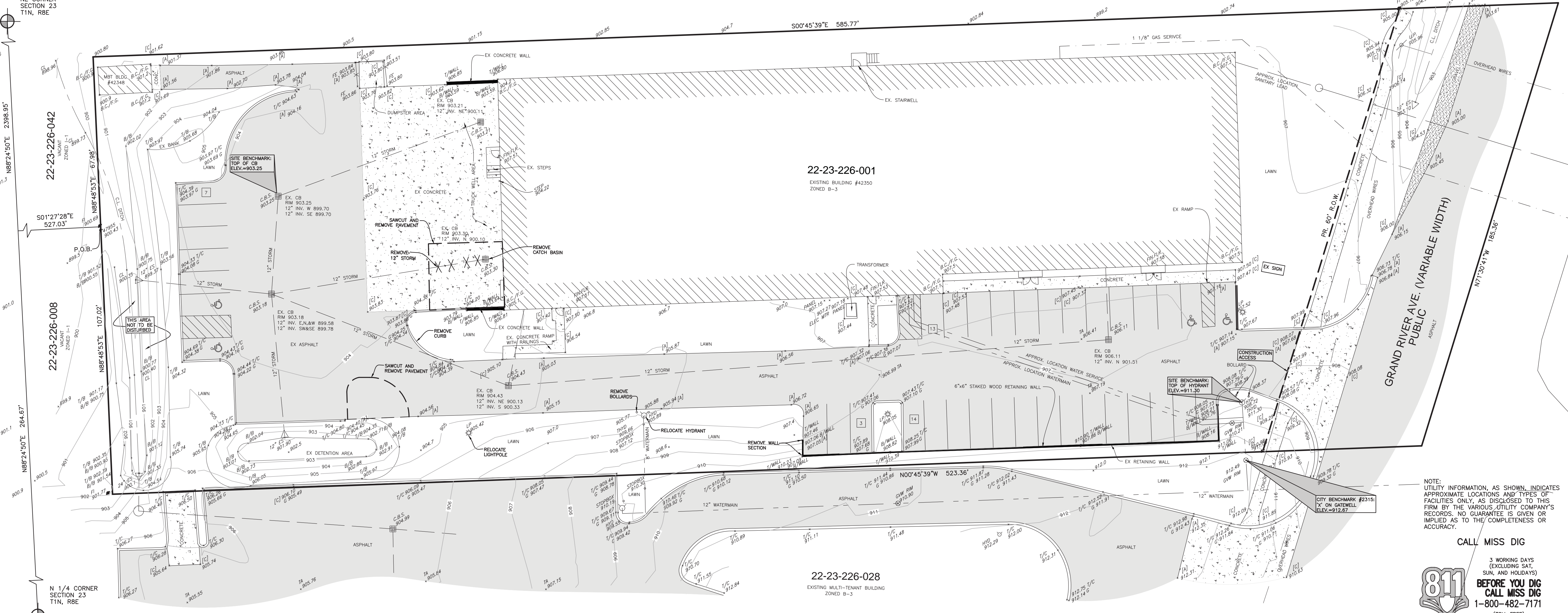
LEGEND

○	SECTION CORNER	R	RECORDED
●	FOUND IRON, MON., NAIL	M	MEASURED
○	SET IRON	C	CALCULATED

EXISTING

—	O.H.W.	ELEC., PHONE OR CABLE TV O.H. LINE, POLE & GUY WIRE
—	CNTY	UNDERGROUND CABLE TV
—	MBT	TELEPHONE U.S. CABLE, SPLICING BOX & MANHOLE
—	UC	ELECTRIC U.S. CABLE & MANHOLE
—	6" G	GAS MAIN & VALVE
—	12" S	WATERMAIN, HTR., GATE VALVE, TAPPING SLEEVE & VALVE
—	12" S	SEWER, CLEANOUT & MANHOLE
—	12" S	STORM DRAIN & MANHOLE
—	12" S	COVERED SEWER & MANHOLE
—	12" S	CATCH BASIN, INLET
—	12" S	YARD BASIN
—	12" S	STORM DRAIN & END SECTION
—	12" S	POST INDICATOR VALVE
—	12" S	WATER SERVICE SHUTOFF, HYDRANT VALVE BOX
—	12" S	MAILBOX
—	12" S	POINT ELEVATION (AT X OR END OF LEADER)
—	6" G	CONTOUR LINE
—	—	FENCE
—	—	GUARD RAIL
—	—	STREET LIGHT
—	—	SOIL
—	—	TREE

NE CORNER SECTION 23 T1N, R8E
 NB82°24'50"E 2398.95'
 S01°27'28"E 527.03'
 NB84°48'53"E 67.98'
 NB84°48'53"E 107.02'
 NB82°24'50"E 264.67'
 N 1/4 CORNER SECTION 23 T1N, R8E



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REVISIONS

DATE	REMARKS
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 248-308-2846

PROJECT NAME
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PROJECT NUMBER
 C18-033.06X

SHEET NAME
 TOPOGRAPHIC
 MAPPING & REMOVAL PLAN

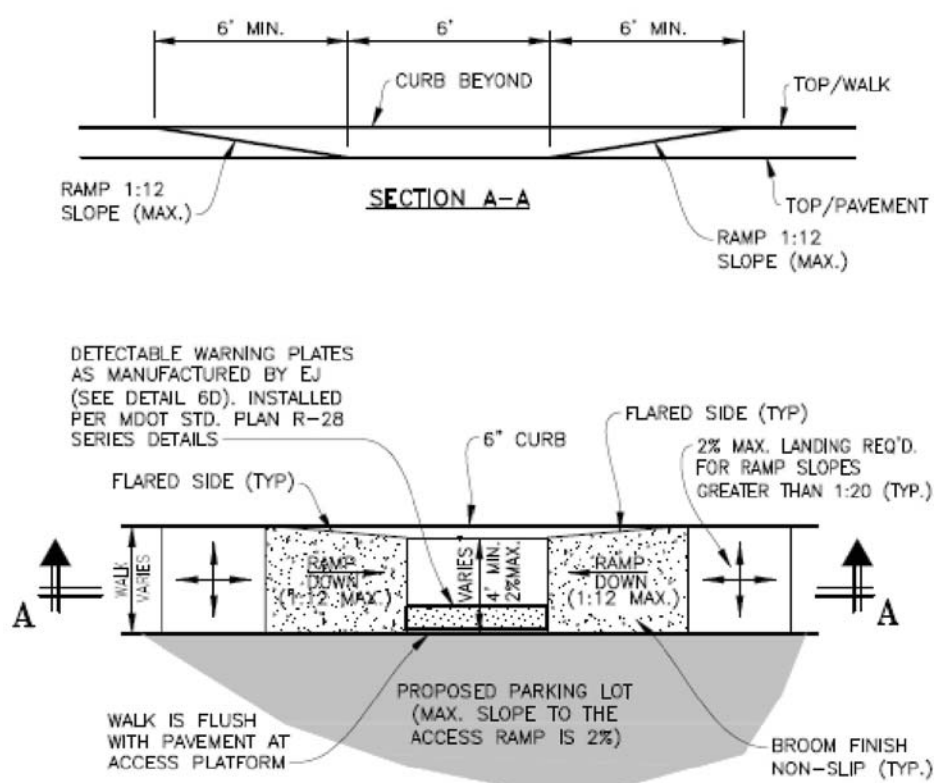
DRAWING SCALE: 1" = 20'
DATE: 09/25/18
PROJECT MANAGER: JSR, P.E.
DESIGNED BY: N/A
DRAWN BY: JJS, P.E.
FIELD BY: BH, VA
CHECKED BY: TWDJSR, PE

SHEET NUMBER
 3 OF 6

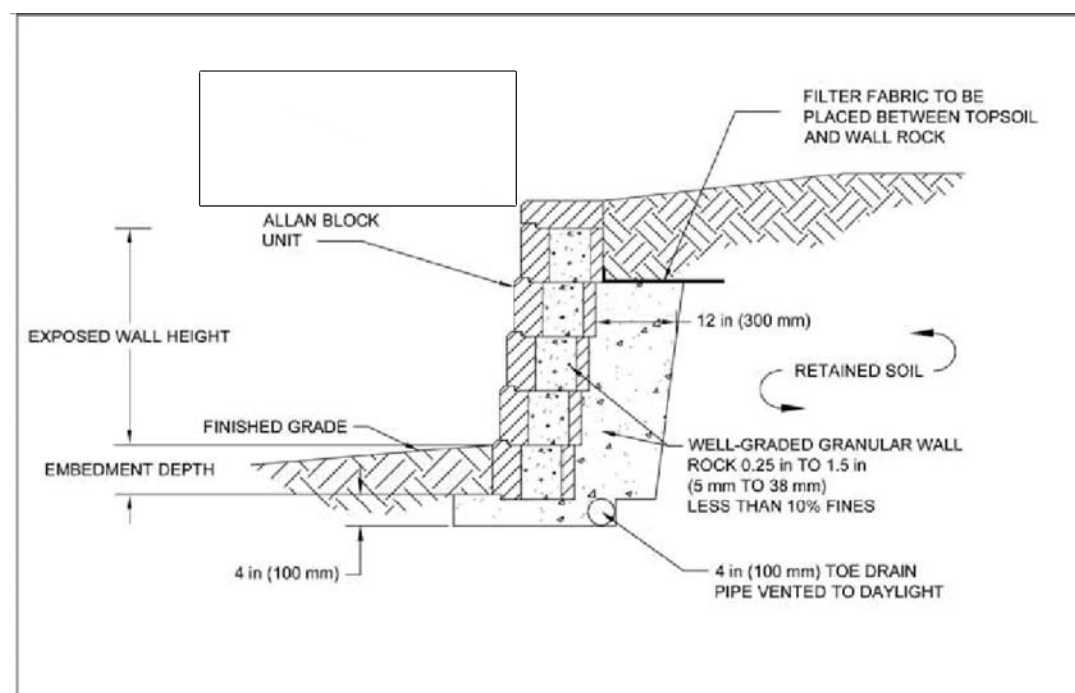
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CALL MISS DIG

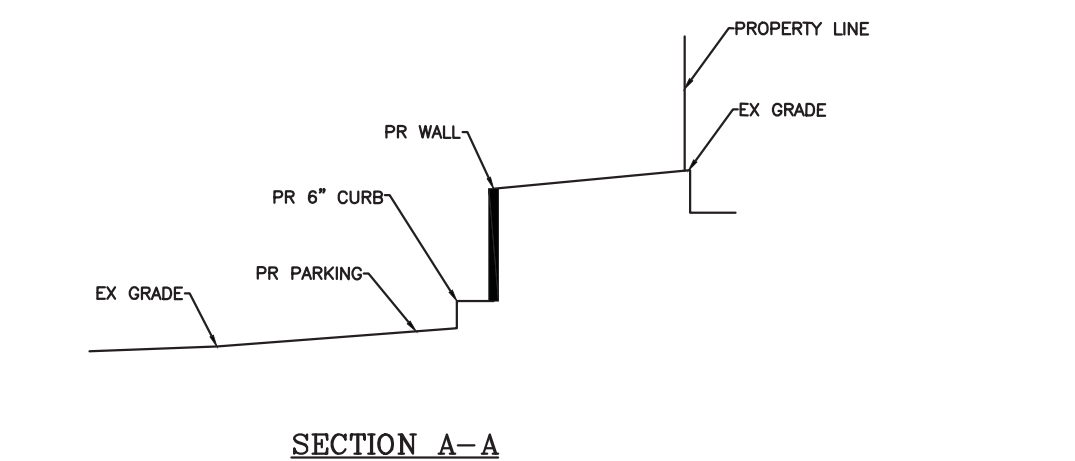
3 WORKING DAYS (EXCLUDING SAT, SUN, AND HOLIDAYS)
BEFORE YOU DIG CALL MISS DIG
 1-800-482-7171 (TOLL FREE)



TYPICAL PARKING LOT SIDEWALK RAMP DETAIL
DETAIL - 6B
NOT TO SCALE



TYPICAL GRAVITY WALL SECTION
USE THIS DETAIL OR APPROVED EQUAL



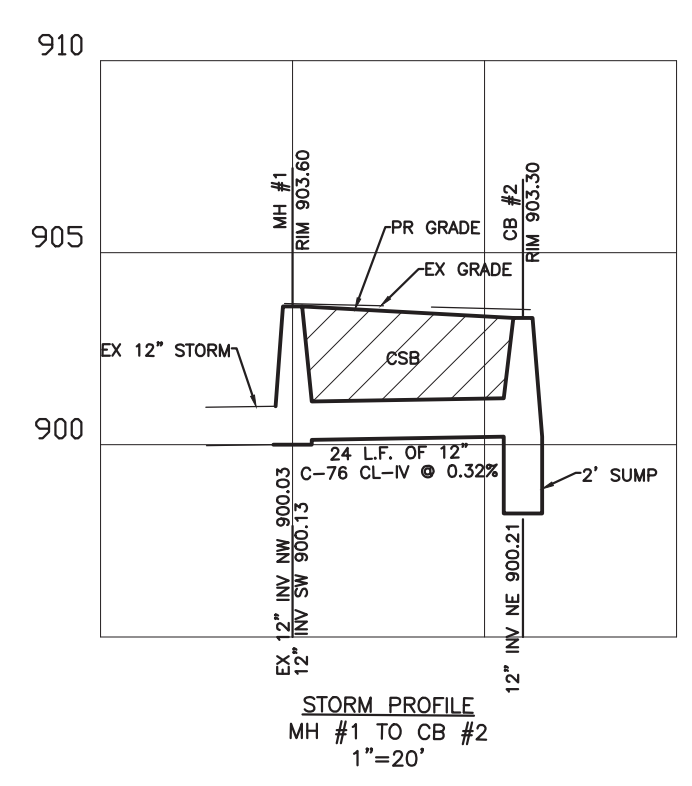
SECTION A-A

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- SEE CITY OF NOVI DETAIL SHEETS FOR ALL PAVEMENT & UTILITY DETAILS.
- ALL BUSINESS, SERVICING OR PROCESSING, EXCEPT FOR OFF-STREET PARKING, LOADING/UNLOADING SHALL BE CONDUCTED WITHIN COMPLETELY ENCLOSED BUILDINGS.
- NO DEWATERING IS ANTICIPATED. IF THE NEED FOR DEWATERING IS ENCOUNTERED DURING CONSTRUCTION, A DEWATERING PLAN MUST BE SUBMITTED TO THE ENGINEERING DIVISION FOR REVIEW.
- RECONNECT ANY ROOF OR TILE DRAINS TO STORM SYSTEM WHEN REMOVING CATCH BASIN.
- ALL FIRE HYDRANTS MUST BE INSTALLED AND OPERATIONAL BEFORE ANY BUILDING CONSTRUCTION BEGINS.
- NO NEW SANITARY LEAD OR WATER SERVICE IS PROPOSED.
- CITY OF NOVI RIGHT OF WAY PERMIT IS REQUIRED FOR WORK WITHIN ANY PUBLIC ROAD RIGHT-OF-WAY OF CITY EASEMENT.
- NO NEW LIGHTING IS PROPOSED FOR THIS SITE.

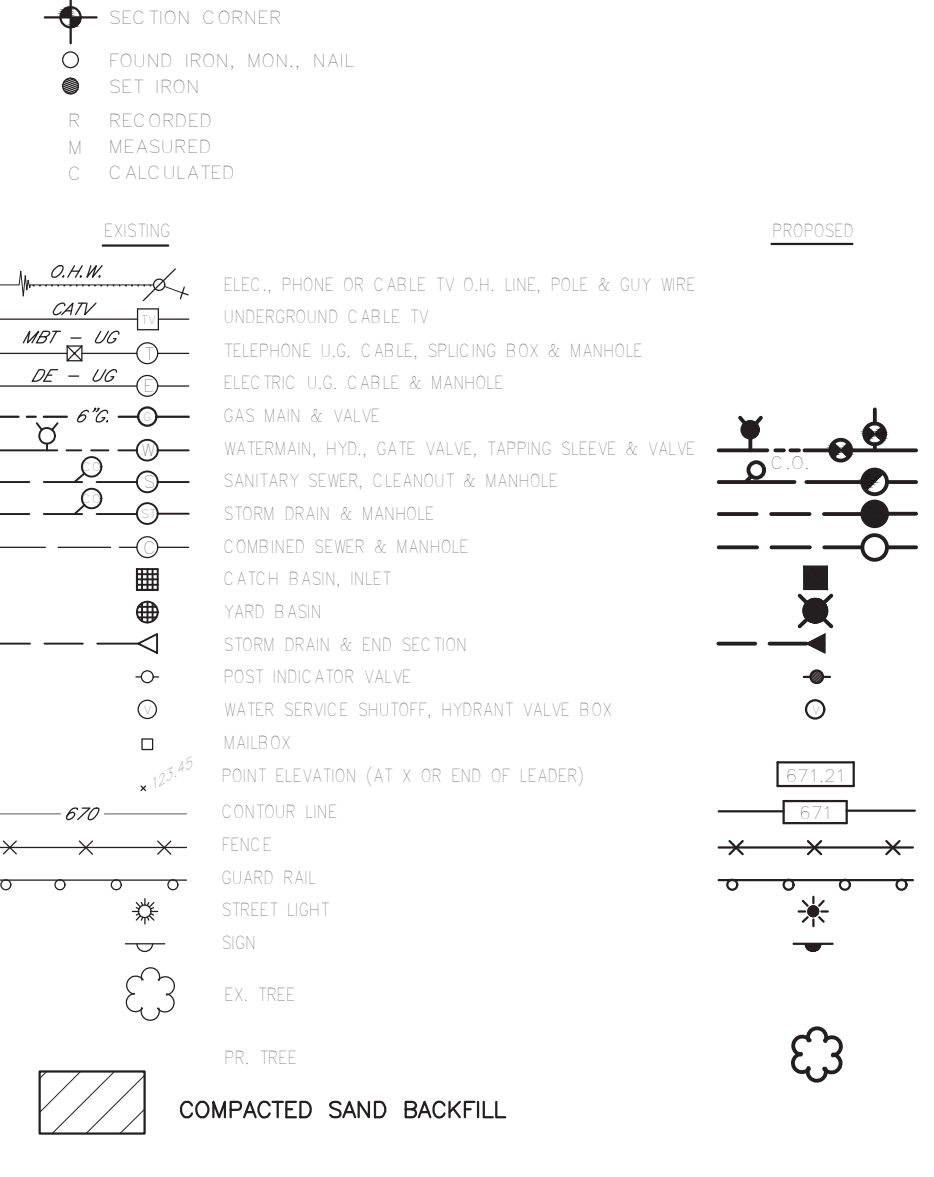
PARCEL DESCRIPTION

A PART OF THE NE 1/4 OF SECTION 23, T1N, R8E, CITY OF NOVI, OAKLAND COUNTY, MICHIGAN, MORE PARTICULARLY DESCRIBED AS: COMMENCING AT THE NORTH 1/4 CORNER OF SAID SECTION 23; THENCE N88°24'50"E 264.67 FEET ALONG THE NORTH LINE OF SAID SECTION 23 AN DTHE CENTERLINE OF ELEVEN MILE ROAD; THENCE S01°27'28"E 527.03 FEET TO THE POINT OF BEGINNING; THENCE N88°48'53"E 67.98 FEET; THENCE S00°45'39"E 585.77 FEET TO THE CENTERLINE OF GRANDE RIVER AVENUE; THENCE N71°30'41"W 185.36 FEET ALONG THE CENTERLINE OF GRAND RIVER AVENUE; THENCE N00°45'39"W 523.36 FEET; THENCE N88°48'53"E 107.02 FEET TO THE POINT OF BEGINNING. CONTAINING 2.23 ACRES OF LAND, MORE OR LESS.



STORM PROFILE
MH #1 TO CB #2
1"=20'

LEGEND



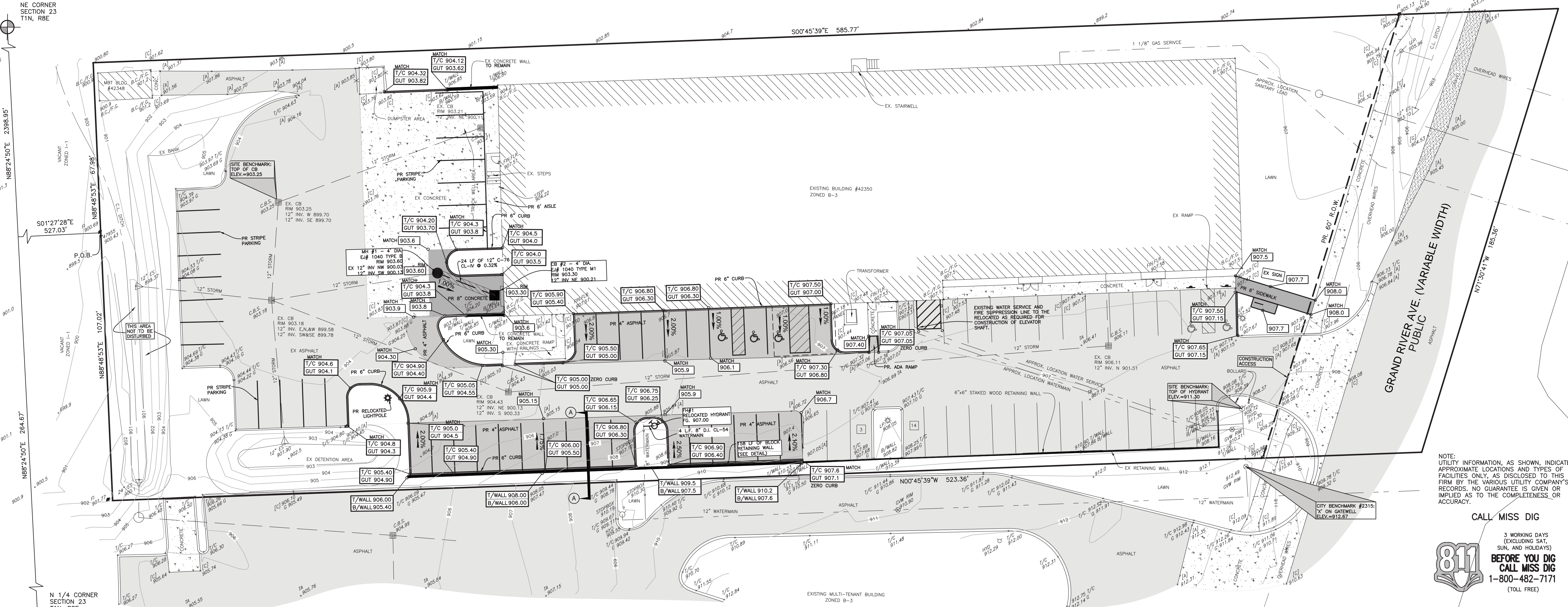
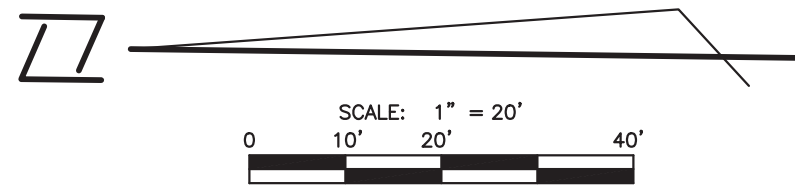
EXISTING ONSITE PAVEMENT - 28,089 SF.
EXISTING PAVEMENT TO REMAIN - 26,970 SF.
PROPOSED PAVEMENT - 5,751 SF.
NEW TOTAL ONSITE PAVEMENT - 32,721 SF.
NOTE: ADDITIONAL PARKING AREA WAS PART OF THE 1994 SITE PLAN APPROVAL CONDITIONS.



LOCATION MAP
1" = 600'

- SITE BENCHMARKS (NAVD88):**
- TOP OF HYDRANT ON WEST SIDE OF SITE ENTRANCE ELEV.: 911.30
 - TOP OF CATCHBASIN IN REAR OF BUILDING ELEV.: 903.25
- CITY BENCHMARK #2315**
X ON NORTH SIDE OF RIM OF GATEWELL LOCATED 6' NORTH OF SIDEWALK ON NORTH SIDE OF GRAND RIVER BETWEEN #42350 & #42400 GRAND RIVER. ELEV.: 912.67

QUANTITY ESTIMATE		
NO.	ITEM	QUANTITY
1.	4" ASPHALT	578 SY
2.	8" CONCRETE	61 SY
3.	234 SF	
4.	6" CONCRETE SIDEWALK	234 SF
5.	6" CURB	505 LF
6.	RETAINING WALL	158 LF
7.	12" C-76 CL-IV	24 LF
8.	4" DIA CATCHBASIN	1 EA
9.	4" DIA MANHOLE	1 EA
10.	RELOCATED HYDRANT	1 EA
11.	8" D.I. CL-54 WATERMAIN	4 LF



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DATE	REMARKS
2/27/19	PER CLIENT
5/8/19	PER CITY
6/7/19	PER CITY
6/24/19	ENG. DESIGN
10/14/19	PER CITY

SEAL

CLIENT
ARIS BERRIS
1072 EQUESTRIAN DRIVE
SOUTH LYON, MI 480178
248-308-2846

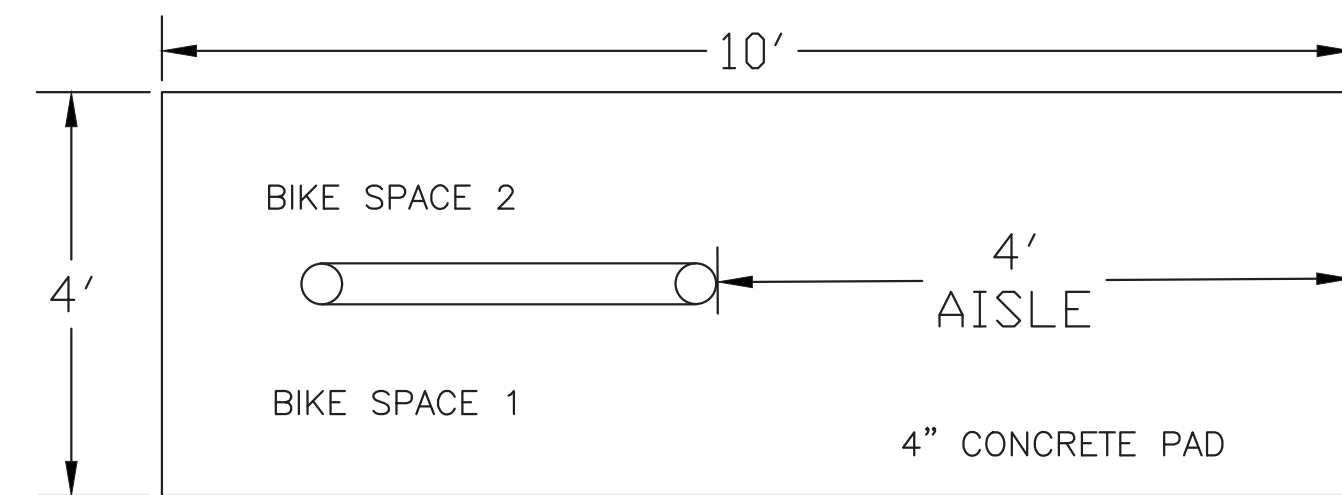
PROJECT NAME
SLI MEDICAL

PROJECT NUMBER
C18-033.06X

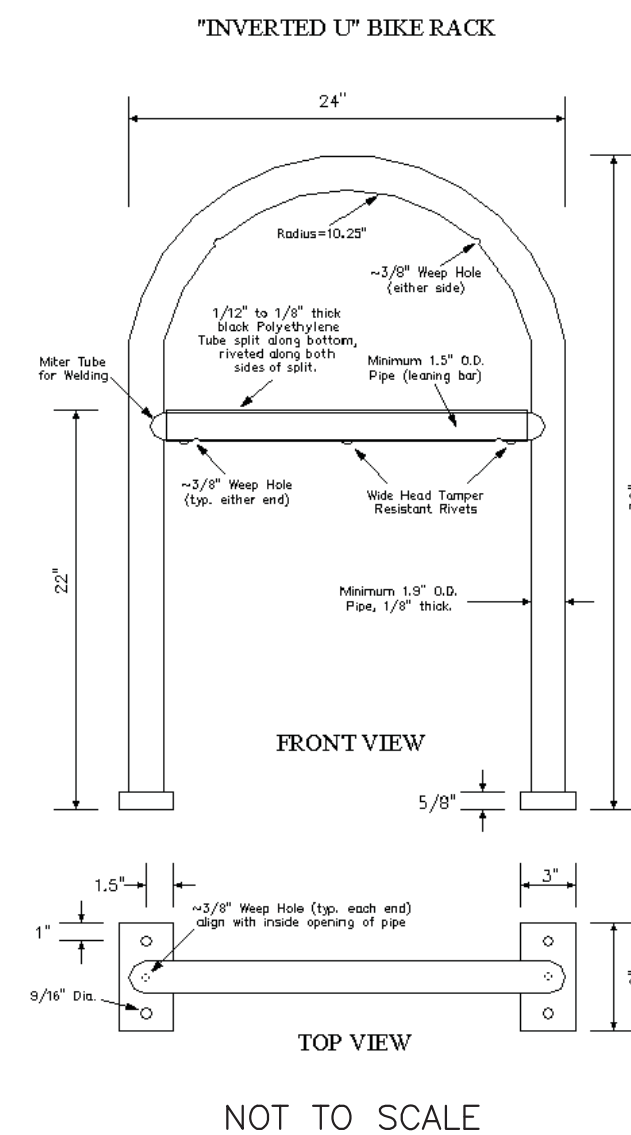
SHEET NAME
GRADING, PAVING & UTILITY PLAN

DRAWING SCALE: 1" = 20'
DATE: 07/08/19
PROJECT MANAGER: JSR, P.E.
DESIGNED BY: J.H.
DRAWN BY: J.H.
FIELD BY: BH, VA
CHECKED BY: TWD/JSR, PE

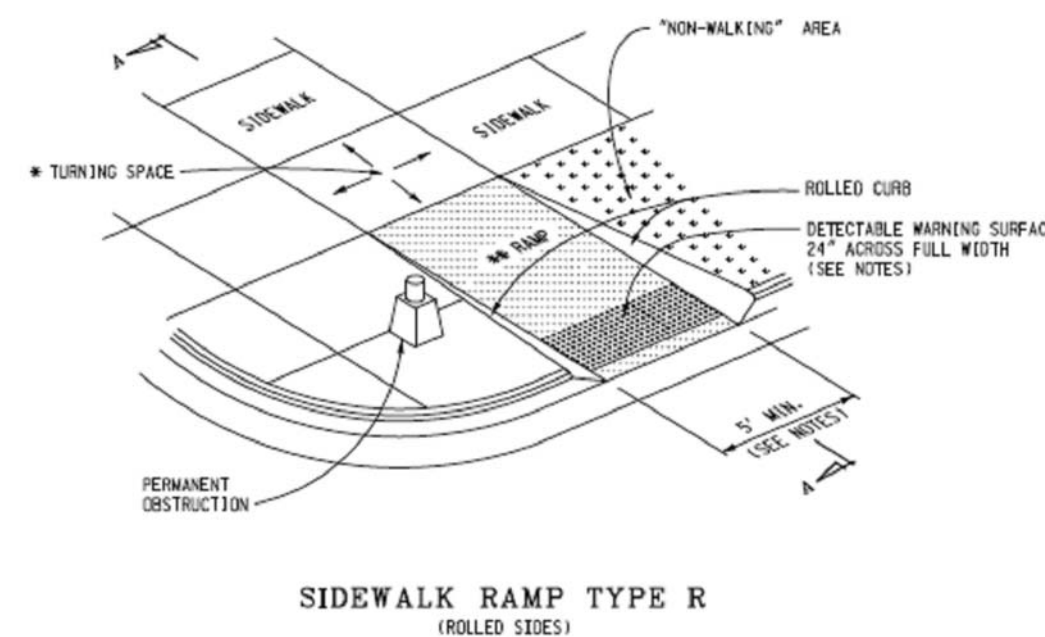
SHEET NUMBER
4 OF 6



INVERTED "U" BIKE PAD DETAIL
NOT TO SCALE



NOT TO SCALE



SIDEWALK RAMP TYPE R
(ROLLED SIDES)



Height of symbol:
Minimum = 28 inches
Special = 41 inches

Width of symbol:
Minimum = 24 inches
Special = 36 inches

*Stroke width:
Minimum = 3 inches
Special = 4 inches

INTERNATIONAL SYMBOL OF
ACCESSIBILITY PAVEMENT DETAIL
NOT TO SCALE



HANDICAP SIGN
(R7-8)



VAN ACCESSIBLE
(R7-8b)



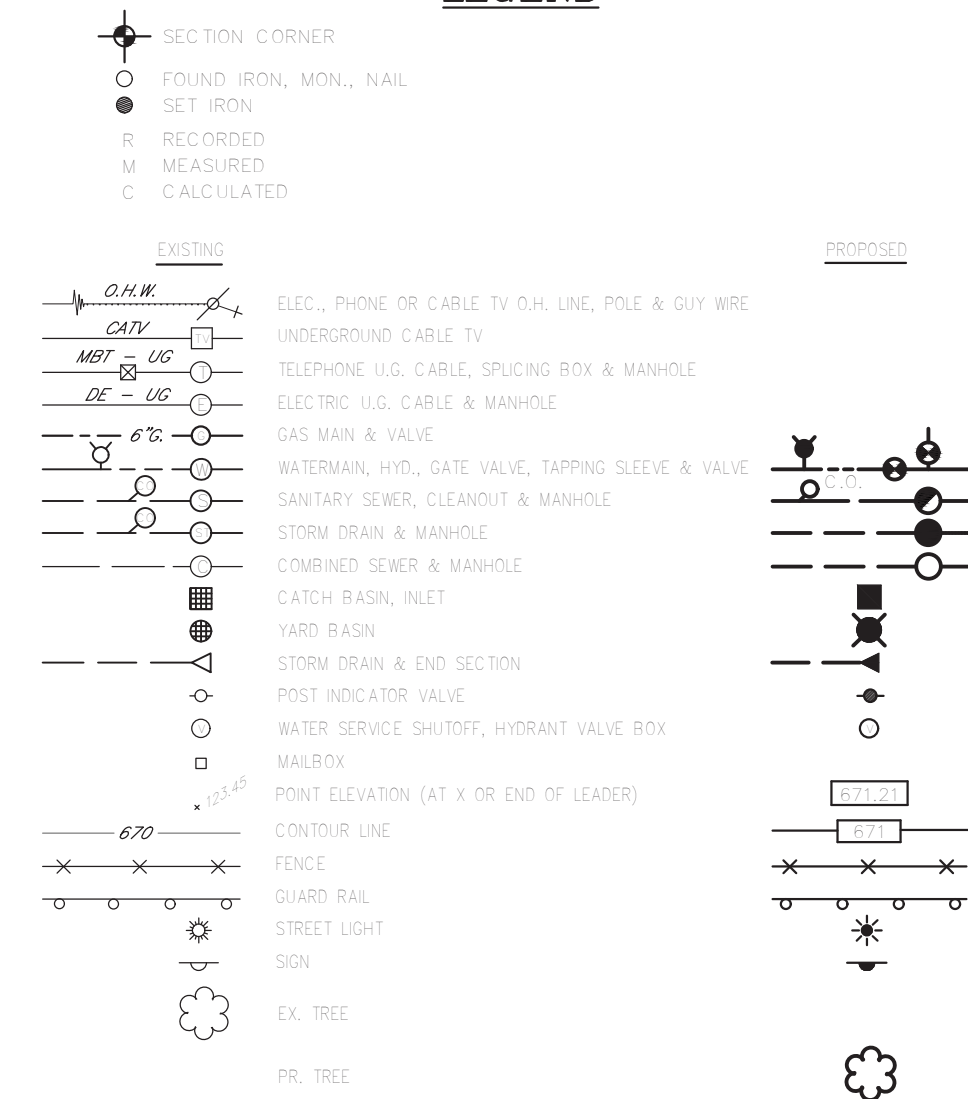
RESERVED SIGN

PARCEL DESCRIPTION

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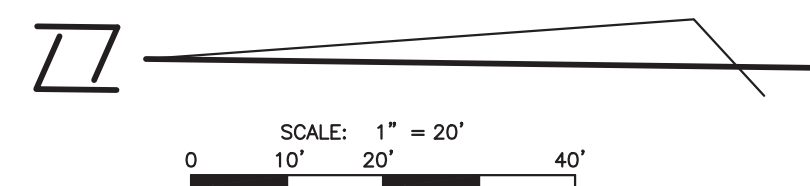
LEGEND



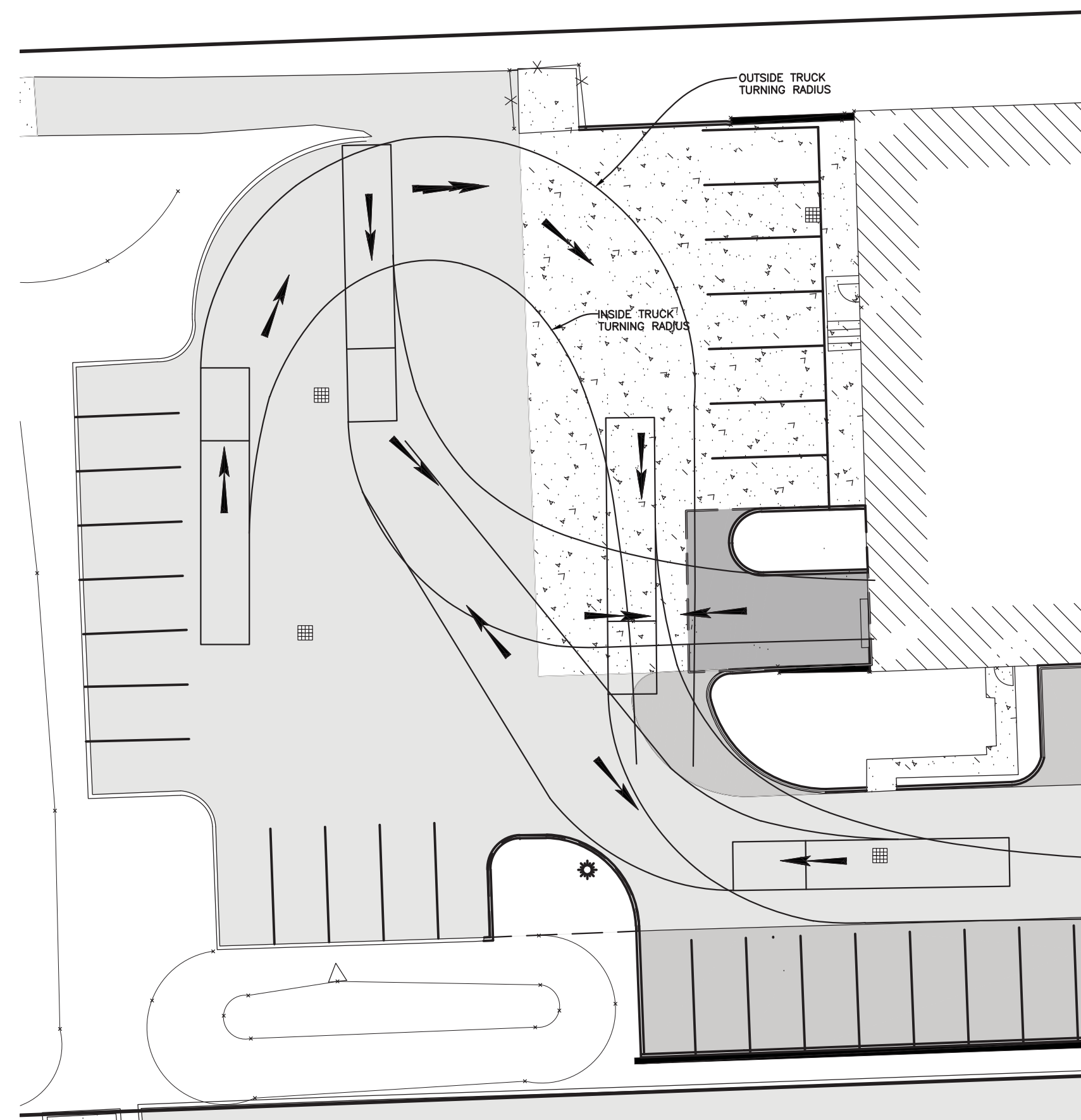
SIGN QUANTITIES TABLE		
TYPE	QUANTITY	DIMENSIONS
HANDICAP SIGN	4	12"x18"
VAN ACCESSIBLE	1	12"x18"
RESERVED SIGN	1	12"x18"

NOTES:

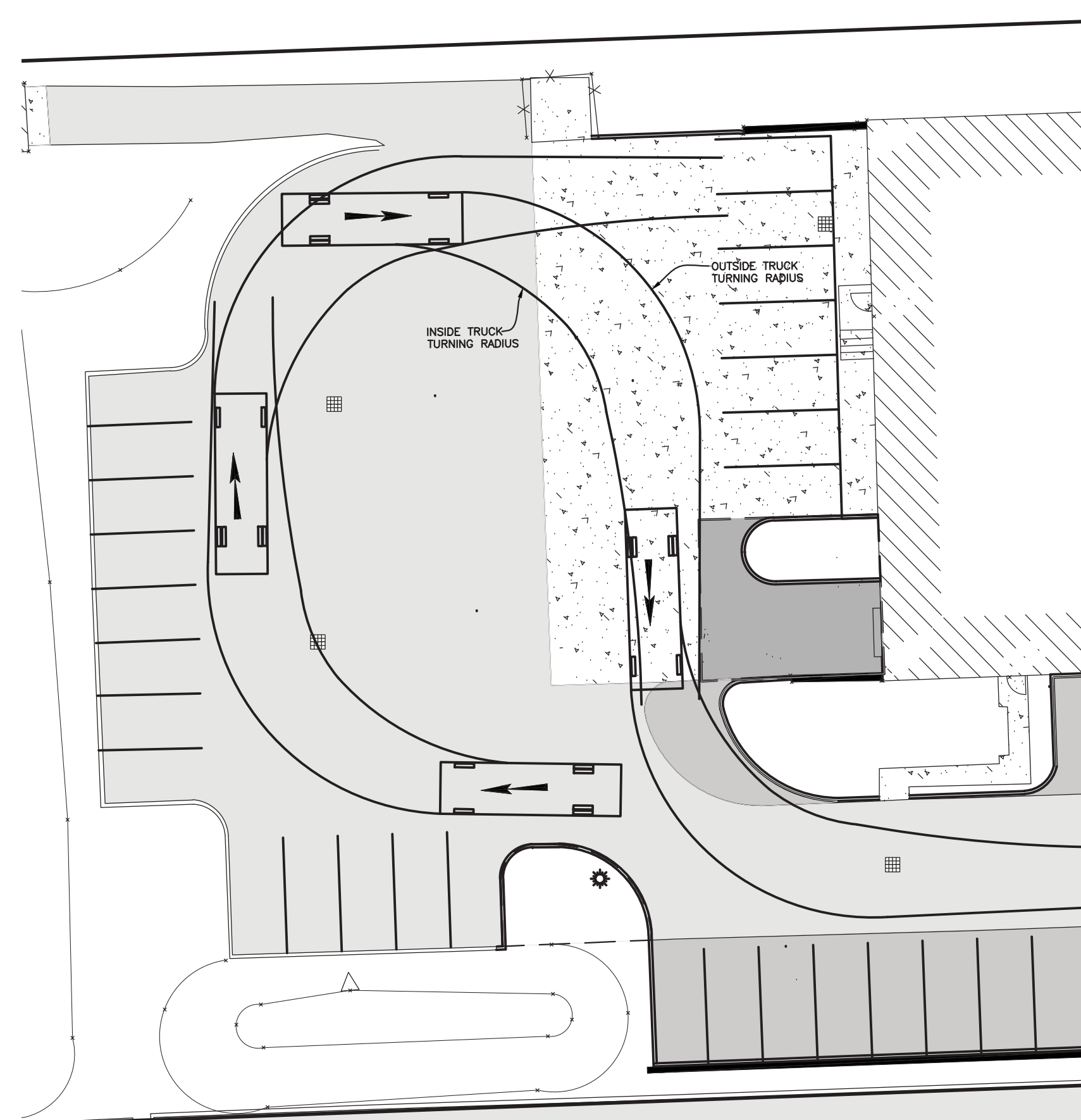
- SIGNING SHALL BE PLACED 2 FEET FROM CURB OR SIDEWALK EDGE.
- ALL SIGNS SHALL BE 7 FEET ABOVE THE FINAL GRADE.
- ALL TRAFFIC SIGNAGE WILL COMPLY WITH THE CURRENT MUTCD STANDARDS.
- SINGLE SIGNS WITH NOMINAL DIMENSIONS OF 12"x18" OR SMALLER SIZE SHALL BE MOUNTED ON A GALVANIZED 2 LB. U-CHANNEL POST. MULTIPLE SIGNS AND OR/SIGNS WITH NOMINAL DIMENSION GREATER THAN 12"x18" SHALL BE MOUNTED ON GALVANIZED 3 LB. OR GREATER U-CHANNEL POST AS DICTATED BY THE WEIGHT OF THE PROPOSED SIGNS.
- TRAFFIC CONTROL SIGNS SHALL USE FHWA STANDARD ALPHABET SERIES.
- TRAFFIC CONTROL SIGNS SHALL HAVE HIGH INTENSITY PRISMATIC (HIP) SHEETING TO MEET FHWA RETROREFLECTIVITY REQUIREMENTS.
- WHEN A STANDARD PARKING SPACE IS ADJACENT TO AN ACCESSIBLE SPACE, ABUTTING BLUE AND YELLOW STRIPES SHALL BE INSTALLED.
- ALL PAVEMENT MARKINGS SHALL BE 4" THICK AND YELLOW STRIPES TO MATCH EXISTING MARKINGS.
- THE LARGEST VEHICLE THAT WILL ACCESS THE SITE IS A WB-40 TRUCK.
- ALL WORK SHALL CONFORM TO THE CITY OF NOVI STANDARDS AND SPECIFICATIONS.



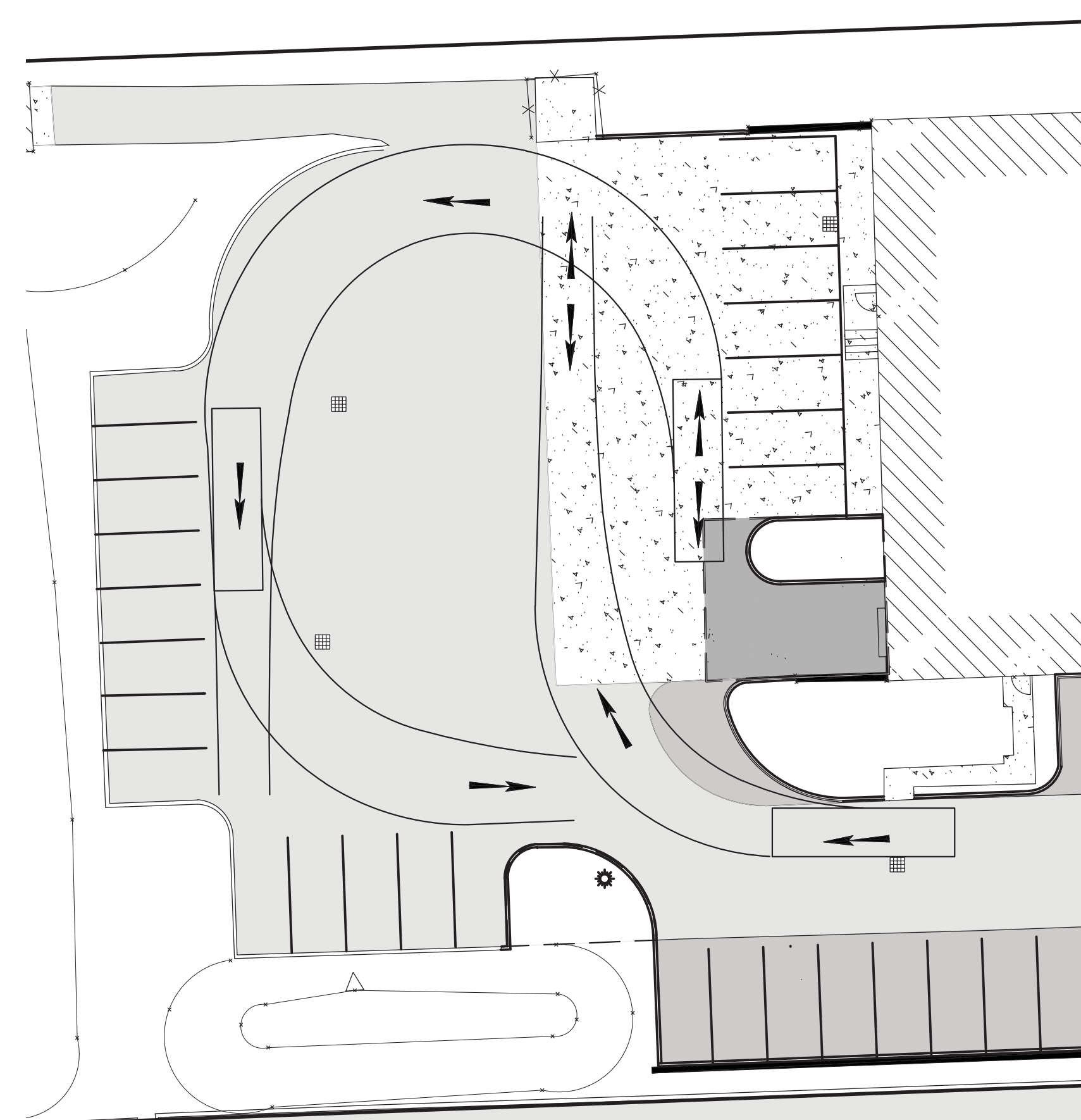
WB-40 TRUCK TURNING PLAN



FIRE TRUCK TURNING PLAN



DUMPSTER TRUCK TURNING PLAN



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6/7/19	PER CITY
6/24/19	ENG. DESIGN
10/14/19	PER CITY

SEAL

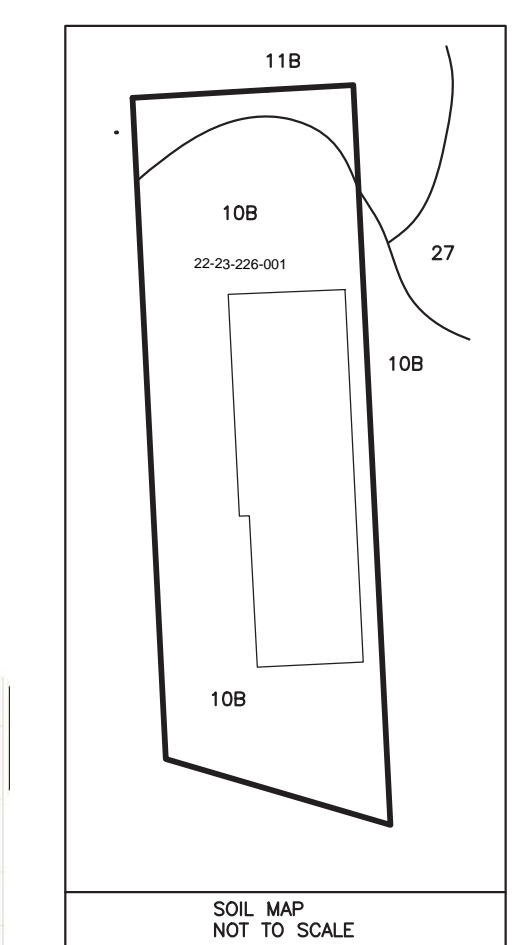
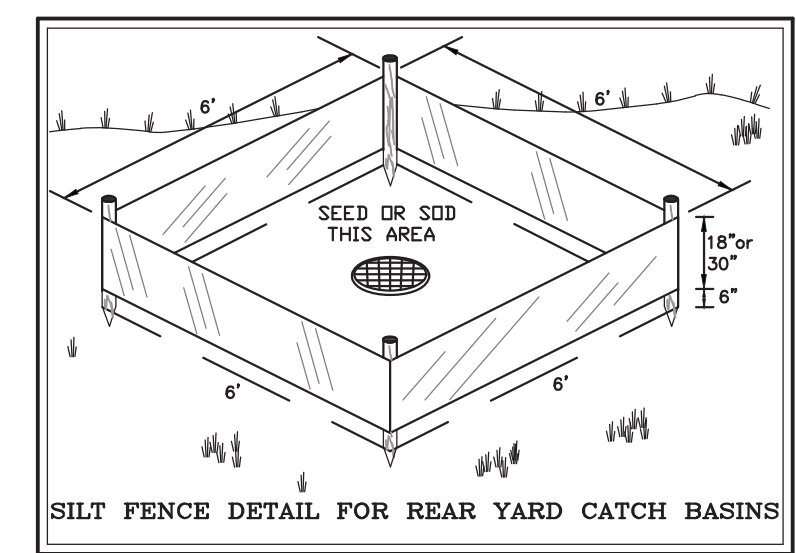
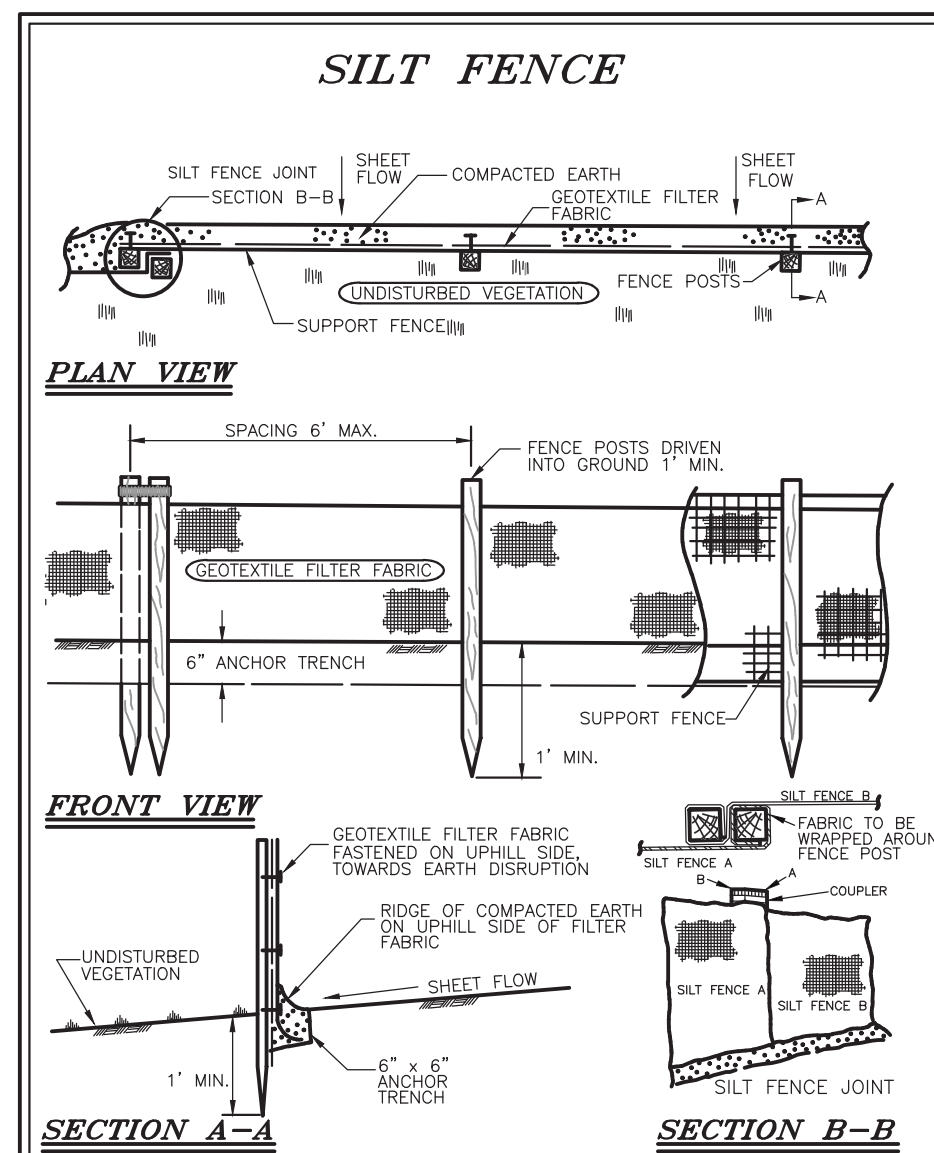
CLIENT
ARIS BERRIS
1072 EQUESTRIAN DRIVE
SOUTH LYON, MI 480178
248-308-2846

PROJECT NAME
SLI MEDICAL

PROJECT NUMBER
C18-033.06X

SHEET NAME
TRUCK TURNING PLAN

DRAWING SCALE: 1" = 20'
DATE: 09/25/18
PROJECT MANAGER: JSR, P.E.
DESIGNED BY: JJS, P.E.
DRAWN BY: JJS, P.E.
FIELD BY: BH, VA
CHECKED BY: TWD/JSR, PE



Oakland County, Michigan (M1125)

Map Unit Symbol	Map Unit Name	Area in AOI	Percent of AOI
10B	Marlette sandy loam, 1 to 6 percent slopes	3.1	78.6%
11B	Capac sandy loam, 0 to 4 percent slopes	0.6	15.4%
27	Houghton and Adrian muskeg	0.2	6.0%
Totals for Area of Interest		4.0	100.0%

SOIL DATA

TOTAL DISTURBED AREA: 0.19 ACRES
 THE CONTRACTOR SHALL CONSTRUCT THIS PROJECT IN COMPLIANCE WITH PART 91 OF ACT NO. 451 OF 1994, AS AMENDED IN THE SOIL EROSION AND SEDIMENTATION CONTROL ACT.

DISTANCE TO NEAREST WATER COURSE: POND 80 FT FROM EAST PROPERTY LINE

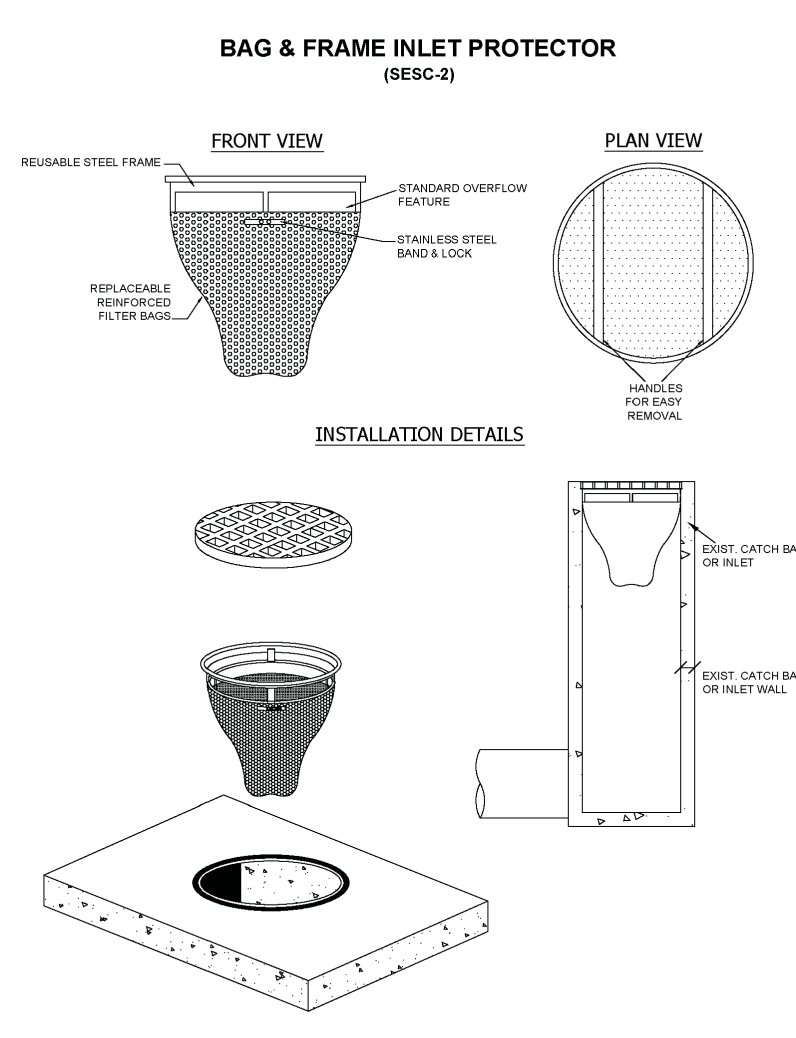
GENERAL SEDIMENTATION AND EROSION CONTROL NOTES

1. ALL EROSION AND SEDIMENT CONTROL WORK SHALL CONFORM TO STANDARDS AND SPECIFICATIONS OF THE CITY OF NOVI.
2. DAILY INSPECTIONS SHALL BE MADE BY THE CONTRACTOR TO DETERMINE EFFECTIVENESS OF EROSION AND SEDIMENT CONTROL MEASURES. ANY NECESSARY REPAIRS SHALL BE PERFORMED WITH OUT DELAY.
3. EROSION AND ANY SEDIMENTATION FROM WORK ON THIS SITE SHALL BE CONTAINED ON THE SITE AND NOT ALLOWED TO COLLECT ON ANY OFF SITE AREAS OR IN WATERWAYS. WATERWAYS INCLUDE BOTH NATURAL AND MAN-MADE OPEN DITCHES, STREAMS, STORM DRAINS, LAKES AND PONDS.
4. CONTRACTOR SHALL APPLY TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURE WHEN REQUIRED AND AS DIRECTED ON THESE PLANS. HE SHALL REMOVE TEMPORARY MEASURES AS SOON AS PERMANENT STABILIZATION OF SLOPES, DITCHES AND OTHER EARTH CHANGES HAVE BEEN ACCOMPLISHED.
5. STAGING THE WORK WILL BE DONE BY THE CONTRACTOR AS DIRECTED IN THESE PLANS AND AS REQUIRED TO ENSURE PROGRESSIVE STABILIZATION OF DISTURBED EARTH.
6. IMMEDIATELY AFTER SEEDING MULCH ALL SEEDING AREAS WITH UNWEATHERED SMALL GRAIN STRAW OR HAY, SPREAD UNIFORMLY AT THE RATE OF 1-1/2 TO 2 TONS PER ACRE OR 100 POUNDS (2 OR 3 BALES) PER 1,000 SQUARE FEET. THIS MULCH SHOULD BE ANCHORED WITH A DISC-TYPE MULCH ANCHORING TOOL OR OTHER MEANS AS APPROVED BY THE CITY.
7. ALL MUD/DIRT TRACKED INTO EXISTING STATE/COUNTY ROADS FROM THIS SITE, DUE TO CONSTRUCTION, SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR/BUILDER. ALL MUD/DIRT TRACKED OR SPILLED ON PAVED ROAD SURFACES WITHIN THIS SITE SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR/BUILDER.
8. PERMANENT SOIL EROSION CONTROL MEASURES FOR ALL SLOPES, CHANNELS, DITCHES, OR ANY DISTURBED LAND AREA SHALL BE COMPLETED WITHIN 9 CALENDAR DAYS AFTER FINAL GRADING OR THE FINAL EARTH CHANGE HAS BEEN COMPLETED. WHEN IT IS NOT POSSIBLE TO PERMANENTLY STABILIZE A DISTURBED AREA AFTER EARTH CHANGE HAS BEEN COMPLETED OR WHERE SIGNIFICANT EARTH CHANGE ACTIVITY CEASES, TEMPORARY SOIL EROSION CONTROL MEASURES SHALL BE IMPLEMENTED IMMEDIATELY. ALL TEMPORARY SOIL EROSION CONTROL SHALL BE MAINTAINED UNTIL PERMANENT SOIL EROSION CONTROL MEASURES ARE IMPLEMENTED. ALL PERMANENT SOIL EROSION CONTROL MEASURES WILL BE IMPLEMENTED AND ESTABLISHED BEFORE A CERTIFICATE OF COMPLIANCE IS ISSUED.

SOIL EROSION CONTROL NOTES

- 1.) ALL SOIL EROSION CONTROL MEASURES SHALL BE INSTALLED AS INDICATED ON THE APPROVED PLAN PRIOR TO THE START OF ANY CONSTRUCTION
- 2.) THE SEDIMENT CONTROL MEASURES WILL BE MAINTAINED, AS NEEDED, AND WILL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE AND THE DISTURBED AREAS ARE STABILIZED WITH VEGETATION.
- 3.) ALL STOCKPILED SOILS SHALL BE MAINTAINED IN SUCH A WAY AS TO PREVENT EROSION FROM THE SITE.
- 4.) ALL MUD TRACKED ONTO THE EXISTING CITY ROADS FROM THE SITE DUE TO CONSTRUCTION OR THE DELIVERY OF MATERIALS SHALL BE REMOVED PROMPTLY BY THE BUILDER.
- 5.) DURING THE CONSTRUCTION OF THE BUILDINGS WITHIN THE SITE, THE DEVELOPER WILL ARRANGE TO HAVE THE SITE STREETS SWEEP AND CLEANED AND INLET FILTERS CLEANED ON A WEEKLY BASIS (AT A MINIMUM).
- 6.) DUST CONTROL TO BE PROVIDED BY CONTRACTOR/BUILDER AS NECESSARY.

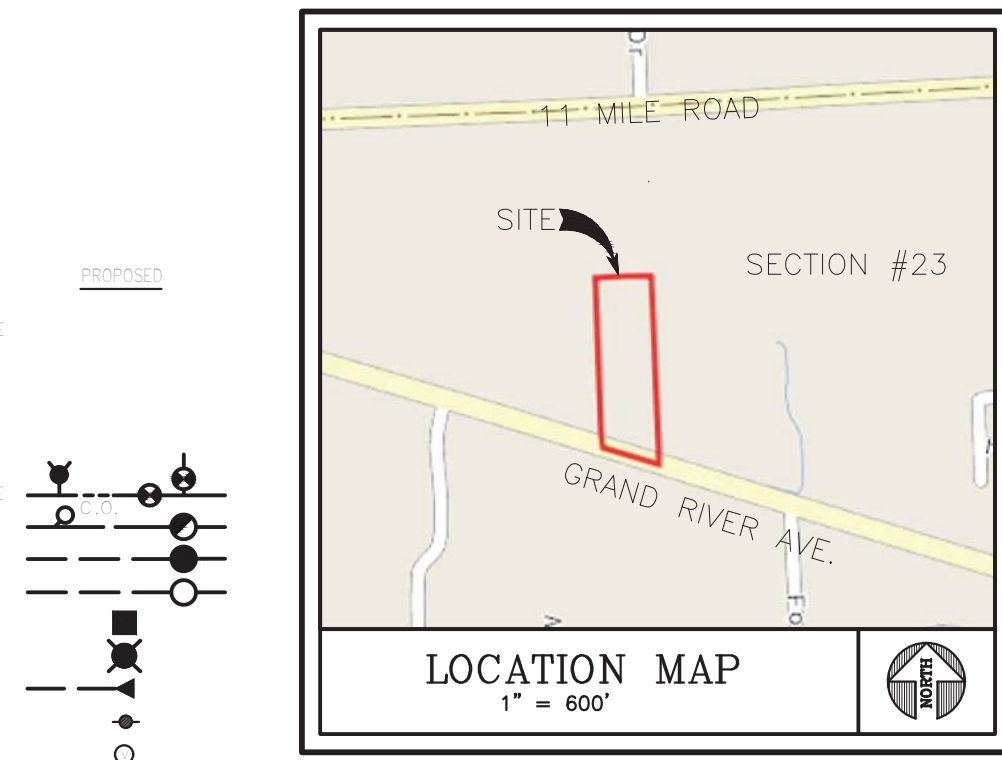
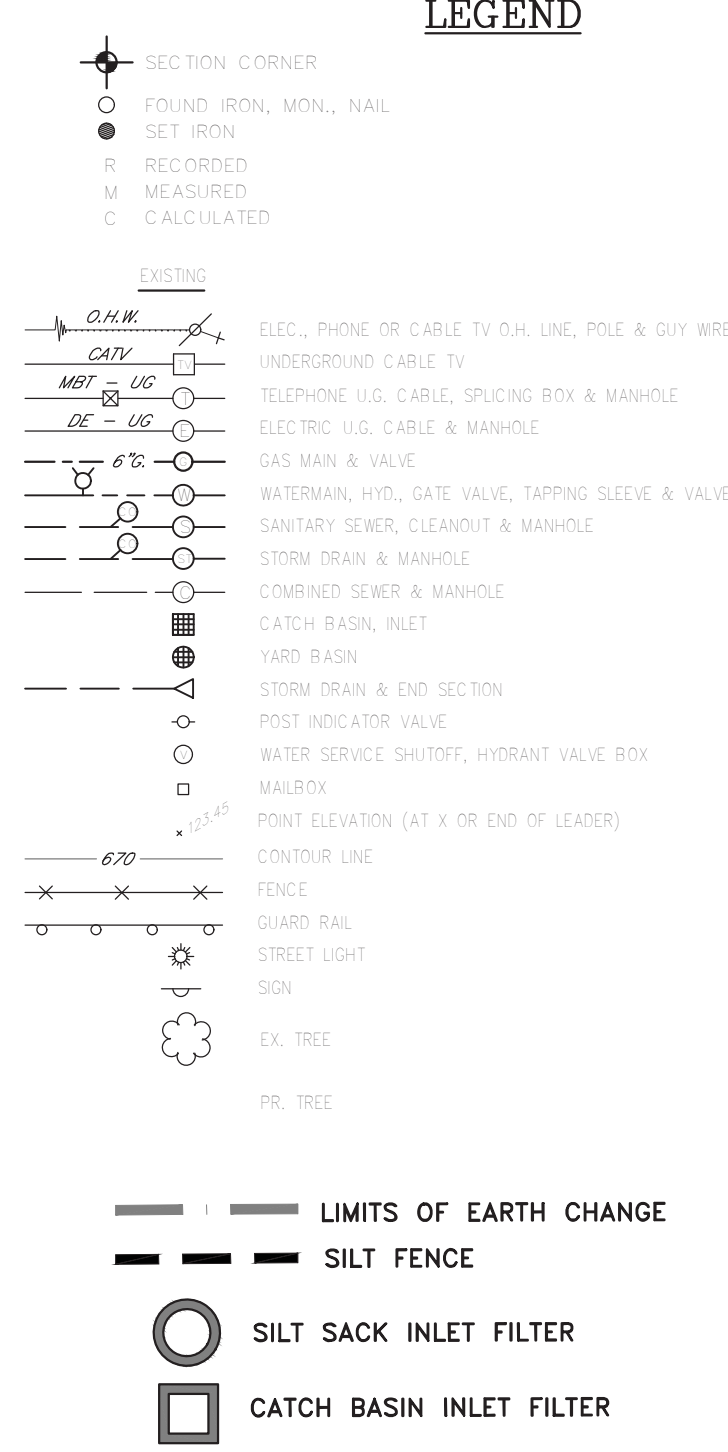
CONSTRUCTION SEQUENCE	OPERATION TIME SCHEDULE											
	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
A SOIL EROSION CONTROL MEASURES												
B MASS GRADING & UTILITIES CONSTRUCTION												
C PAVING												
D PERMANENT SEEDING & SODDING												



QUANTITY ESTIMATE

NO.	ITEM	QUANTITY
1.	CATCH BASIN INLET FILTER	6 EA
2.	SILT FENCE	639 LF

NOTE: CONTRACTOR SHALL CALCULATE HIS OWN QUANTITIES FOR ALL ITEMS OF WORK WHETHER OR NOT INDICATED ON THESE PLANS. THE OWNER OR ENGINEER IS NOT RESPONSIBLE FOR ANY DIFFERENCES IN QUANTITIES.

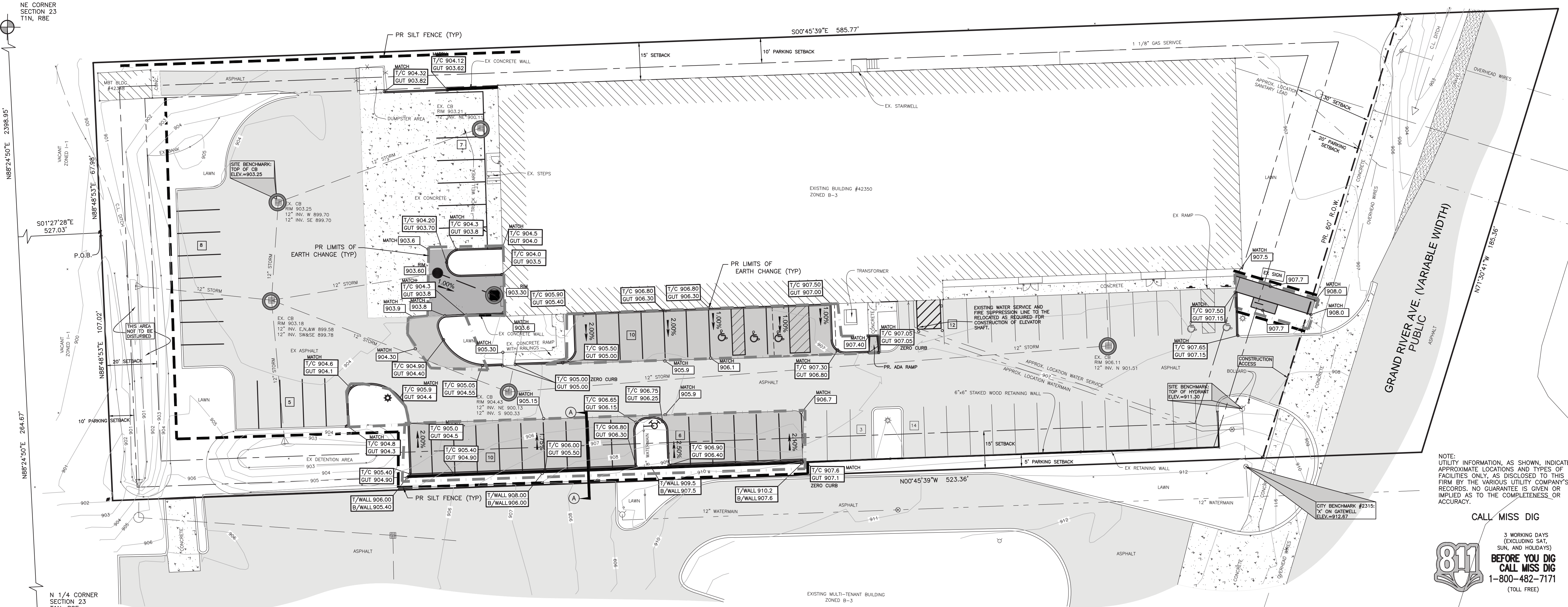


- SITE BENCHMARKS (NAV88):**
1. TOP OF HYDRANT ON WEST SIDE OF SITE ENTRANCE ELEV.: 911.30
 2. TOP OF CATCHBASIN IN REAR OF BUILDING ELEV.: 903.25

CITY BENCHMARK #2315
 X ON NORTH SIDE OF RIM OF GATEWELL LOCATED 5' NORTH OF SIDEWALK ON NORTH SIDE OF GRAND RIVER BETWEEN #42350 & #42400 GRAND RIVER. ELEV.: 912.67

PARCEL DESCRIPTION
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SITE DATA:
 ADDRESS: 42350 GRAND RIVER AVENUE
 PID: 22-23-226-001
 ZONED: B-3, GENERAL BUSINESS
 AREA: 2.23 ACRES



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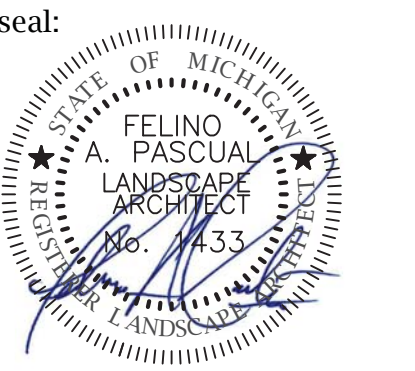
PROJECT NAME
SLI MEDICAL

PROJECT NUMBER
C18-033.06X

SHEET NAME
S.E.S.C. PLAN

DRAWING SCALE: 1" = 20'
 DATE: 07/08/19
 PROJECT MANAGER: JSR, P.E.
 DESIGNED BY: J.H.
 DRAWN BY: J.H.
 FIELD BY: BH, VA
 CHECKED BY: TWDJSR, PE

REVISED LANDSCAPE PLAN



client:
ARIS BERRIS
 1702 EQUESTRIAN
 DRIVE, SOUTH
 LYON, MI 480178

project:
**SLI MEDICAL
 BUILDING**

project location:
 City of NOVI

GRAND RIVER AVE

sheet title:
**overall landscape
 planting detail**

job no./issue/revision date:
 LS19.081.06 SPA 6-7-2019
 LS19.081.07 REVIEW 7-22-2019
 LS19.081.07 SPA 7-29-2019
 LS19.081.10 SPA 10-14-2019

drawn by:
JP

checked by:
FP

date:
6-5-2019

notice:
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 and Associates

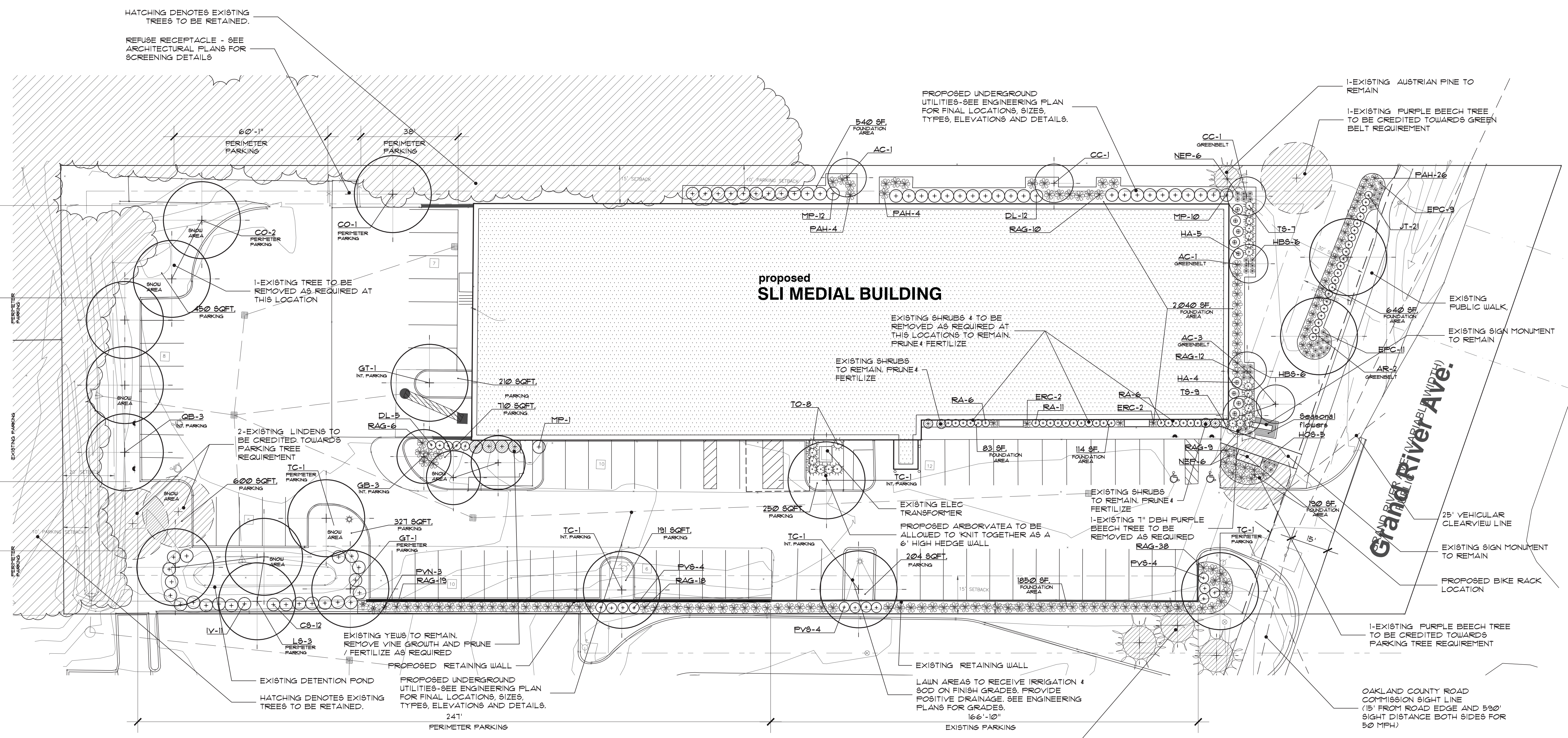
Do Not scale drawings. Use
 figured dimensions only



The location and elevations of existing
 underground utilities as shown on this
 drawing are only approximate; no guarantee
 is either expressed or implied as to the
 completeness of accuracy; contractor shall be
 exclusively responsible for determining the
 exact location and elevation prior to the start
 of construction

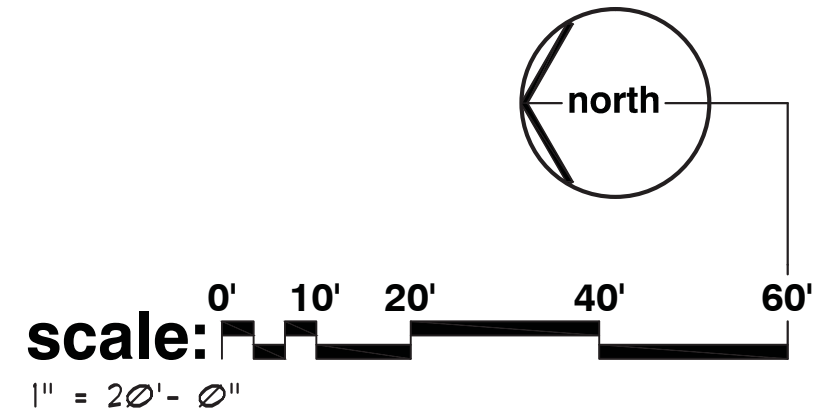
project no:
LS19.081.06

sheet no:
LS-1 of 2



landscape improvement plan for:
SLI Medical Building
 City of Novi, Michigan

note:
 unless noted otherwise, numerical value on landscape
 quantities specified on plan take precedence over
 graphic representation.



**LANDSCAPE WAIVERS GRANTED BY THE PLANNING COMMISSION
 ON JUNE 12, 2019**

- Lack of street trees along Grand River Avenue frontage due to conflicts with existing utilities in right-of-way
- Not meeting the minimum required parking lot perimeter trees due to a lack of room between the parking and the west property line, provided that the applicant work with the City's landscape architect to maximize number of trees along other areas of the parking lot.
- Reduction in required greenbelt width along Grand River Avenue due to existing conditions which were previously approved
- Landscape waiver from Sec. 5.5.3.B.ii for lack of berm along Grand River Avenue, due to existing grading which is not changing
- Less than 75% of the building foundation is landscaped, and some foundation area landscaping is located away from the building along the existing berm

commercial retail landscape requirements:

street trees (Grand River ave)	REQUIRED	PROVIDED
TOTAL LIN. FT. OF ROW, ROAD FRONTAGE-NET (LESS DRIVEWAY & 25' CLEARVIEW AREAS *105'-77'=108')	108'±	
ONE (1) 3" DECIDUOUS PER 35 LIN. FT. (108' / 35' PER TREE= 3.08 TREES)	3	(WAIVER #1)

greenbelt (Grand River ave)	REQUIRED	PROVIDED
ROAD FRONTAGE NO PARKING		
TOTAL LIN. FT. OF ROW, ROAD FRONTAGE NET (LESS DRIVEWAY & 25' CLEARVIEW AREAS *105'-77'=112')	108'±	
ONE (1) 3" DECIDUOUS OR EVERGREEN TREE PER 40 LIN. FT. (112' / 40' PER TREE= 2.8 TREES)	2	2-EXISTING
ONE (1) SUBCANOPY TREE PER 20 LIN. FT. (112' / 20' PER TREE= 5.6 TREES)	3	3

ROAD FRONTAGE WITH PARKING FRONTAGE	REQUIRED	PROVIDED
TOTAL LIN. FT. OF ROW, ROAD FRONTAGE NET	40'±	
ONE (1) 3" DECIDUOUS OR EVERGREEN TREE PER 40 LIN. FT. (40' / 35' PER TREE= 1.14 TREE)	1	1-EXISTING
ONE (1) SUBCANOPY TREE PER 20 LIN. FT. (40' / 20' PER TREE= 2 TREES)	2	2

parking lot trees (CATEGORY-I)	REQUIRED	PROVIDED
VEHICULAR ACCESS AREA		32,504 ± SF.
VEHICULAR AREA FORMULA (32,504 SF. X .075 (7.5%) = 2,437.8 SF.)	2,437.8 SF.	
TOTAL AREA OF INTERIOR LANDSCAPE ISLANDS AREA	2,421 SF.	2,942 ± SF.
TOTAL NO. OF PARKING LOT ISLAND TREES (2,437.8 SF. LANDSCAPE ISLANDS / 200 = 12.18 TREES)	12 TREES	9-TREES + 3-EXISTING
TOTAL NO. OF PERIMETER PARKING TREES	19 TREES	11-TREES (WAIVER #2)
LENGTH OF PERIMETER PARKING (680 FT. / 35 FT (1-TREE PER 35 FT) = 19.42 TREES)	19 TREES	

building foundation	REQUIRED	PROVIDED
BUILDING FOUNDATION LANDSCAPE AREA (601' PERIMETER LIN. FT.) X 8' = 4,808 SF.)	4,808 SF.	5,451 ± SF.

	REQUIRED	PROVIDED
BUILDING PERIMETER (FOUNDATION)	601±	
BLDG FOUNDATION PLANTINGS=75% BLDG TOTAL LENGTH (601' X .75 (75%) = 450.75' OF PLANTING REQUIRED)	451'	473' (78% & WAIVER #5)
BLDG FOUNDATION PLANTINGS PROVIDED (WEST BLDG PLANTINGS= 120') (EAST BLDG PLANTINGS= 200') (SOUTH BLDG PLANTINGS= 49') (BERM PLANTINGS= 60')		

plant material list

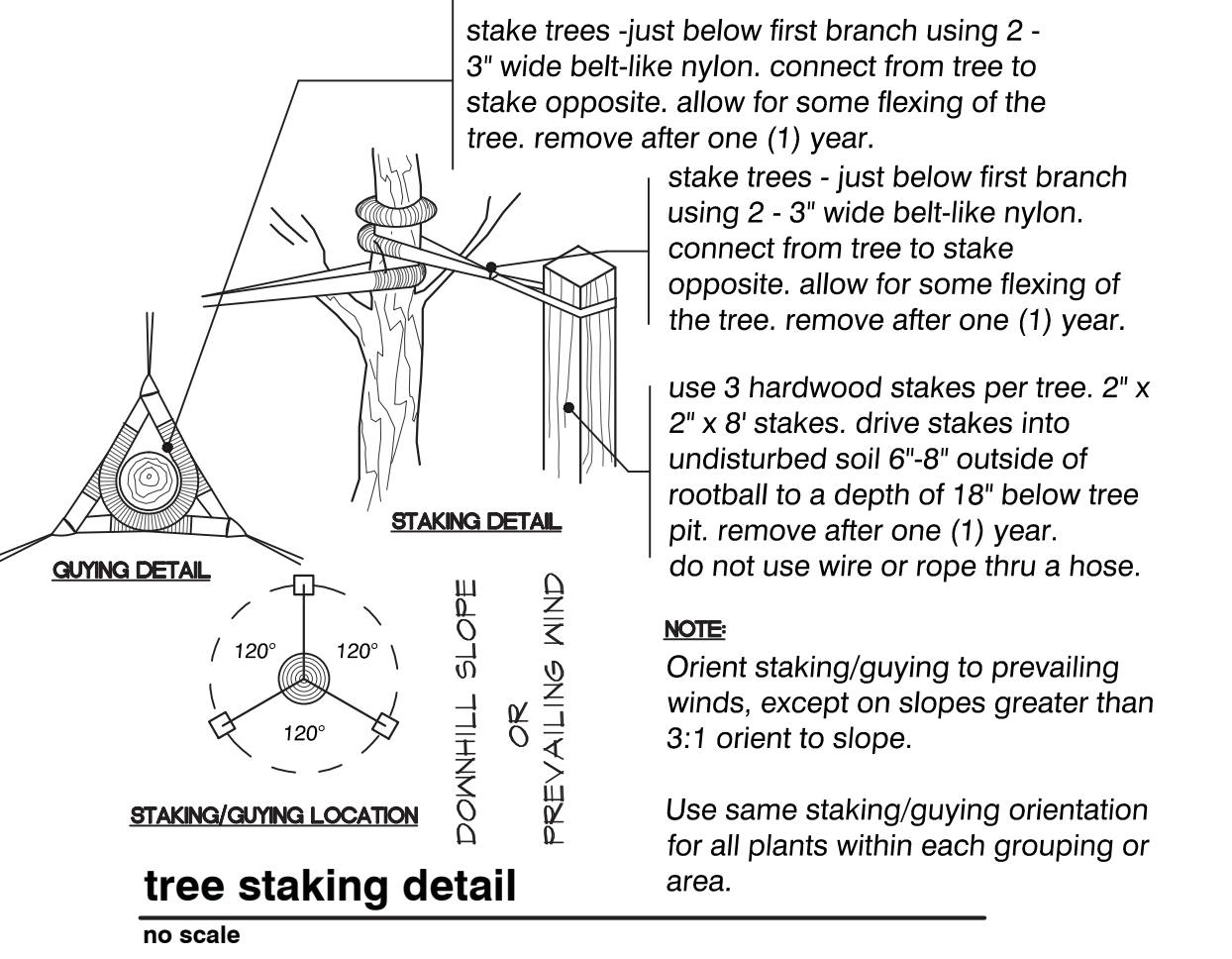
key	quant.	botanical name	common name	size	native	comments	unit cost	total	
LARGE AND SMALL DECIDUOUS TREES									
GT	2	GLEDITSIA TRIACANTHOS	HONEYLOCUST	3" BB	NATIVE		1400	1800	
LB	3	LIQUIDAMBAR STRACIFLUA	AMERICAN SWEETGUM	3" BB	NATIVE		1400	1200	
CO	3	CELTIS OCCIDENTALIS	HACKBERRY TREE	3" BB	NATIVE		1400	1200	
AR	2	ACER RUBRUM	RED MAPLE	3" BB	NATIVE		1400	1800	
GB	3	GINKGO BILOBA 'PRINCETON SENTRY'	PRINCETON SENTRY MAIDENHAIR TREE	3" BB			1400	1200	
TC	5	TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LINDEN	3" BB			1400	12000	
QB	3	QUERCUS BI-COLOR	WHITE SWAMP OAK	3" BB	NATIVE		1400	1200	
AC	5	AMELANCHIER CANADENSIS	SHADBLOW SERVICEBERRY	8" BB	NATIVE (MULTI-STEM)		1250	1250	
CC	2	CERSIS CANADENSIS	EASTERN REDBUD	8" BB	NATIVE (MULTI-STEM)		1250	1500	
TOTAL								10150	
SHRUBS									
DL	11	DIERVILLA LONICERA	BUSH HONEYSUCKLE	5" CONT.	NATIVE		150	1650	
HA	9	HYDRANGEA ANNEBELLE	ANNABELLE HYDRANGEA	5" CONT.			150	1350	
MP	23	MYRICA PENNSYLVANICA	BARBERRY	3" BB	NATIVE	42" O.C.	150	1150	
JT	21	JUNIPERUS S. 'TAMARISAFOLIA'	TAM'S JUNIPER	24" BB			150	11050	
RAG	12	RHUS AROMATICA 'GRO-LOW'	GRO-LOW FRAGRANT SUMAC	5" CONT.	HYB. NATIVE		150	1500	
TS	16	TAXUS 'SEBIAN'	SEBIAN YEW	24" 30" BB		32" O.C.	150	1800	
TO	8	THUJA OCCIDENTALIS 'TECHNY'	MISSION ARBORVITAE	6" BB		32" O.C.	150	1400	
CS	12	CORNUS STOLONIFERA	REDTUIG DOGWOOD	5" CONT.	NATIVE		150	1600	
IV	11	ILEX VERTICILLATA	MICHIGAN HOLLY	5" CONT.	NATIVE		150	1550	
RA	23	RIBES ALPNUM	ALPINE CURRANT	5" CONT.			150	1150	
TOTAL								17600	
PERENNIALS AND GRASSES									
ERC	4	EUPATORIUM RUGOSUM 'CHOCOLATE'	JOE PYE WEED	5" CONT.	HYB. NATIVE		115	160	
FYS	12	PANICUM VIRGATUM 'SHENANDOAH'	SHENANDOAH SWITCH GRASS	5" CONT.	HYB. NATIVE	36" O.C.	115	1180	
PAH	34	PENNISETUM ALPHECUIROIDES 'HAMELN'	DWARF FOUNTAIN GRASS	5" CONT.		36" O.C.	115	1510	
HBS	12	HEUCHERA 'BERRY SMOOTHIE'	BERRY SMOOTHIE CORAL	4" CONT.	HYB. NATIVE	24" O.C.	115	1180	
NEP	12	NEPETA X FAASSENII 'WALKER'S LOW'	WALKER'S LOW CATNIP	4" CONT.		24" O.C.	115	1180	
HOS	5	HOSTA 'PATRIOT'	PATRIOT HOSTA	4" CONT.	HYB. NATIVE	24" O.C.	115	115	
EPC	20	ECHINACEA 'CRAZY PINK'	CRAZY PINK CONE FLOWER	4" CONT.	HYB. NATIVE	24" O.C.	115	1300	
FVN	3	PANICUM VIRGATUM 'NORTHWIND'	NORTHWIND SWITCH GRASS	5" CONT.	HYB. NATIVE	36" O.C.	115	145	
TOTAL								11330	
					66% NATIVE				

general landscape notes:

- LANDSCAPE CONTRACTOR SHALL VISIT THE SITE, INSPECT EXISTING CONDITIONS, REVIEW PROPOSED PLANTINGS AND RELATED WORK, CONTACT THE OWNER AND/OR LANDSCAPE ARCHITECT WITH ANY CONCERNS OR DISCREPANCY BETWEEN THE PLAN, PLANT MATERIAL LIST, AND/OR SITE CONDITIONS.
- PRIOR TO BEGINNING OF CONSTRUCTION ON ANY WORK, CONTRACTORS SHALL VERIFY LOCATIONS OF ALL ON SITE UTILITIES, GAS, ELECTRIC, TELEPHONE, CABLE TO BE LOCATED BY CONTACTING MISS DIG 1-800-482-7171. ANY DAMAGE OR INTERRUPTION OF SERVICES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. COORDINATE ALL RELATED WORK ACTIVITIES WITH OTHER TRADES AND REPORT ANY UNACCEPTABLE JOB CONDITIONS TO OWNER PRIOR TO COMMENCING.
- NUMERICAL VALUE ON THE LANDSCAPE QUANTITIES SPECIFIED ON THE PLAN TAKE PRECEDENCE OVER GRAPHIC REPRESENTATION. VERIFY ANY CONCERN-DISCREPANCY WITH LANDSCAPE ARCHITECT.
- ALL CONSTRUCTION AND PLANT MATERIAL LOCATION TO BE ADJUSTED ON SITE IF NECESSARY.
- ALL SUBSTITUTIONS OR DEVIATIONS FROM THE LANDSCAPE PLAN MUST BE APPROVED IN WRITING BY CITY OF NOVI.
- ALL LARGE TREES AND EVERGREENS TO BE STAKED, GUYED AND WRAPPED AS DETAILED, SHOWN ON PLAN.
- PLANT BEDS TO BE DRESSED WITH MIN. 4" OF FINELY DOUBLE SHREDDED HARDWOOD MULCH.
- DIG SHRUB PITS 1' LARGER THAN SHRUB ROOT BALLS AND TREE PITS 2' LARGER THAN ROOT BALL. BACK FILL WITH ONE PART TOP SOIL AND ONE PART SOIL FROM EXCAVATED PLANTING HOLE.
- NATURAL COLOR, FINELY SHREDDED HARDWOOD BARK MULCH REQUIRED FOR ALL PLANTINGS.
- REMOVE ALL TWINE, WIRE AND BURLAP FROM TREE AND SHRUB EARTH BALLS, AND FROM TREE TRUNKS. 4" THICK BARK MULCH FOR TREES IN 4" DIA. CIRCLE WITH 3" PULLED AWAY FROM TRUNK. 4" THICK BARK MULCH FOR SHRUBS AND 4" THICK BARK MULCH FOR PERENNIALS.
- PLANT MATERIAL QUALITY 4 INSTALLATION SHALL BE IN ACCORDANCE WITH THE CURRENT AMERICAN ASSOCIATION OF NURSERYMEN LANDSCAPE STANDARDS.
- PROVIDE PEAT SOD FOR ALL NEW AND DISTURBED LAWN AREAS UNLESS NOTED OTHERWISE.
- ALL PLANTING AREAS TO BE PREPARED WITH APPROPRIATE SOIL MIXTURES AND FERTILIZER BEFORE PLANT INSTALLATION.
- PLANT TREES AND SHRUBS GENERALLY NO CLOSER THAN THE FOLLOWING DISTANCES FROM SIDEWALKS, CURBS AND PARKING STALLS:
 - SHADE TREES _____ 5 FT.
 - ORNAMENTAL AND EVERGREEN TREES (CRAB, PINE, SPRUCE, ETC.) _____ 10 FT.
 - SHRUBS THAT ARE LESS THAN 1 FOOT TALL AND WIDE AT MATURITY _____ 2 FT.
- NO TREES OR EVERGREENS TO BE INSTALLED OVER ANY PROPOSED OR EXISTING UTILITY LINES AS SHOWN ON THE OVERALL LANDSCAPE PLAN. SEE ENGINEERING PLANS FOR LOCATION AND DETAILS.
- ALL LAWN AREAS AND LANDSCAPE BEDS TO BE FULLY IRRIGATED WITH AN AUTOMATIC UNDERGROUND SYSTEM. IRRIGATION SYSTEM TO HAVE SEPARATE ZONES FOR LAWN AREAS, PARKING ISLANDS, AND SHRUB BEDS WITH DIFFERENT CONTROL. MOISTURE LEVEL ADJUSTMENT PER ZONE AS REQUIRED.
- UNLESS NOTED OTHERWISE, LANDSCAPE BEDS ADJACENT TO LAWN TO RECEIVE EDGINGS. EDGING SHALL BE 4" X 1/2" METAL (FINISH BLACK OR GREEN) OR APPROVED EQUAL AND TO BE INSTALLED WITH HORIZONTAL METAL STAKES AT 32" O.C. OR PER MANUFACTURER'S SPECIFICATION.

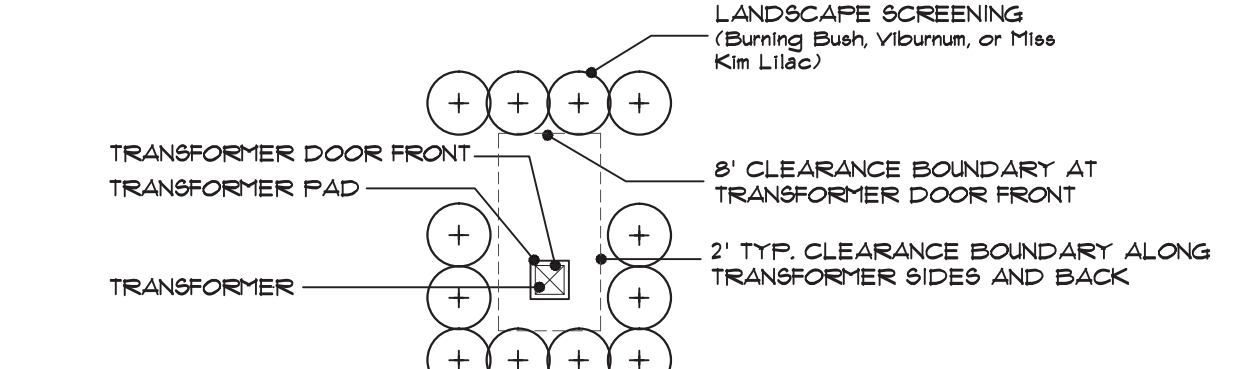
landscape maintenance notes:

- LANDSCAPE MAINTENANCE PROCEDURES AND FREQUENCIES TO BE FOLLOWED SHALL BE SPECIFIED ON THE LANDSCAPE PLAN. ALONG WITH THE MANNER IN WHICH THE EFFECTIVENESS, HEALTH AND INTENDED FUNCTIONS OF THE VARIOUS LANDSCAPE AREAS ON THE SITE WILL BE ENSURED.
- LANDSCAPING SHALL BE KEPT IN A NEAT, ORDERLY AND HEALTHY GROWING CONDITION, FREE FROM DEBRIS AND REFUSE.
 - PRUNING SHALL BE MINIMAL AT THE TIME OF INSTALLATION. ONLY TO REMOVE DEAD OR DISEASED BRANCHES. SUBSEQUENT PRUNING SHALL ASSURE PROPER MATURATION OF PLANTS TO ACHIEVE THEIR APPROVED PURPOSE.
 - ALL DEAD OR DISEASED PLANT MATERIAL SHALL BE REMOVED AND REPLACED WITHIN THREE (3) MONTHS AFTER IT DIES OR IN THE NEXT PLANTING SEASON, WHICHEVER OCCURS FIRST. THE PLANTING SEASON FOR DECIDUOUS PLANTS SHALL BE BETWEEN MARCH 15 AND NOVEMBER 15 OR UNTIL THE PREPARED SOIL BECOMES FROZEN. THE PLANTING SEASON FOR EVERGREEN PLANTS SHALL BE BETWEEN MARCH 1 AND JUNE 1. PLANT MATERIAL INSTALLED TO REPLACE DEAD OR DISEASED MATERIAL SHALL BE AS CLOSE AS PRACTICAL TO THE SIZE OF THE MATERIAL IT IS INTENDED TO REPLACE.
 - THE APPROVED LANDSCAPE PLAN SHALL BE CONSIDERED A PERMANENT RECORD AND INTEGRAL PART OF THE SITE PLAN APPROVAL. UNLESS OTHERWISE APPROVED IN ACCORDANCE WITH THE AFOREMENTIONED PROCEDURES, ANY REVISIONS TO, OR REMOVAL OF, PLANT MATERIALS WILL PLACE THE PARCEL IN NON-COMPLIANCE WITH THE APPROVED LANDSCAPE PLAN, AND SHALL BE VIEWED AS A VIOLATION OF THIS ORDINANCE AND THE AGREED UPON TERMS OF THE FINAL SITE PLAN APPROVAL.



cost estimate summary

TOTAL ESTIMATED PLANT MATERIALS COST	124280
TOTAL ESTIMATED IRRIGATION COST	14500
TOTAL ESTIMATED SOD COST (100 SQ.YD. X 16 PER SQ.YD. = 1600)	1600
TOTAL ESTIMATED MULCH COST (35 CU.YD. X 135 PER CU.YD. = 11225)	11225
TOTAL ESTIMATED LANDSCAPE COST	151605

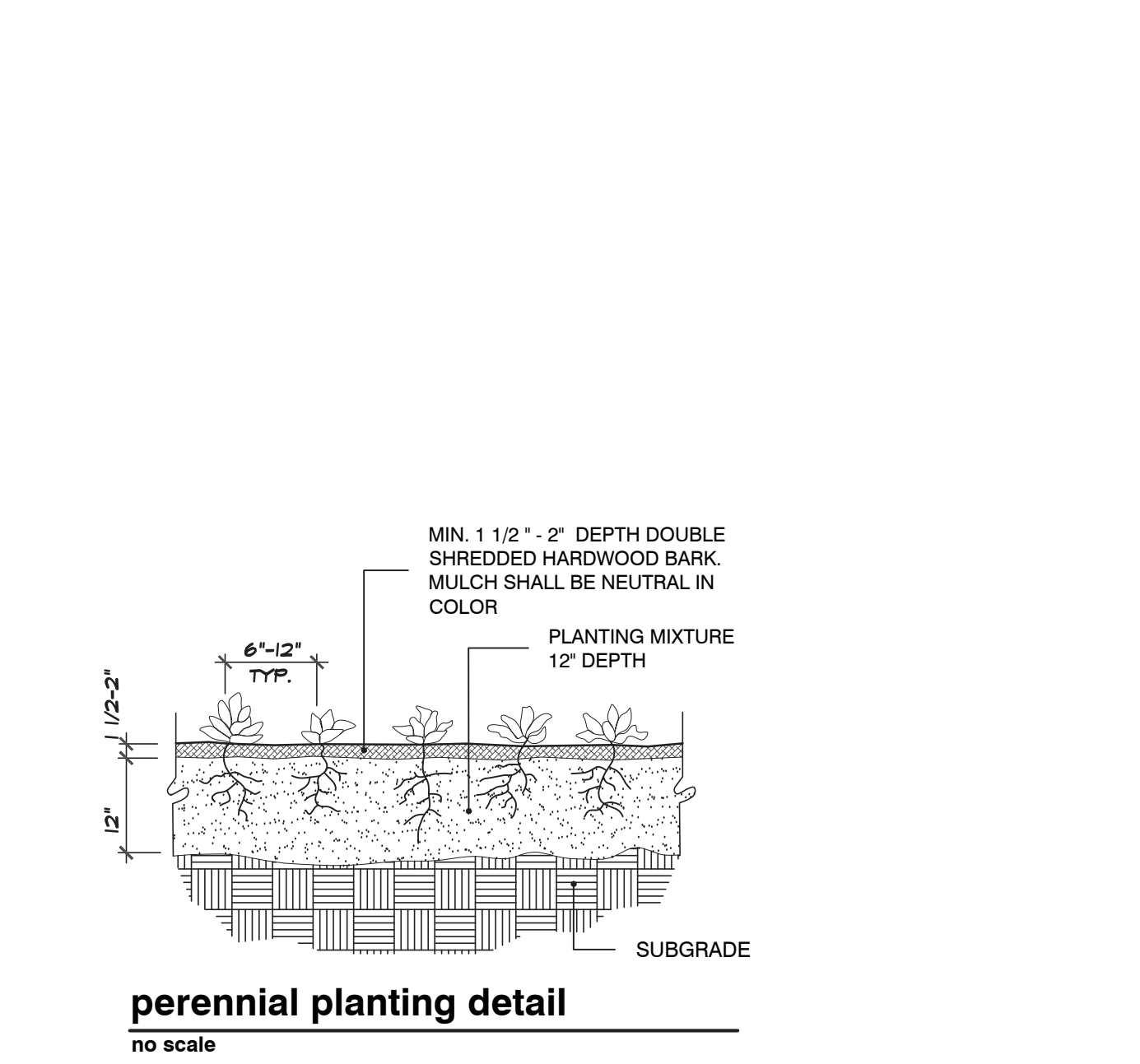
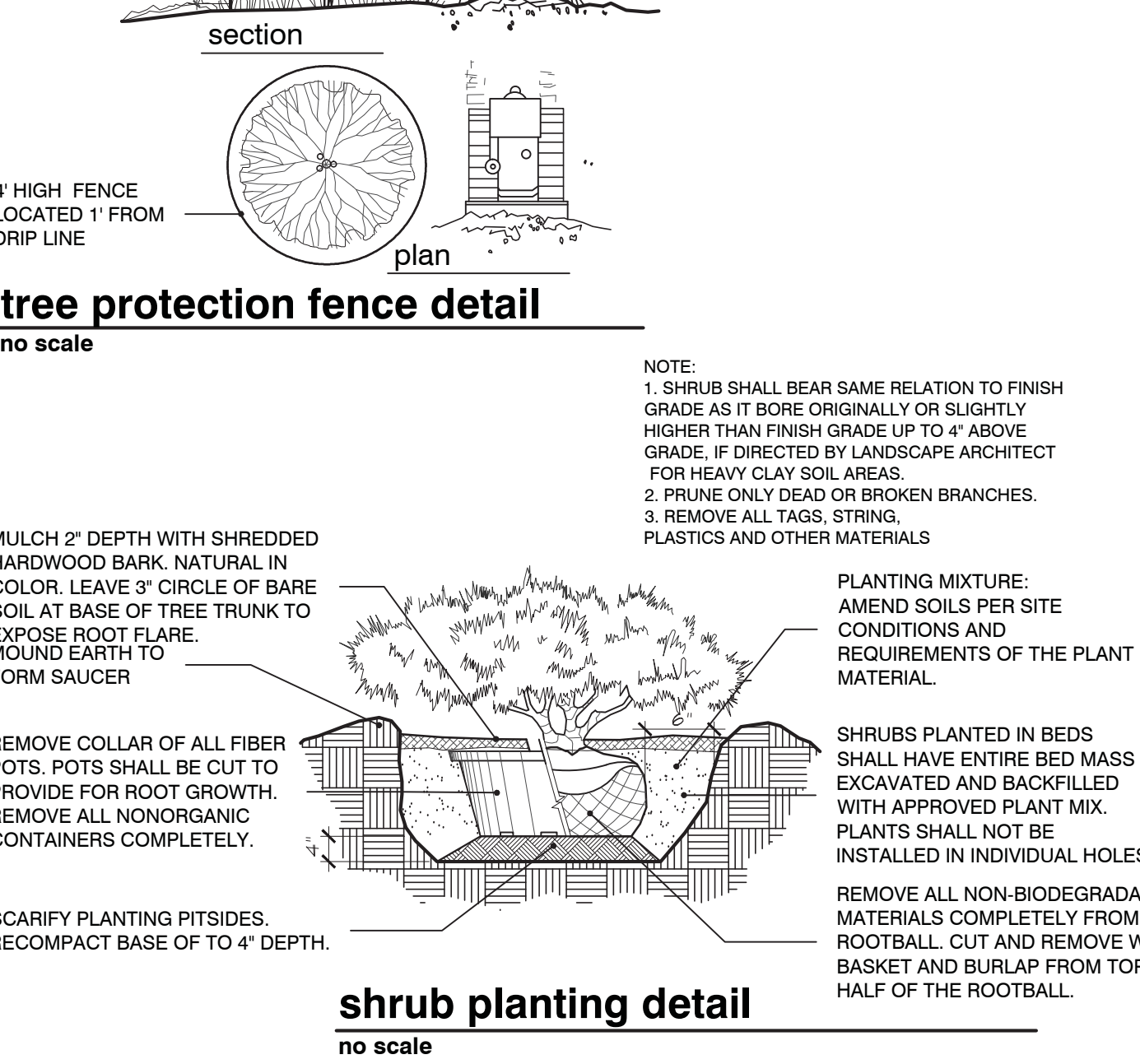
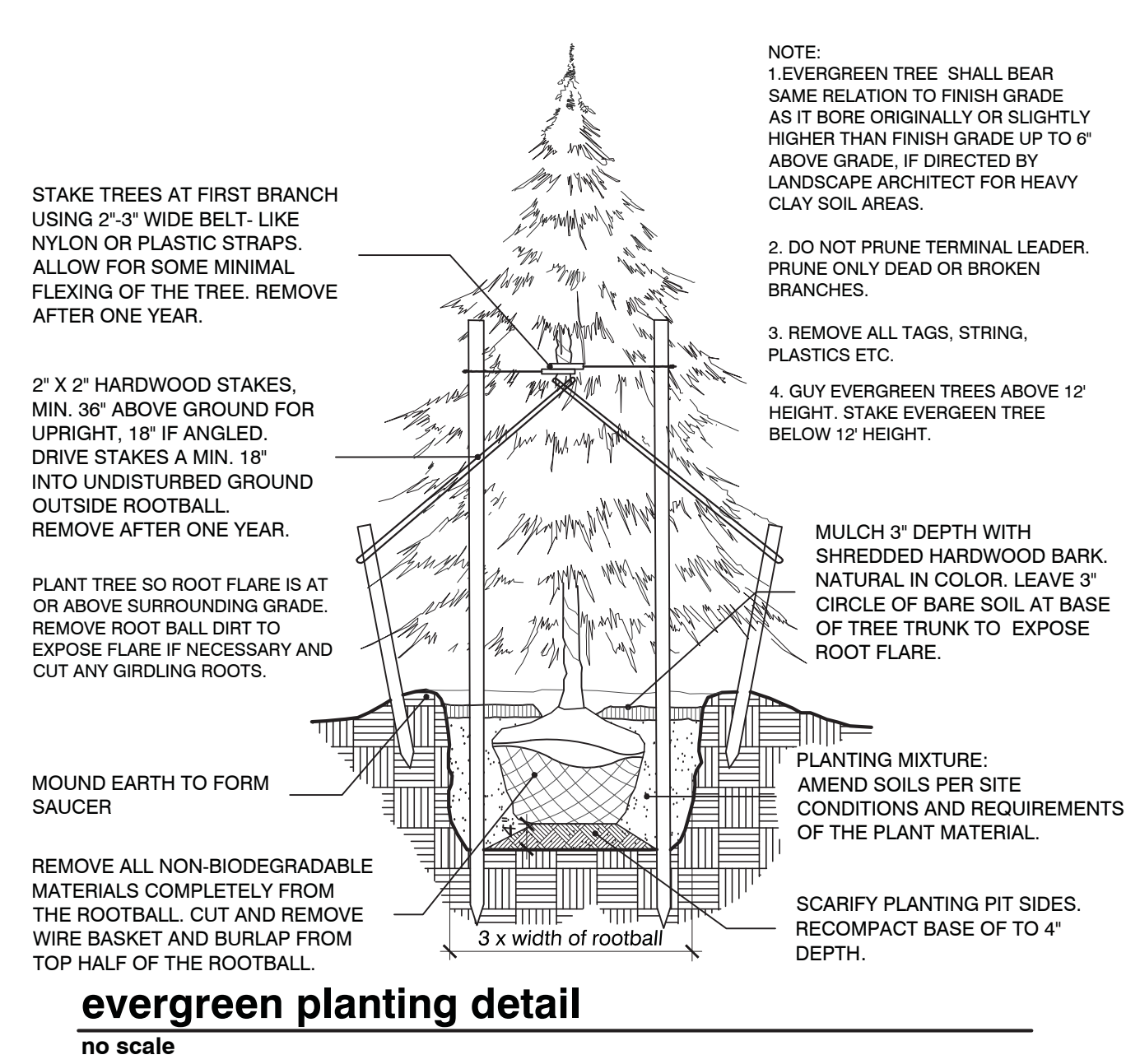
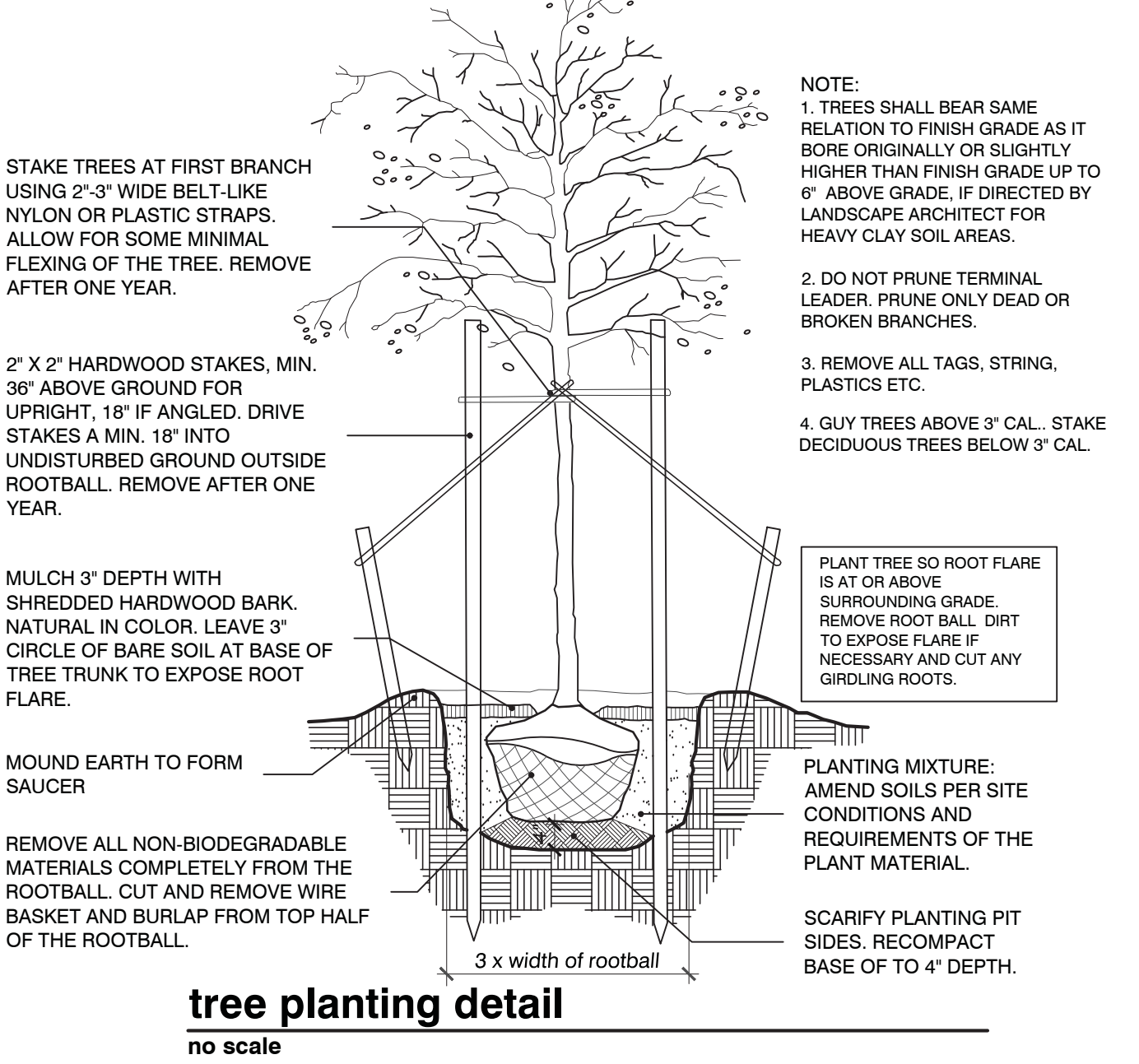


transformer pad planting detail

SCALE 1"=20'-0"

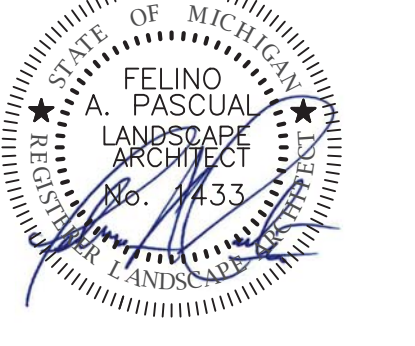
A MINIMUM OF 2' SEPARATION BETWEEN TRANSFORMER AND FULL GROWN SHRUBS AND TREES. GROUND COVERS ALLOWED UP TO TRANSFORMER PAD IF MAINTAINED BELOW 4" FULL GROWTH. IF TRANSFORMER FACES TOWARDS THE HOUSE, THEN THE SAME CONDITIONS EXIST, NO FULL GROWTH IN FRONT OF THE TRANSFORMER FOR AT LEAST A MINIMUM OF 8'. THERE ARE NO WAIVERS GRANTED TO THE ABOVE CONDITION. DETAIL PER THE DETROIT EDISON COMPANY-SERVICE PLANNING DEPARTMENT (9-11-98)

SEE ENGINEERING PLANS FOR PROPOSED LOCATIONS, TOTAL NUMBER OF TRANSFORMERS AND FINAL LOCATION PER DETROIT EDISON REQUIREMENTS.



Plant Material List, Planting Details and Notes

seal:



client:
ARIS BERRIS
 1702 EQUESTRIAN DRIVE, SOUTH LYON, MI 480178

project:

SLI MEDICAL BUILDING

project location:
 City of NOVI

GRAND RIVER AVE

sheet title:

plant material list and planting details

job no./issue/revision date:
 LS19.081.06 SPA 6-7-2019
 LS19.081.07 REVISION 7-22-2019
 LS19.081.07 SPA 7-29-2019
 LS19.081.10 SPA 10-14-2019

drawn by:
JP

checked by:
FP

date:
6-5-2019

notice:
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Do Not scale drawings. Use figured dimensions only



The location and elevations of existing underground utilities as shown on this drawing are only approximate; no guarantee is either expressed or implied as to the completeness of accuracy; contractor shall be exclusively responsible for determining the exact location and elevation prior to the start of construction

project no:
LS19.081.06

sheet no:
LS-2 of 2

APPLICANTS RESPONSE LETTER

October 15th, 2019

Sri Ravali Komaragiri
Novi City Planning & Zoning
45175 W 10 Mile Rd
Novi, MI 48375

Re: Plan Review Center Report
Planning Review, dated 10/1/19
SLI Medical Office
42350 Grand River Avenue
Novi, MI 48375
JSP 18-74

The engineering plans have been revised per your review comments dated October 1st, 2019. The landscape review comments will be provided by the Landscape Architect in a separate letter. Our responses to your comments are as follows:

Planning Review

1. The applicant shall provide a truck circulation diagram as noted in the Traffic review letter at the time of final site plan.

A fire truck turning template, a dumpster/garbage truck turning template, and a WB-40 truck turning template are shown on sheet 5 of the revised plans.

2. The applicants architect should indicate if additional handicap spaces would be required based on the use and try to accommodate those spaces at this time. The Building Department indicates that a minimum of 20% of parking required for physical therapy use should be barrier free.

Physical therapy accounts for 1, 150 square feet of the total proposed building area. Per the city ordinance, seven (7) parking spaces are required for this size area. 20% of those seven (7) spaces equates to two (2) ADA spaces.

The total parking area had 75 spaces. Per federal requirements, three (3) of those 75 spaces should be ADA spaces.

A total of five (5) ADA spaces are being proposed for this site. This includes the three (3) federally required ADA spaces and the two (2) additional ADA spaces for the physical therapy portion of the building.

To require 20% of the entire parking area to be ADA spaces is unreasonable given the relatively small portion of the building that is proposed for physical therapy use.

These calculations are show on sheet 2.

3. Please clarify if any additional site lighting or building lighting is proposed. If yes, a lighting and photometric plan as noted in the plan review chart is required at this time.

No new lighting is proposed for this site. Note has been added to plans.

Planning Review Chart

1. Parking setback screening – Screening is deficient.

See landscape review letter and plan by-others.

2. End Islands – Required some revisions.

Islands have been revised. See traffic review comments for more information.

3. Barrier Free Spaces – Two van accessible spaces to the north do not provide a safe and convenient access to building. Are additional barrier free spaces required for the type of medical use located in the building? Please clarify.

An ADA ramp has been proposed near these north ADA spaces.

See item 2 under Planning Review for ADA parking explanation.

4. Bicycle Parking Lot Layout – Layout not provided.

A bike rack detail has been added to sheet 5.

5. Exterior Lighting – A lighting plan is required if additional lighting is proposed.

No new lighting is proposed for this site. Note has been added to plans.

6. Roof top equipment and wall mounted utility equipment – The applicant should get a façade permit for rooftop equipment screening.

Applicant will apply and obtain for a façade permit for rooftop equipment screening if new rooftop equipment is deemed necessary. Note has been added to sheet 2.

7. General layout and dimension of proposed physical improvements – They do not provide dimensions for the parking improvements.

Overall improvement dimensions have been added to sheet 2.

8. Lighting and Photometric Plan – Submit a lighting plan or indicate if additional lighting is not proposed.

No new lighting is proposed for this site. Note has been added to plans.

Engineering Review

1. Revise the plan set to reference at least one City established benchmark.

A city established benchmark has been added to the plans.

2. Provide a note on the plans that all work shall conform to the current City of Novi standards and specifications.

A note has been added to the plans.

3. Provide the City's standard detail sheets for storm sewer (2 sheets, rev. 2.16.19) and paving (2 sheets, rev. 3.5.18) at the time of the stamping set submittal.

Detail sheets are attached.

4. The attached Non-Domestic User Survey form shall be submitted to the City so it can be forwarded to Oakland County.

The applicant will complete and submit the Non-domestic User Survey.

5. The dedication of the master-planned 60-foot half width right-of-way along Grand River Avenue is requested for the project.

The 60-foot half width right-of-way is proposed to be dedicated and shown as such on the plans.

6. Provide a note along with the traffic control sign table on sheet 5 stating all traffic signage will comply with the current MMUTCD standards.

Note has been added to sheet 5.

7. Provide a note stating if dewatering is anticipated or encountered during construction a dewatering plan must be submitted to the Engineering Division for review.

A note has been added to the plans.

8. Show all existing watermain/leads, sanitary sewer/leads and associated easement on the plans to ensure no permanent structures interfere with the utilities or easements.

Existing utilities and leads are shown on the plans. Title work has been requested from the applicant to determine existing easement locations.

9. Provide the watermain length, diameter, and material type where the hydrant is being relocated.

Proposed watermain information has been added to sheet 4.

10. Label the finished grade for the relocated hydrant.

Finished grade of the relocated hydrant is shown on sheet 4.

11. Provide a schedule listing the casting type and other relevant information for each proposed storm structure on the utility plan. Round castings shall be provided on all catch basins except curb inlet structures.

Since only two (2) storm structure are proposed, relevant information is listed at each structures callout on sheet 4. Casting information has been added.

12. Indicate the amount of pavement that is existing and the amount of pavement that is proposed. If the difference is determined by the engineering department to be negligible, then no additional storm water management requirements will be requested. If the additional amount of pavement is deemed to be significant increase in impervious surfaces then the site will be required to meet the City's current storm water management standards.

Existing and proposed pavement amounts are listed on sheet 4. All additional parking was part of the 1994 site plan approval conditions.

13. The city requires parking lots to be curbed as outlined in Section 11-239(b) of the design and construction standards. Provide curbs for the entire parking lot.

Proposed curbs have been added to the existing parking lot.

14. A sidewalk easement is requested for the portion of the sidewalk along Grand River Avenue that lies outside the right-of-way.

A sidewalk easement has been added to sheet 2.

15. Provide additional spot grades for the proposed sidewalk connection to the building.

Grades are shown on sheet 4.

16. The two proposed end islands must be 3 feet shorter than the adjacent 19-foot parking stalls.

Dimensions have been added to these areas on sheet 2.

17. Sheet 4 shall be signed and sealed by the design engineer responsible for the proposed retaining wall design and all associated calculations.

To be provided.

18. If the proposed retaining wall is within an existing easement, a license agreement will be required.

Title work has been requested from the applicant to determine existing easement locations. A license agreement will be obtained if necessary.

19. A note on sheet 4 indicates that the pavement on the north side of the site shall be extended 2 feet towards the swale. The current paving plan does not show this extension. Either remove the note or show this extension of paving on the plans.

Note has been removed.

Traffic Review

1. Truck turning movement patterns should be provided to show that the trash receptacle locations do not interfere with the use of the adjacent proposed parking spaces.

A fire truck turning template, a dumpster/garbage truck turning template, and a WB-40 truck turning template are shown on sheet 5 of the revised plans.

2. The applicant could also secure a shared parking agreement with the neighboring property in case of overflow in the future.

The applicant will obtain a shared parking agreement if necessary.

3. The barrier free parking requirement for a Physical therapy building should be applied to the parking for the building as a whole, not a subsection based on PT area.

To require 20% of the entire parking are to be ADA spaces is unreasonable given the relatively small portion of the building that is proposed for physical therapy use.

See item 2 under Planning Review for ADA parking explanation.

4. Note that all end islands adjacent to the travel way shall be constructed three (3) feet shorter than the adjacent parking space per Section 5.2.12 of the city's zoning ordinance. The island that is interior to the parking bay on the west side of the site may be extended to be flush with the parking spaces on either side of it. The island with the sidewalk on the east side of the parking lot next to the loading zone may remain at its current length as the island is more than three (3) feet shorter than the loading zone area adjacent to it.

The islands have been revised as indicated above. Additional dimensions have been added to sheet 2.

5. The applicant has indicated bicycle parking on the site plans. A detail of the layout has been provided, however, a bike rack has not been detailed and should be provided to ensure compliance with the city's zoning ordinance.

A bike rack detail has been added to sheet 5.

6. The bike rack must be a minimum of 36" tall.

A bike rack detail has been added to sheet 5.

7. Section 5.16.1.E of the city's zoning ordinance required that the bicycle parking be no more than 120 feet from the entrance being served or the parking space nearest that entrance. The applicant should dimension this distance to ensure that the bicycle parking location meets this requirement.

This dimension is show on sheet 2.

8. The applicant has indicated proposed sidewalk widths. The location of proposed ramps should also be included. The proposed barrier free parking spaces do not have a ramp at the first available sidewalk, according to the site plan. The

10/15/2019

Page 7 of 7

applicant should also consider providing a ramp at the sidewalk on the northwest side of the building.

An ADA ramp has been added to the first available sidewalk near these barrier free spaces. The existing sidewalk at the northwest side of the building is an existing ramp with handrails.

9. The applicant has provided a signing quantities table indicating the number of each proposed sign and its size. MMCTCD sign codes should be included as well.

MMUTCD codes and note have been added to sheet 5.

Please contact us if you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "Jeffrey S. Rizzo". The signature is written in a cursive, flowing style.

Jeffrey S. Rizzo, PE
Fenn & Associates

October 14, 2019

Mr. Rick Meader, Landscape Architect
City of Novi Community Development
45175 West Ten Mile Road
Novi, MI 48375

RE: SLI Medical Building Landscape Review, Job # jsp18-0074

Dear Mr. Meader:

In response to the city landscape review comments dated September 17, 2019, please see our revised landscape plans for Manchester, Job No. LS18.081.10, dated 10-14-2018, sheets LS-1 through LS-2 for details. We offer the following comments below.

“LANDSCAPE WAIVERS GRANTED BY THE PLANNING COMMISSION ON JUNE 12, 2019”, has been copied to sheet LS-1.

Ordinance Considerations

Existing Soils (Preliminary Site Plan checklist #10, #17)

1.(Noted) Provided

Existing and proposed overhead and underground utilities, including hydrants.(LDM

2.e.(4))

1.(Noted) Provided

Existing Trees (Sec 37 Woodland Protection, Preliminary Site Plan checklist #17 and LDM 2.3 (2))

1. 1-7” dbh Purple Beech tree is proposed to be removed due to the required pedestrian walk connection. See site survey for location

Adjacent to Residential - Buffer (Zoning Sec. 5.5.3.B.ii and iii)

1.(Noted) Property is not adjacent to Residential.

Right-of-Way Landscape Screening (Zoning Sec. 5.5.3.B)

1. Clear vision zone at Grand River per OCRC standards
2. (Noted) Calculations and all required trees are provided.
3. (Noted) A landscape waiver was granted by the Planning Commission was granted to not plant the required street trees as there is insufficient room in the right-of-way for them.

Parking Lot Interior (Zoning Sec 5.5.3.C)

1. (Noted) Based on the vehicular use area, 2,438sf of landscape area and 12 interior trees are required. 2,943sf of area and 12 trees are proposed.
2. Move the tree located in the small greenspace near the transformer to a position in front of the transformer



Parking Lot Perimeter (Zoning Sec 5.5.3.C)

1. Updated parking lot perimeter to 680lf. Based on this, the calculation has been updated to 19 trees required and 11 provided.
2. (Noted) A waiver was granted due to the lack of room on the site to provide all of the required trees.

Building Foundation Landscape (Zoning Sec 5.5.3.D.)

1. Revised the net building perimeter to 601lf, and 4,808sf required. Updated calculations accordingly.
2. (Noted) A landscape waiver was granted to not have 75% of the building foundation landscaped and to locate some of the foundation area away from the base of the building, on the front berm.
3. All foundation landscape areas are in SF.
4. Added additional landscaping (shrubs, perennials, grasses, annuals, ornamentals, etc) to meet the required foundation landscape area.

Loading Zone screening (Zoning Sec. 3.14, 3.15, 4.55, 4.56, 5.5)

(Noted)

Plant List (LDM 2.h. and t., 4)

1. Added more native species to reach at least 50% native species.
2. Replaced the privet with viburnums.
3. Revise the cost of ornamentals to \$250 each and grasses to \$15 each.

Planting Notations and Details (LDM)

1. Revise General Landscape Note #5 to add "in writing" after "approved".
2. Note#21 (LS-2), added a note stating that the landscaping must be guaranteed for 2 years after city approval
3. Revised Maintenance Note #3 to read 3 months, not 6 months.

Storm Basin Landscape (Zoning Sec 5.5.3.E.iv and LDM 1.d.(3))

1. Replaced 1-dogwood species for Michigan holly

Irrigation (LDM 1.a.(1)(e) and 2.s)

1. Irrigation plan will be provided during final stamping set.

Snow Deposit (LDM.2.q.)

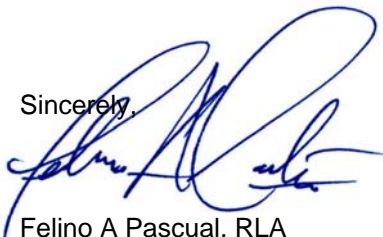
Provided

Corner Clearance (Zoning Sec 5.9)

1. Clear vision zone at Grand River has been shown and no proposed trees or shrubs are located within that zone.

Please do not hesitate to contact me should you have any questions or comments.

Sincerely,



Felino A Pascual, RLA





KEFORD COLLISION AND TOWING JSP 18-31

KEFORD COLLISION AND TOWING JSP 18-31

Public hearing at the request of Keford Collision & Towing for Planning Commission's approval of the Preliminary site plan, Wetland permit, and Storm water management plan. The subject property is currently zoned I-2 (general industrial) with an associated planned rezoning overlay (PRO) agreement. The subject property is approximately 7.61 acres and is located on the south side of Grand River Avenue between Taft Road and Novi Road (Section 15). The applicant proposes to use the existing larger building for an auto body collision repair shop and related offices, along with an accessory use of car rental services. No particular subtenants for the second existing building have been identified yet. The plan proposes an enclosed yard of up to 160 spaces in the rear yard for storage of towed vehicles.

REQUIRED ACTION

Postpone the public hearing for approval/deny of the Preliminary Site Plan, Wetland Permit, and Stormwater Management Plan.

REVIEW	RESULT	DATE	COMMENTS
Planning	Approval recommended	10-07-19	<ul style="list-style-type: none"> • A Planning Commission approval is required for any proposed use for the smaller building, when the use is determined. • Items to be addressed on the Final Site Plan submittal.
Engineering	Approval recommended	10-10-19	<ul style="list-style-type: none"> • Items to be addressed on the Final Site Plan submittal.
Landscaping	Approval recommended	9-30-19	<ul style="list-style-type: none"> • Items to be addressed on the Final Site Plan submittal.
Wetlands	Approval recommended	10-07-19	<ul style="list-style-type: none"> • Items to be addressed on the final site plan submittal.
Traffic	Approval recommended	10-09-19	<ul style="list-style-type: none"> • 20 feet clear fire land should be maintained within outside storage yard. • Items to be addressed on the Final Site Plan submittal.
Façade	Approval recommended	10-17-19	<ul style="list-style-type: none"> • No additional comments
Fire	Approval recommended	09-24-19	<ul style="list-style-type: none"> • Items to be addressed on the Final Site Plan submittal.

MOTION SHEET

Postpone – Preliminary Site Plan, Wetland Permit and Stormwater Management Plan

In the matter of Keford Collision and Towing JSP 18-31, motion to **postpone** the Preliminary Site Plan, Wetland Permit and Storm water Management Plan based on and subject to the following:

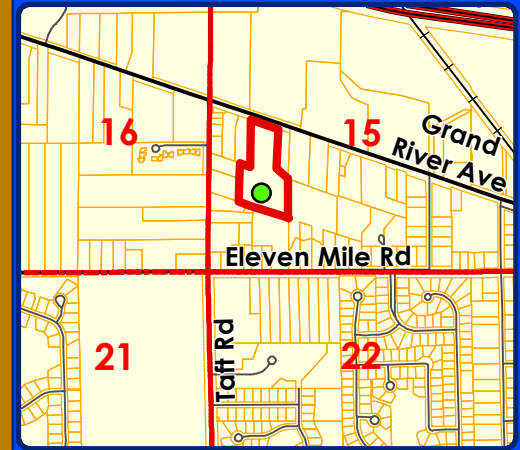
- a. To give staff additional time to properly advertise for the public hearing;
- b. *(additional conditions here if any)*

(This motion is made because the plan is otherwise in compliance with Article 3, Article 4 and Article 5 of the Zoning Ordinance and all other applicable provisions of the Ordinance.)

MAPS
Location
Zoning
Future Land Use
Natural Features

JSP 18-31 KEFORD COLLISION AND TOWING

Location



LEGEND

 Sections



City of Novi

Dept. of Community Development
City Hall / Civic Center
45175 W Ten Mile Rd
Novi, MI 48375
cityofnovi.org

Map Author: Sri Komaragiri
Date: 10/25/19
Version #: 1



1 inch = 164 feet

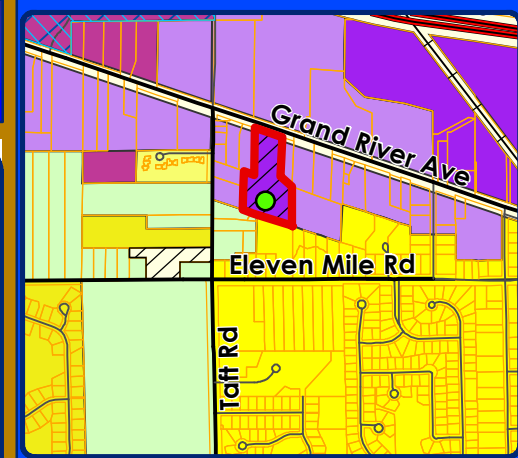
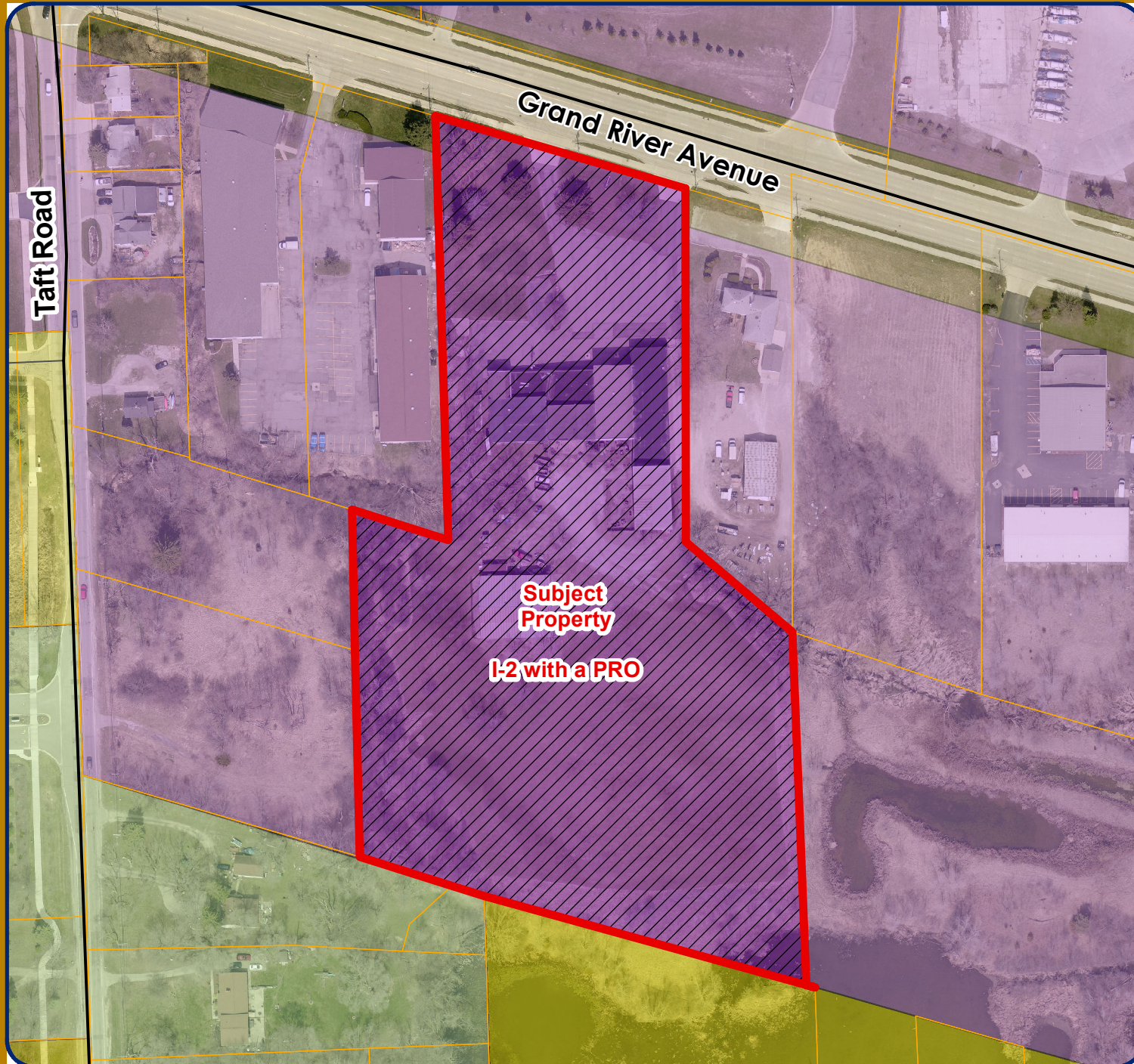


MAP INTERPRETATION NOTICE

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JSP 18-31 KEFORD COLLISION AND TOWING











Zoning

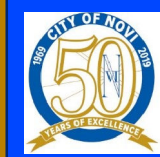


LEGEND

Zoning Overlay

Overlay Name

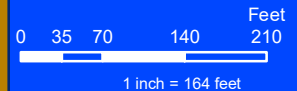
-  Exposition (EXO)
-  Planned Rezoning (PRO)
-  R-A: Residential Acreage
-  R-1: One-Family Residential District
-  R-2: One-Family Residential
-  R-4: One-Family Residential District
-  EXO: OST District with EXO Overlay
-  I-1: Light Industrial District
-  I-2: General Industrial District
-  OST: Office Service Technology



City of Novi

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 City Hall / Civic Center
 45175 W Ten Mile Rd
 Novi, MI 48375
cityofnovi.org

Map Author: Sri Komaragiri
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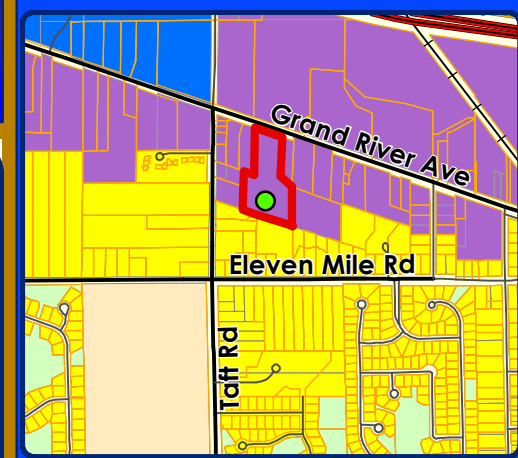
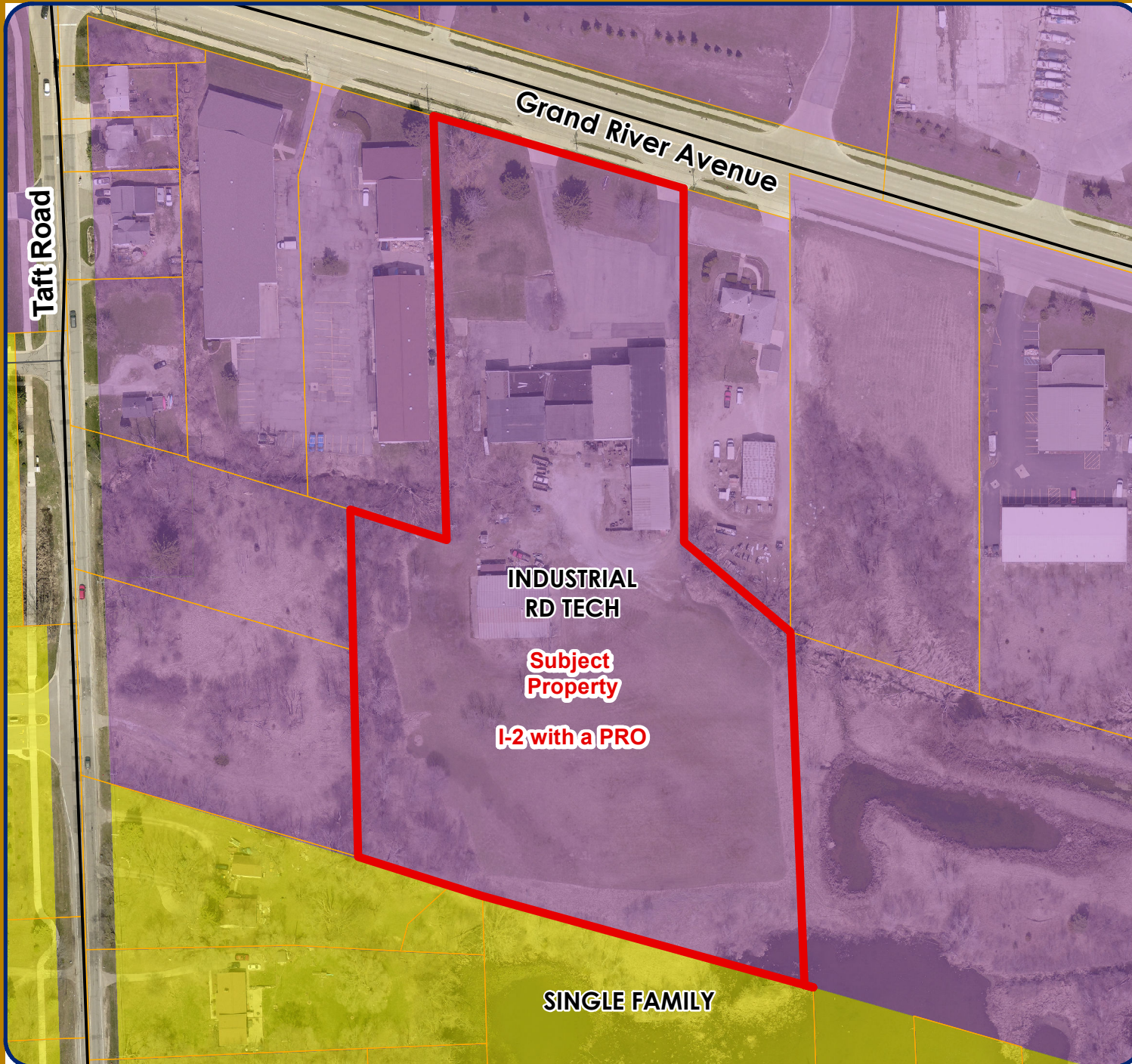


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JSP 18-31 KEFORD COLLISION AND TOWING

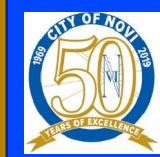
Future Landuse



LEGEND

FUTURE LAND USE

- Single Family
- Office RD Tech
- Industrial RD Tech
- Regional Commercial
- Educational Facility
- Private Park



City of Novi

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 Novi, MI 48375
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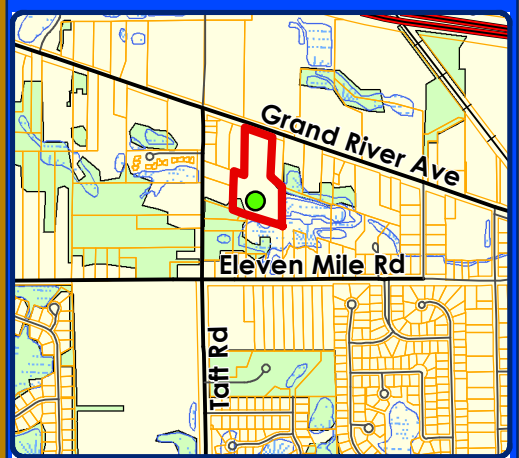


MAP INTERPRETATION NOTICE

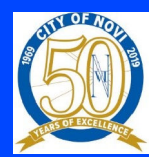
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JSP 18-31 KEFORD COLLISION AND TOWING

Natural Features

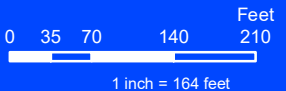


- LEGEND**
- WETLANDS
 - WOODLANDS



City of Novi
Dept. of Community Development
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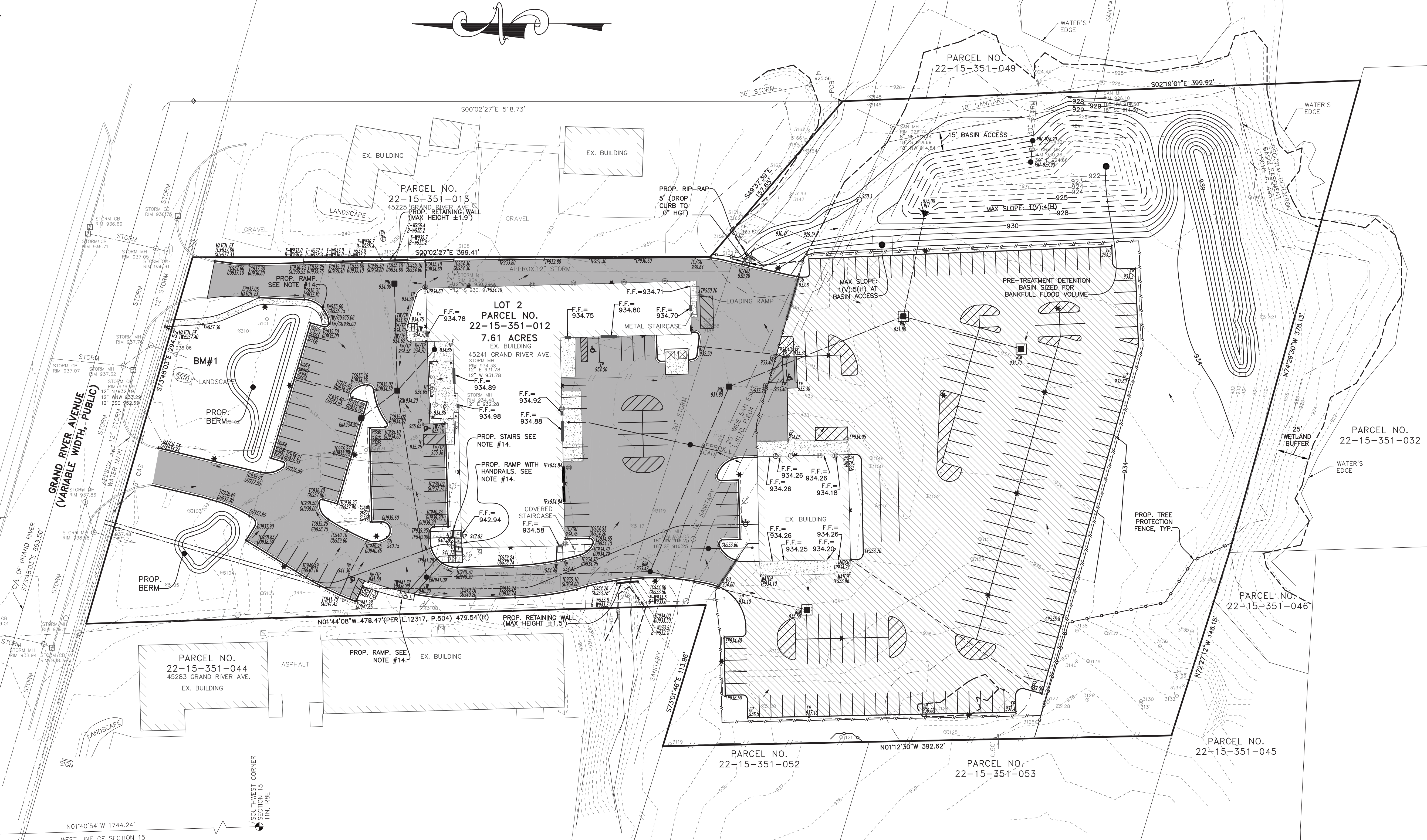
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SITE PLAN

(Full plan set available for viewing at the Community Development Department.)

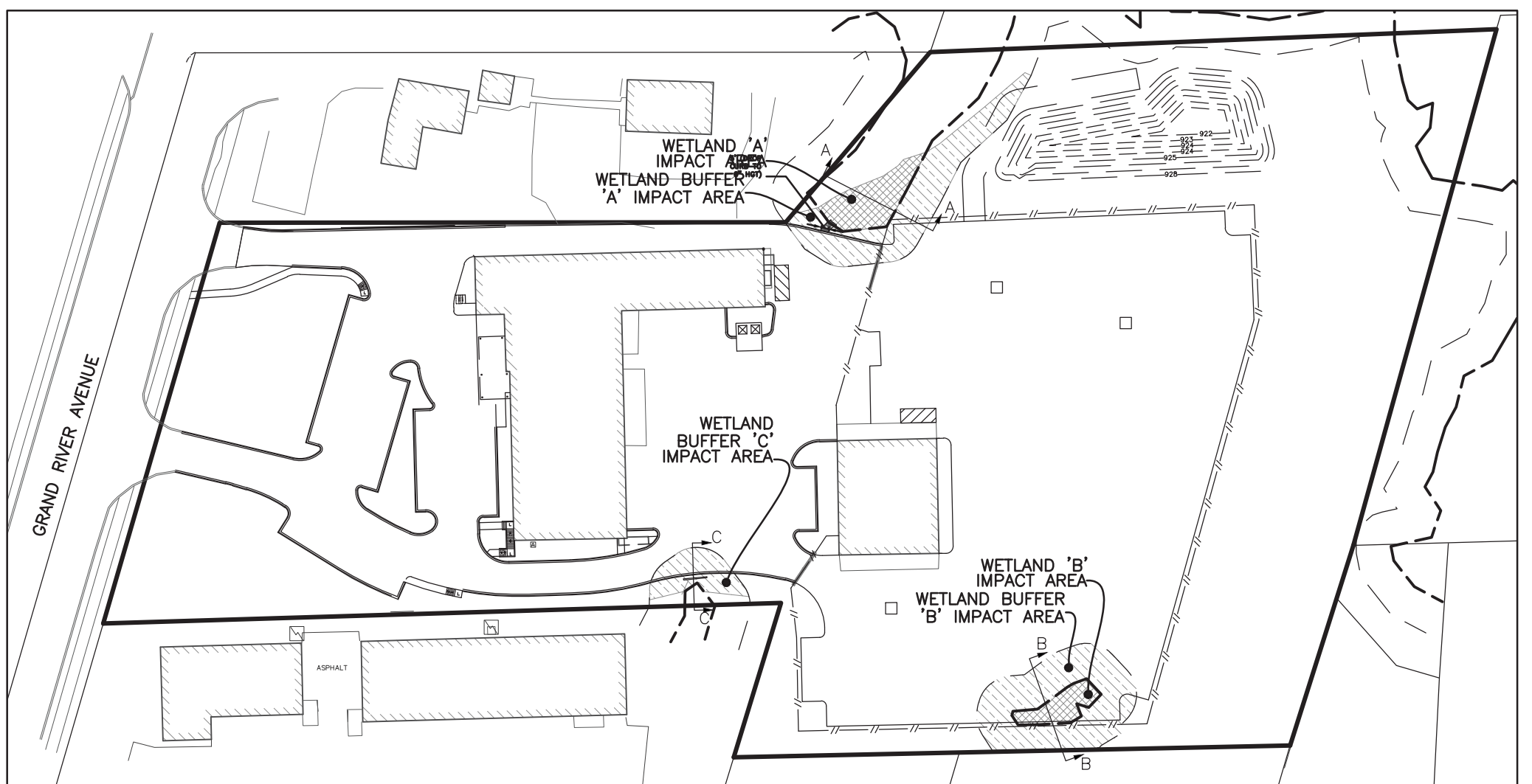
LEGEND:

- EX. CATCH BASIN
- EX. STORM MANHOLE
- EX. SANITARY MANHOLE
- EX. CLEANOUT
- EX. WATER GATE VALVE
- EX. COMMUNICATIONS MANHOLE
- EX. HYDRANT
- EX. WATER VALVE
- EX. WATER SHUTOFF
- EX. GAS SHUTOFF
- EX. GAS VENT
- EX. ELECTRIC MANHOLE
- EX. HANDHOLE
- EX. PEDESTAL
- EX. TRANSFORMER
- EX. LIGHTPOLE
- EX. UTILITY POLE
- EX. GUY ANCHOR
- EX. COMMUNICATION MANHOLE
- EX. SIGN
- EX. POST/BOLLARD
- EX. TRAFFIC SIGNAL
- EX. FENCE
- EX. SOIL BORING
- PROP. FENCE
- PROP. FINISH FLOOR ELEVATION
- PROP. LIGHT POLE
- PROP. STORM SEWER
- PROP. CURB & GUTTER
- PROP. SANITARY SEWER
- PROP. WATER MAIN
- PROP. STRUCTURE
- PROP. END SECTION
- PROP. CLEAN-OUT
- PROP. HYDRANT
- PROP. GATE VALVE
- PROP. CURB BOX
- PROP. GUTTER ELEV.
- PROP. TOP OF CURB ELEV.
- PROP. TOP OF WALK ELEV.
- PROP. TOP OF PAVEMENT ELEV.
- PROP. SPOT ELEV.
- PROP. DRAINAGE ARROW
- PROP. SILT FENCE
- PROP. TREE PROTECTION FENCE
- PROP. INLET FILTER
- PROP. ASPHALT
- PROP. CONCRETE
- PROP. GRAVEL

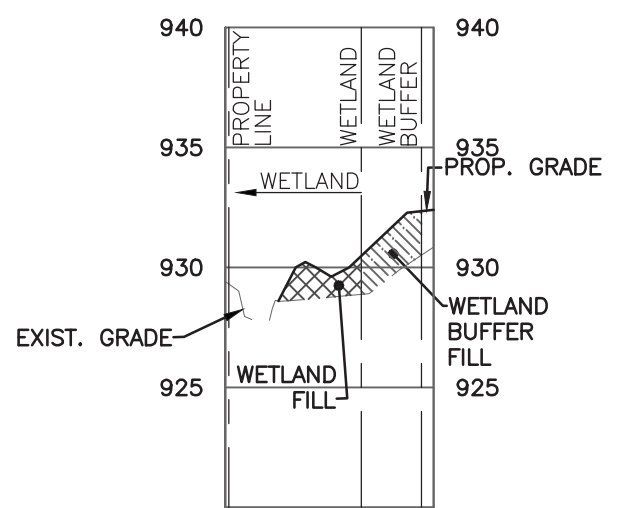


NOTICE:
CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OF PERSONS ENGAGED IN THE WORK, OF ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS.

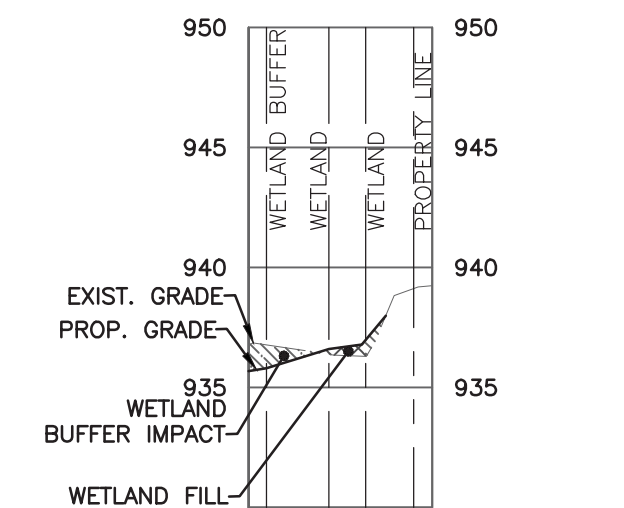
NOTE:
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AS DISCLOSED BY AVAILABLE UTILITY COMPANY RECORDS AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE COMPANY. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY IF A CONFLICT IS APPARENT.



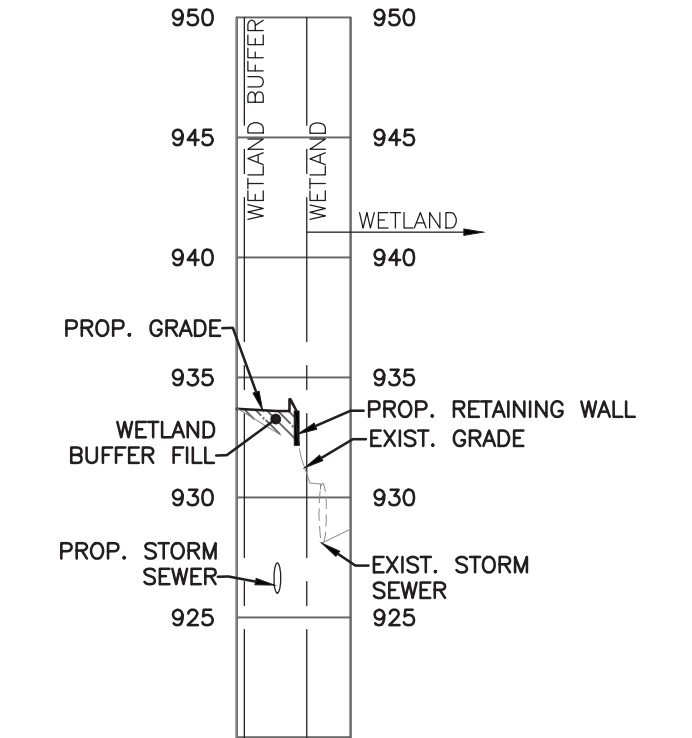
WETLAND IMPACT:
SCALE: 1" = 100 FEET



CROSS-SECTION A-A
SCALE: 1" = 80'(H)
1" = 8'(V)



WETLAND CROSS-SECTION B-B
SCALE: 1" = 80'(H)
1" = 8'(V)



CROSS-SECTION C-C
SCALE: 1" = 80'(H)
1" = 8'(V)

LEGEND:

- WETLAND 'A'
EXIST. AREA: 7,026 SQ.FT.
PROP. IMPACT AREA: 0.04 ACRES
1,841 SQ. FT.
PROP. VOLUME: ±120 CU. YD. (FILL)
- WETLAND 'B'
EXIST. AREA: 973 SQ.FT.
PROP. IMPACT AREA: 0.02 ACRES
1,841 SQ. FT.
PROP. VOLUME: ±15 CU. YD. (FILL)
- WETLAND 'C'
EXIST. AREA: 207 SQ.FT.
PROP. IMPACT AREA: 0.00 ACRES
0 SQ. FT.
PROP. VOLUME: ±0 CU. YD. (FILL)
- WETLAND BUFFER 'A' IMPACT AREA
EXIST. AREA: 6,194 SQ.FT.
PROP. IMPACT AREA: 0.11 ACRES
4,987 SQ. FT.
PROP. VOLUME: ±200 CU. YD. (FILL)
- WETLAND BUFFER 'B' IMPACT AREA
EXIST. AREA: 5,464 SQ.FT.
PROP. IMPACT AREA: 0.13 ACRES
5,464 SQ. FT.
PROP. VOLUME: ±20 CU. YD. (CUT)
- WETLAND BUFFER 'C' IMPACT AREA
EXIST. AREA: 1,992 SQ.FT.
PROP. IMPACT AREA: 0.04 ACRES
1,998 SQ. FT.
PROP. VOLUME: ±40 CU. YD. (FILL)

- NOTES:**
1. PLAN SET FOR PRELIMINARY SITE PLAN SUBMITTAL ONLY.
 2. ALL WORK SHALL CONFORM TO THE CURRENT CITY OF NOVI STANDARDS AND SPECIFICATIONS.
 3. REFER TO THE "PRELIMINARY STORM WATER MAINTENANCE PLAN" FOR INFORMATION REGARDING PROPOSED STORM WATER PRE-TREATMENT AND DETENTION.
 4. RIGHT OF WAY PERMIT IS REQUIRED FROM THE CITY OF NOVI FOR ANY WORK IN THE GRAND RIVER AVENUE RIGHT-OF-WAY.
 5. NOTIFY THE CITY OF NOVI A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
 6. CALL MISS DIG (811) A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
 7. PRIOR TO ISSUANCE OF AN OCCUPANCY PERMIT, ENGINEERING SITE INSPECTION IS REQUIRED.
 8. CONTRACTOR TO FIELD VERIFY LOCATION, DEPTH, AND SIZE OF ALL EXISTING UTILITIES PRIOR TO THE START OF CONSTRUCTION.
 9. THE CONTRACTOR IS RESPONSIBLE FOR ALL DAMAGE TO EXISTING UTILITIES, CURB, SIDEWALK, ETC.
 10. ALL SOIL EROSION AND SILT MUST BE CONTROLLED AND CONTAINED ON-SITE.
 11. ALL EXCAVATION UNDER OR WITHIN A 1 ON 1 INFLUENCE OF PUBLIC PAVEMENT, EXISTING OR PROPOSED, SHALL BE BACKFILLED AND COMPACTED WITH SAND (CLASS II MDDT).
 12. IF DEWATERING IS ANTICIPATED OR ENCOUNTERED DURING CONSTRUCTION THE CONTRACTOR IS REQUIRED TO SUBMIT A DEWATERING PLAN TO THE CITY ENGINEERING DIVISION FOR REVIEW.
 13. ALL RIP-RAP IS REQUIRED TO BE GROUNDED.
 14. SIDEWALK RAMPS TO COMPLY WITH ADA REQUIREMENTS. REFER TO MDDT DETAIL 28-J. PROVIDE HANDRAILS MEETING CITY/STATE BUILDING CODES AS NECESSARY AT RAMPS AND STAIRS.

COMMERCIAL
SITE PLANNING
SITE ENGINEERING
INDUSTRIAL & MULTI-UNIT
LAND SURVEYING
CONSTRUCTION LAYOUT

SURVEYING
ALTA SURVEYS
BOUNDARY SURVEYS
TOPOGRAPHIC SURVEYS
PARCEL SPLITS

RESIDENTIAL
SUBDIVISIONS
SITE CONDOMINIUM
MULTI-FAMILY
LOT PLANS
CONSTRUCTION LAYOUT

ALPINE ENGINEERING INC.
CIVIL ENGINEERS & LAND SURVEYORS

46892 WEST ROAD
SUITE 109
NOVI, MICHIGAN 48377

(248) 926-3701 (BUS)
(248) 926-3765 (FAX)
WWW.ALPINE-INC.NET

811
Know what's below
Call before you dig.

CLIENT: KEFORD COLLISION & TOWING

PRELIMINARY GRADING PLAN

SECTION: 15
45241 GRAND RIVER AVENUE
RANGE: 8 E
TOWNSHIP: 1 N
CITY OF NOVI
OAKLAND COUNTY
MICHIGAN

REVISED

2019-09-12 PSP SUBMITTAL
2019-07-17 PRO RESUBMITTAL
2018-08-29 PRO RESUBMITTAL
2018-07-12 PRO CONCEPT PLAN SUBMITTAL
2018-05-22 REVISE PER CLIENT
2018-05-18 PRE-APPLICATION SUBMITTAL

DATE: 2018-02-02

DRAWN BY: SD

CHECKED BY: TG/SD

FBK:

CHF:

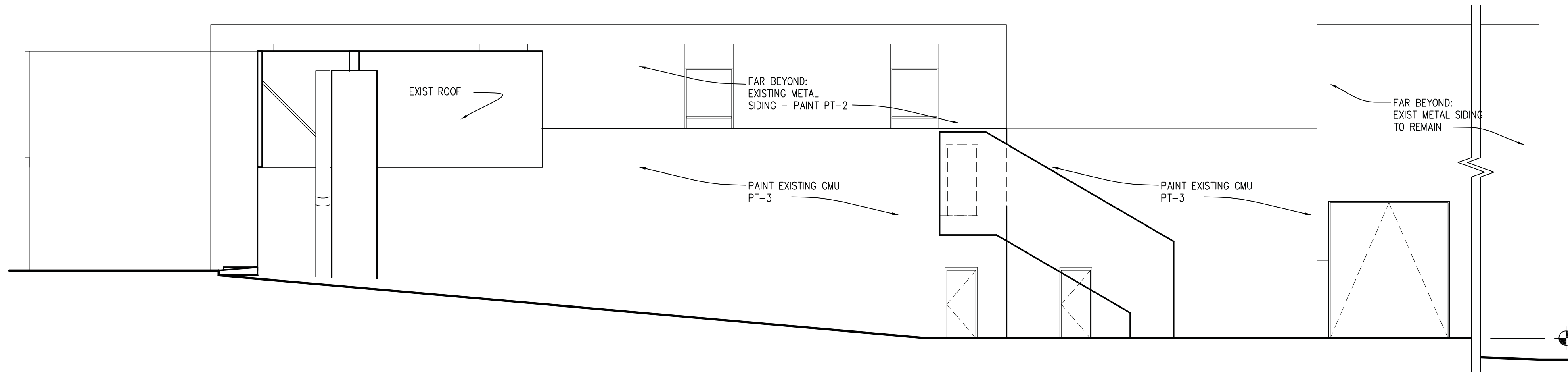
SCALE: HOR 1" = 40 FT.
VER 1" = 8 FT.

3

17-504

NOT FOR CONSTRUCTION

1/16" = 1'-0"



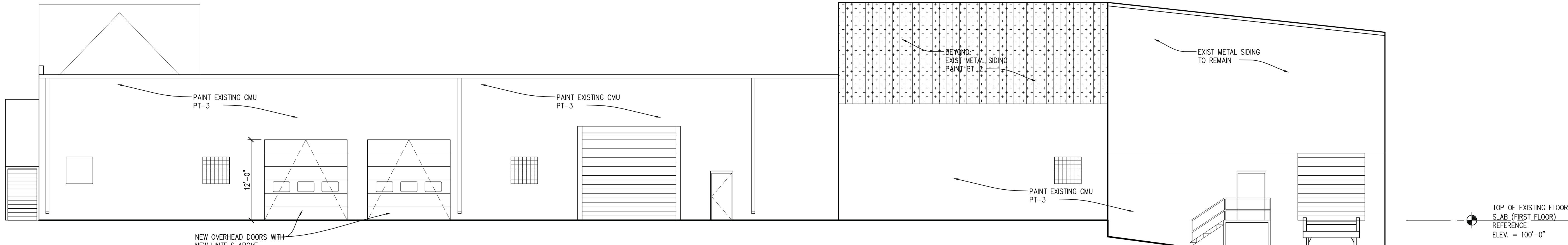
4 SOUTH ELEVATION

1/8" = 1'-0"

KEY NOTES	
SYMBOL	DESCRIPTION
S-1	SIGNAGE BY OWNER - G.C. TO COORDINATE EXACT SIZE, LOCATION, POWER AND MOUNTING REQUIREMENTS WITH SIGNAGE SUPPLIER THRU OWNER. ALL SIGNAGE TO BE PERMITTED SEPARATELY.

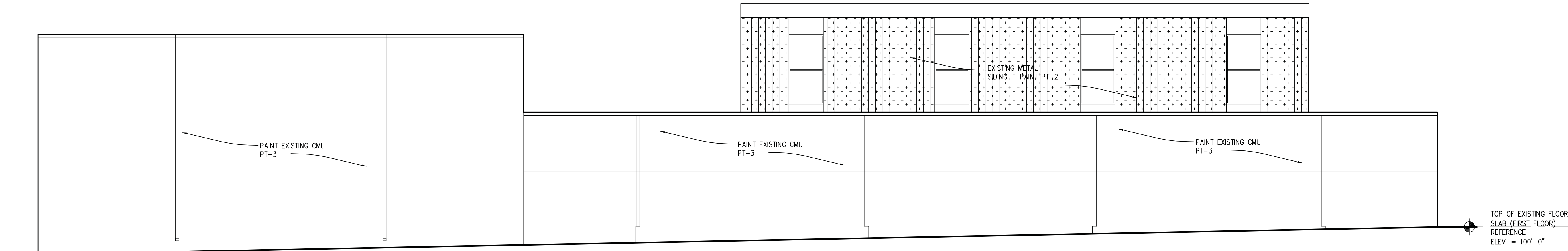
IMPORTANT NOTE:
NO RTU EXIST ON EXISTING ROOFTOP, AND THERE WILL BE NO NEW ROOFTOP UNITS PROPOSED AS PART OF THIS FUTURE RENOVATION

EXTERIOR FINISH MATERIALS SCHEDULE		SPECIFICATION SECTION
SYMBOL	DESCRIPTION	
CMP-1	1/2" CORRUGATED METAL PANEL WITH EXPOSED FASTENERS MFR: PAC CLAD COLOR: PREFINISHED TO MATCH PT-1	
CMP-2	CORRUGATED METAL PANEL WITH EXPOSED FASTENERS PROFILE TO MATCH EXISTING ADJACENT METAL PANEL MFR: PAC CLAD COLOR: PREFINISHED TO MATCH PT-2	
PT-1	PAINT: BENJAMIN MOORE POPPY 1315 THIS DESIGNATION REFERS TO COLOR AND SHEEN ONLY. REFER TO SPECIFICATION FOR PAINT TYPE AND ADDITIONAL INFORMATION.	09 9100
PT-2	PAINT: SHERWIN WILLIAMS CITYSCAPE SW7067 THIS DESIGNATION REFERS TO COLOR AND SHEEN ONLY. REFER TO SPECIFICATION FOR PAINT TYPE AND ADDITIONAL INFORMATION.	
PT-3	PAINT: BENJAMIN MOORE STORMY SKY 1616 THIS DESIGNATION REFERS TO COLOR AND SHEEN ONLY. REFER TO SPECIFICATION FOR PAINT TYPE AND ADDITIONAL INFORMATION.	



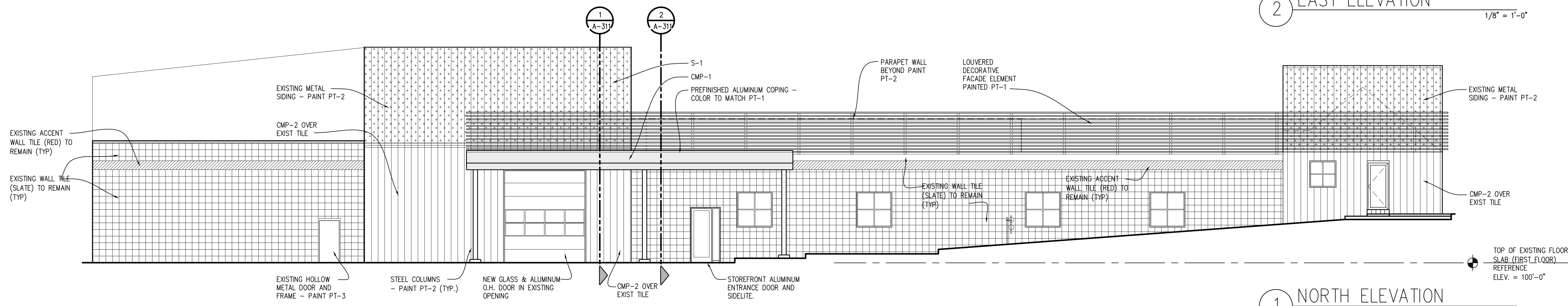
3 SOUTH ELEVATION

1/8" = 1'-0"



2 EAST ELEVATION

1/8" = 1'-0"



1 NORTH ELEVATION

1/8" = 1'-0"

CITYSCAPE

architects

design · planning · interiors

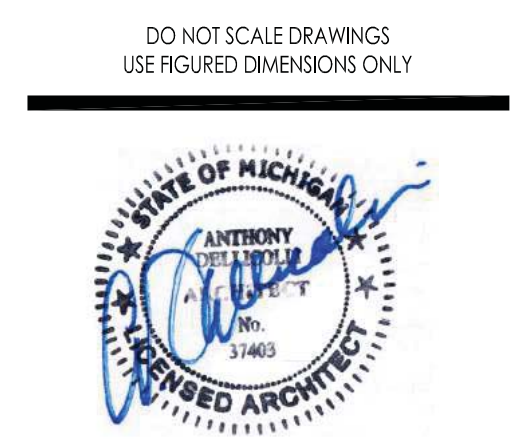
40850 Grand River Ave Suite 200
Novi, MI 48375
248 471 7877 ph
248 471 7868 fax

client
CARSTAR
Auto Body Repair Experts

notice
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COPYRIGHT YEAR 2019

project title
Carstar (Keford) Collision
45241 GRAND RIVER AVE
NOVI, MI 48375

sheet title
EXTERIOR ELEVATIONS
DO NOT SCALE DRAWINGS
USE FIGURED DIMENSIONS ONLY



project number
17062

drawn JW
approved TD

issued	date
DESIGN DEVELOPMENT	03-08-2019
OWNER UPDATES	06-10-2019
PRO	07-17-2019
PRELIM. SITE PLAN	09-13-2019
REV. SPA SUB.	10-17-2019

sheet number

A-211



Existing



Proposed

10/17/19

PLANNING REVIEW



PLAN REVIEW CENTER REPORT

October 07, 2019

Planning Review
 Keford Collision & Towing
 JSP 18-31

PETITIONER

Keford Collision & Towing

REVIEW TYPE

Preliminary Site Plan

PROPERTY CHARACTERISTICS

Section	15	
Site Location	South of Grand River Avenue and east of Taft Road	
Site School District	Novi School District	
Current Site Zoning	I-1 Light Industrial	
Proposed Site Zoning	I-2 General Industrial	
Adjoining Zoning	North	I-1 Light Industrial
	East	I-1 Light Industrial
	West	I-1 Light Industrial
	South	R-4 and RA: One Family Residential
Current Site Use	Vacant manufacturing facility	
Adjoining Uses	North	Corrigan Worldwide, Inc
	East	Construction Company
	West	Warehouse Supply Office and Vacant Lots
	South	City Regional Detention Pond and Single Family Homes
Site Size	7.61 Acres	
Plan Date	September 12, 2019	

PROJECT SUMMARY

The petitioner is requesting a Zoning Map amendment for 7.61acre property on the south side of Grand River Ave. between Taft Road and Novi Road (Section 15) from I-1 (Light Industrial) to I-2 (General Industrial). The subject property contains two existing buildings which are currently unoccupied. The applicant proposes to use the larger building (23,493 square feet) for an auto body collision repair shop and related offices, along with an accessory use of car rental services. The car rental service proposes to use up to a maximum of 10 parking spaces in the rear. The applicant states that the potential use for the out building (5,703 square feet) would be a small tool and die shop. No particular subtenants have been identified yet. In addition to the indoor uses, the applicant proposes to use up to 160 spaces in an enclosed yard in the rear yard for storage of towed vehicles.

RECOMMENDATION

Approval of the Preliminary Site Plan is recommended. The plan mostly conforms to the requirements of the Zoning Ordinance, with a few deviations that were approved by City Council as part of PRO Concept plan approval. **Planning Commission's approval of Preliminary Site Plan, Wetland Permit and Storm Water Management Plan approval is required.**

PRO OPTION

The PRO option creates a "floating district" with a conceptual plan attached to the rezoning of a parcel. As part of the PRO, the underlying zoning is proposed to be changed (in this case from I-1 to I-2) and the applicant enters into a PRO agreement with the City, whereby the applicant submits a conceptual plan for development of the site. The City Council reviews the Concept Plan, and if the plan may be acceptable, it directs for preparation of an agreement between the City and the applicant, which also requires City Council approval. Following final approval of the PRO concept plan and PRO agreement, the applicant will submit for Preliminary and Final Site Plan approval under standard site plan review procedures. The PRO runs with the land, so future owners, successors, or assignees are bound by the terms of the agreement, absent modification by the City of Novi. If the development has not begun within two (2) years, the rezoning and PRO concept plan expires and the agreement becomes void.

PROJECT REVIEW HISTORY FOR THE REZONING REQUEST

Action summaries from all the public meetings listed below are provided in the attachment.

- On June 11, 2018, a Pre-application meeting was held.
- On August 22, 2018, the plan was presented to Master Planning and Zoning Committee.
- On September 26, 2018, the Planning Commission considered the proposed development and made a favorable recommendation to Council.
- On November 13, 2018, the City Council tentatively approved the proposed zoning amendment.
- On June 03, 2019, the City Council approved the proposed PRO Concept plan and the agreement.

REVIEW COMMENTS

This project was reviewed for conformance with the Zoning Ordinance with respect to Article 3 (Zoning Districts), Article 4 (Use Standards), Article 5 (Site Standards), and any other applicable provisions of the Zoning Ordinance. **Please see the attached charts for information pertaining to ordinance requirements.** Items in **bold** below must be addressed and incorporated as part of the stamping set submittal.

1. **PRO Conditions and Deviations:** Please add the PRO Conditions and deviations listed in this letter on the site plan sheet for reference. Please refer to additional comments that need to be addressed to comply with the PRO Conditions listed in next section.
2. **Uses Permitted per approved PRO Agreement.** The Land as reclassified shall be used only as follows:
 - a. *The larger existing building (23,493 square feet) shall be used for an auto body collision repair shop and related offices, along with an accessory use of car rental services, which shall have use of no more than 10 parking spaces in the rear yard of the building.*
 - b. *The smaller existing out-building (5,703 square feet) shall be used only for a use specifically approved by the Planning Commission, in its reasonable discretion. **It appears that there is no proposed use or a prospective tenant for the out building. The applicant should provide an update. A Planning Commission approval is required for any proposed use. The applicant should contact Community Development department once a prospective tenant is determined***
 - c. *An enclosed yard in the rear yard for storage of towed vehicles of no more than 160 spaces. **Current plan shows a total of 158 spaces.***
3. **Designated Parking:** The plan should clearly label the 10 dedicated spaces for car rental, dedicated spaces for staging.
4. **Lighting and Photometric Plan:**

- a. **Building Lighting** (Sec. 5.7.2.A.iii): Relevant building elevation drawings showing all fixtures, the portions of the walls to be illuminated, luminance levels of walls and the aiming points of any remote fixtures. **Please provide the photometric information on building facades, in addition to the site photometric plan.**
- b. **Lighting Plan** (Sec.5.7.A.2): **Specification sheets for all proposed lighting are not provided. Please provide as required**
- c. **Security Lighting** (Sec. 5.7.3.H): **Indicate what lights will be turned on past hours of operation for security reasons. A separate photometric plan is required for security lights only**

MAJOR CONDITIONS OF PLANNED REZONING OVERLAY AGREEMENT

Some selected conditions that are part of draft PRO are included below. Please refer to the draft PRO agreement for other details. Staff comments are provided in **bold**.

1. *The applicant shall provide \$10,000 for use by the City of Novi Grand River Corridor Improvement Authority in a manner consistent with the Grand River Corridor Improvement Plan, as determined by the Authority in its discretion (by way of example only, funding the installation of sidewalks in certain "gap" areas along Grand River to improve mobility). Such amount shall be paid to the City immediately upon recording of this Agreement. **The applicant has paid the amount. This condition has been met.***
2. *Outside storage of vehicles in the enclosed rear yard shall be limited to 160 parking spaces only. **Current plan shows a total of 158 spaces.***
3. *The smaller existing out-building shall be used only for a use permitted in the I-2 District and specifically approved by the Planning Commission, in its sole discretion. Developer acknowledges that the PRO process is a voluntary undertaking on its part, and a discretionary determination by the City. Because Developer cannot identify any particular use of the building, the City cannot determine is the use of that building will be an appropriate use or an overall benefit to the public or will instead detract from the PRO Plan and primary use of the Land as described in this Agreement. Given the lack of a specified use, the Planning Commission shall have the authority to (a) approve or deny any proposed future use, and (b) review and approve or deny any improvements to the Land in connection with such use, in accordance with the terms and conditions of this Agreement. **It appears that there is no proposed use or a prospective tenant for the out building. The applicant should provide an update.***
4. *If the Developer, or a successor or company that acquires the Developer, ceases operation of the proposed outdoor vehicle storage operation at the site, then the City shall have the ability, without objection or challenge in any way by Developer, to rezone the land to its prior classification of I -1, Light Industrial. **It appears that there is name change to the business. It is unclear if the ownership changed as well. The applicant should clarify.***
5. *In consideration of the City's waiver of the ordinance requirement that the rear storage area be paved, Developer shall*
 - i. *Keep the access aisle as shown on the PRO plan free from parked vehicles or other obstructions so that there is fire truck access at all times (with the details of the Developer's plan to designate or demarcate the access aisle shown on the final approved site plan); **The plans do not show clear demarcation as required. Refer to Traffic review for more details.***
 - ii. *Undertake regular maintenance of the gravel storage area so as to prevent the migration of the gravel storage area to other areas of the Land or Development or adjacent properties or roadways or nearby waterbodies. To ensure compliance with these requirements, the City shall have a reasonable right to enter onto the Land or Development for purposes of inspection; provided, however, the City shall give 48*

hours' notice of any expected inspection. If the City determines that corrective action is required to be taken, it shall issue a notice of corrective action, which shall include a time period for correction. Failure to comply shall be treated as a breach of this Agreement.

ORDINANCE DEVIATIONS

The following deviations from the standards of the zoning ordinance are hereby authorized pursuant to §7.13.D.i.c (2) of the City's Zoning Ordinance.

1. *Planning deviation from Section 3.1.19.D for not meeting the minimum requirements for side yard setback for Parking (20 feet minimum required, 10.7 proposed in the northwest parking lot);*
2. *Landscape deviation from Section 5.5.3.A for not meeting the minimum requirements for a 10-15 foot tall landscaped berm or not providing the minimum required screening trees between residentially zoned property and industrial. A berm approximately 7 feet in height is proposed south of the southeast corner of the storage lot, but not along the entire southern frontage, nor at the southwestern corner of the property (not including the preserved woodland);*
3. *Landscape deviation from Section 5.5.3.C.ii and iii. for lack of interior canopy trees, in the southern portion of the vehicular storage area due to conflict with truck turning patterns.*
4. *Landscape deviation from Section 5.5.3.C.iv for lack of parking lot perimeter trees along 400 feet of eastern edge of property due to lack of room between drive and adjacent property;*
5. *Landscape deviation from Section 5.5.3.C.iv to allow planting of parking lot perimeter trees, more than 15 feet away from the edge of the vehicular storage area;*
6. *Landscape deviation from Section 5.5.3.D for the shortage of a total of 2980 square feet (37%) of required building foundation landscaping for the two buildings;*
7. *Landscape deviation from Section 5.5.3.D for allowing less than 75 percent of each building perimeter to be landscaped;*
8. *Landscape deviation from Section 5.5.3.D for the shortage of green scape along the building frontage facing Grand River (60% required, 54% proposed);*
9. *Landscape deviation from Section 5.5.3.C.ii.i. for the lack of landscape islands every 15 spaces within the enclosed outside storage yard due to the nature of the proposed use;*
10. *Traffic deviation from Section for proposing painted end islands in lieu of the required raised end islands.*
11. *Design and Construction waiver of Section 11-239 to allow gravel parking for storage yard in the rear.*
12. *Design and Construction waiver of Section 11-239 for lack of curb and gutter within the rear yard storage area.*
13. *Design and Construction waiver of Section 11-239 for lack of parking lot striping.*

SUMMARY OF REVIEWS

- a. Engineering Review: Additional comments to be addressed with Final Site Plan. Engineering recommends conditional approval.
- b. Landscape Review: Additional comments to be addressed with Final Site Plan. Landscape recommends conditional approval.
- c. Wetlands Review: A City of Novi Non-minor Wetland Permit and Buffer Authorization are required for the proposed impacts to wetlands and regulated wetland setbacks. Additional comments to be addressed with Final Site Plan. Wetlands recommend approval.
- d. Woodlands Review: Not applicable
- e. Traffic Review: Additional comments to be addressed with Final Site Plan. Traffic recommends conditional approval.
- f. Facade Review: Façade is currently note recommending approval.

- g. Fire Review: Additional comments to be addressed with Final Site Plan. Fire recommends conditional approval.

NEXT STEP: REVISED SITE PLAN SUBMITTAL

Façade review is currently not recommending approval. The proposed elevations are different from what was reviewed/approved as part of the PRO approval. However, the façade review notes the colors that are currently proposed are neutral and can be recommended with the exception of these comments:

- a. The color proposed for the louver feature and the accent band (SW6868 Real Red) is not consistent with Section 5.15.2 of the Façade Ordinance which prohibits intense colors. **Please work with our façade consultant to find a compatible red.**
- b. The façade materials on the side and rear elevations are not proposed to be changed. The PRO approval for the front façade was contingent on the side and rear elevations be painted or otherwise treated in a manner that is consistent with the front façade and that the existing natural fired clay tile will not be painted. **Please provide revised side and rear elevations as noted.**

Please submit revised elevations addressing comments in Façade review letter.

PLANNING COMMISSION MEETING

The site plan requires **Planning Commission's approval of Preliminary Site Plan, Wetland Permit and Storm Water Management Plan approval.** The site plan also requires a public hearing. **All façade issues should be resolved prior to scheduling the Planning Commission Meeting.**

Please work with the Planner to discuss timelines and deadlines.

FINAL SITE PLAN SUBMITTAL

After receiving the Preliminary Site Plan approval, please submit the following for Final site plan review and approval

1. Seven copies of Final Site Plan addressing all comments from Preliminary review
2. Response letter **addressing all comments and refer to sheet numbers where the change is reflected**
3. [Final Site Plan Application](#)
4. [Final Site Plan Checklist](#)
5. Engineering Cost Estimate
6. Landscape Cost Estimate
7. [Other Agency Checklist](#)
8. [Hazardous Materials Packet](#) (Non-residential developments)
9. [Non-Domestic User Survey](#) (Non-residential developments)
10. [No Revision Façade Affidavit](#) (if no changes are proposed for Façade)
11. Legal Documents as required
12. Drafts of any legal documents (note that off-site easements need to be executed and any on-site easements need to be submitted in draft form before stamping sets will be stamped)

ELECTRONIC STAMPING SET SUBMITTAL AND RESPONSE LETTER

After receiving Final Site Plan approval, please submit the following for Electronic stamping set approval:

1. Plans addressing the comments in all of the staff and consultant review letters in PDF format.
2. Response letter addressing all comments in ALL letters and ALL charts and **refer to sheet numbers where the change is reflected.**

STAMPING SET APPROVAL

Stamping sets are still required for this project. After having received all of the review letters from City staff the applicant should make the appropriate changes on the plans and submit **10 size 24" x 36" copies with original signature and original seals,** to the Community Development Department for final Stamping Set approval.

SITE ADDRESSING

A new address is not required for this project. The applicant should contact the Building Division for an address prior to applying for a building permit. Building permit applications cannot be processed without a correct address. The address application can be found by clicking on this [link](#).

Please contact the Ordinance Division 248.735.5678 in the Community Development Department with any specific questions regarding addressing of sites.

STREET AND PROJECT NAME

This project does not require approval from the Street and Project Naming Committee. Please contact Madeleine Kopko (248-347-0579) in the Community Development Department for additional information. The address application can be found by clicking on this [link](#).

PRE-CONSTRUCTION MEETING

A Pre-Construction meeting is required for this project. Prior to the start of any work on the site, Pre-Construction (Pre-Con) meetings must be held with the applicant's contractor and the City's consulting engineer. Pre-Con meetings are generally held after Stamping Sets have been issued and prior to the start of any work on the site. There are a variety of requirements, fees and permits that must be issued before a Pre-Con can be scheduled. If you have questions regarding the checklist or the Pre-Con itself, please contact Sarah Marchioni [248.347.0430 or smarchioni@cityofnovi.org] in the Community Development Department.

CHAPTER 26.5

Chapter 26.5 of the City of Novi Code of Ordinances generally requires all projects be completed within two years of the issuance of any starting permit. Please contact Sarah Marchioni at 248-347-0430 for additional information on starting permits. The applicant should review and be aware of the requirements of Chapter 26.5 before starting construction.

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5607 or skomaragiri@cityofnovi.org



Sri Ravali Komaragiri – Planner



PLANNING REVIEW CHART: I-2: General Industrial District with a PRO

Review Date: October 07, 2019
Review Type: Preliminary Site Plan
Project Name: JSP 18-31 Keford Towing
Plan Date: September 12, 2019
Prepared by: Sri Komaragiri, Planner
E-mail: skomaragiri@cityofnovi.org; **Phone:** (248) 735-5607

Items in **Bold** need to be addressed by the applicant with the next submittal.

Item	Required Code	Proposed	Meets Code	Comments
Zoning and Use Requirements				
Master Plan <i>(adopted August 25, 2016)</i>	Industrial Research Development Technology	Heavy Industrial	No	<i>On June 03, 2019, the City Council approved the proposed PRO Concept plan and the agreement.</i>
Area Study	Grand River Corridor Study		No	
Zoning <i>(Effective December 25, 2013)</i>	I-1: Light Industrial District	I-2 General Industrial	Yes	
Uses Permitted (Sec 3.1.18.B & C)	Permitted use are subject to the PRO Agreement. Refer to Planning review letter for more details	Outdoor storage yard for towed vehicles (160 cars) Auto body repair shop with 19 service bays and a Car rental services with 10 spaces (23,493 SF) Tool and dye shop (5,703 SF)	Yes	<i>Uses permitted are consistent with the PRO agreement.</i> The applicant should contact Community Development department once a prospective tenant is determined
Phasing	Provide phases lines and detail description of activities in each phase	Phasing not proposed	NA	
Automobile Service Establishment (Sec. 4.50)				
Site area	2 acres minimum	7.61 acres		
Site frontage	200 feet minimum	294.52 ft.		

Item	Required Code	Proposed	Meets Code	Comments
Vehicle parking within front yard setbacks	Not allowed	Not proposed	NA	
Vehicle parking within side yard setbacks	Not allowed	Not proposed	NA	
Service bay doors	No service bay doors shall face a major thoroughfare.	No doors	NA	
Curb cuts	Only 1 curb cut PC may require a marginal access roads; setbacks are measured from marginal access road	Two curb cuts existing	NA	
Height, bulk, density and area limitations (Sec 3.1.19)				
Frontage on a Public Street. (Sec. 5.12)	Frontage on a Public Street is required	Frontage on Grand River Avenue	Yes	
Access to Major Thoroughfare (Sec. 5.13)	Vehicular access shall be provided only to an existing or planned major thoroughfare or freeway service drive	Access to Grand River Avenue	Yes	
Minimum Zoning Lot Size for each Unit in Ac (Sec 3.6.2.D)	Except where otherwise provided in this Ordinance, the minimum lot area and width, and the maximum percent of lot coverage shall be determined on the basis of off-street parking, loading, greenbelt screening, yard setback or usable open space		NA	
Minimum Zoning Lot Size for each Unit: Width in Feet			NA	
Open Space Area	----		NA	---
Maximum % of Lot Area Covered (By All Buildings)	(Sec 3.6.2.D)	Existing Building	Yes	
Building Height (Sec. 3.1.19.D)	1-2: 60 ft.	Existing Building: varies from 11 feet to 33 feet	Yes	
Building Setbacks (Sec. 3.1.19.D)				
Front	100 ft.	181.5 ft.	Yes?	<i>Setbacks do not conform to the code, but they are considered legal non-conforming. No changes to existing setbacks</i>
Rear	50 ft.	Appears to be in conformance		
Side	50 ft.	18 feet east 48.9 feet west (Existing setbacks)		
Parking Setback (Sec 3.1.19.D) & Refer to applicable notes in Sec 3.6.2				

Item	Required Code	Proposed	Meets Code	Comments
Front	No parking in front building setback of 100 ft. Minimum site area: 2 acres Parking area > 50 % of front yard	No parking proposed within 100 feet	Yes/No?	<i>This deviation is included the approved PRO agreement.</i>
Rear	100 ft. min (Sec. 3.6.2.F)	103.4 ft.	Yes	
Side	20 ft. min	10.7 ft. west 20 ft. min on east	No	
Note To District Standards (Sec 3.6.2) For I-1 and I-2				
Exterior Side Yard Abutting a Street (Sec 3.6.2.C)	All exterior side yards abutting a street shall be provided with a setback equal to front yard.	No side yard abutting street	NA	
Off-Street Parking in Front Yard (Sec 3.6.2.E)	Development is 2 acres in size	7.61 acres	Yes	
	Parking does not extend into required building setback (100 ft.)	102 ft.	Yes	
	Parking does not occupy more than 50% of area b/w front setback and bldg. façade	Appears to comply	Yes	
	Parking is screened with 2.5 ft. brick wall or landscape berm	A berm is proposed;	Yes	
	Planning Commission finds parking is compatible with surrounding area	To be determined	TBD	
Off-Street Parking in Side and Rear Yards abutting residential (Sec 3.6.2.F)	Parking does not occupy more than 50% of area b/w side and rear abutting residential and bldg. façade	Applicant is proposing outside storage for a major part of the rear yard.	Yes	
	100 ft. setback	Appears to be in conformance		
Setback from Residential District (Sec 3.6.2.H)	Building shall be setback 3 feet for each foot of building height	33 feet. Maximum height 99 feet building setback provided	Yes	
Wetland/Watercourse Setback (Sec 3.6.2.M)	A setback of 25ft from wetlands and from high watermark course shall be maintained	Buffers are indicated on the plan	Yes	
Additional Height (Sec 3.6.2.O)	Additional heights for selected building is allowed based on conditions listed in Sec 3.6.2.O	Existing building	NA	
Parking setback screening (Sec 3.6.2.P)	Required parking setback area shall be landscaped per sec 5.5.3.	Provided	Yes	

Item	Required Code	Proposed	Meets Code	Comments
Modification of parking setback requirements <i>(Sec 3.6.2.Q)</i>	The Planning Commission may modify parking setback requirements based on conditions listed in Sec 3.6.2.Q	10.7 ft. setback proposed for western side yard in front of the building	No	<i>This deviation is included the approved PRO agreement.</i>
Parking, Loading and Dumpster Requirements				
Number of Parking Spaces Sec. 5.2 Sec. 4.50 Automobile Service Establishment Outside Storage As determined Car rental services	<u>Automobile Service Establishment</u> 2 spaces per each service plus 1 space for each employee -19 service bays and 20 employees Required: 58 spaces <u>Outside Storage</u> 158 vehicles <u>Out Building</u> Industrial warehouse establishment 1 space for 700 sf or five plus 1 per each employee 5703/700 = 8 spaces <u>Car rental (2,318 sf)</u> 1 per 222 GLA plus number of spaces designated or rental car parking 2318/222 = 10 spaces Refer to Section 5.2. To be determined based on the proposed use type	Total proposed parking: 263 158 vehicle storage 10 spaces for rental cars 94 spaces for office uses	Yes?	<i>Per approved PRO agreement, the car rental use shall have use of no more than 10 parking spaces in the rear yard of the building.</i> Clearly label rental car parking on the plan The applicant also referred to parking for vehicles prior to moving them inside for repair. Please clearly label dedicated spaces used for staging.
Parking Space Dimensions and maneuvering Lanes (Sec. 5.3.2)	90°: 9 ft. x 19 ft. parking spaces with 24 ft. drives 9 ft. x 17 ft. parking spaces along 7 ft. interior sidewalks, provided a 4 in. curb at these locations & along landscaping 0°: 8 ft. x 23 ft. parking spaces with 13 ft. drives	9 ft. x 17ft. with 24' to 34' wide aisles to accommodate tow trucks 9 ft. 19 ft. parking	Yes	
Parking stall adjacent to entrance <i>(Sec. 5.3.13)</i>	- shall not be located closer than twenty-five (25) feet from the street right-of-way (ROW) line, street easement	Not applicable	NA	

Item	Required Code	Proposed	Meets Code	Comments
	or sidewalk, whichever is closer			
End Islands (Sec. 5.3.12)	<ul style="list-style-type: none"> - End Islands with landscaping and raised curbs are required at the end of all parking bays that abut traffic circulation aisles. - The end islands shall generally be at least 8 feet wide, have an outside radius of 15 feet, and be constructed 3' shorter than the adjacent parking stall as illustrated in the Zoning Ordinance 	The plan does not propose end islands in the rear yard and the outside storage	No	<i>This deviation is included the approved PRO agreement.</i>
Barrier Free Spaces Barrier Free Code	To be determined based on required parking	Proposed	Yes	
Barrier Free Space Dimensions Barrier Free Code	<ul style="list-style-type: none"> - 8' wide with an 8' wide access aisle for van accessible spaces - 5' wide with a 5' wide access aisle for regular accessible spaces 			
Barrier Free Signs Barrier Free Code	One sign for each accessible parking space.	Proposed	Yes	
Minimum number of Bicycle Parking (Sec. 5.16.1)	Four (4) spaces	Minimum 4 spaces	Yes	
Bicycle Parking General requirements (Sec. 5.16)	No farther than 120 ft. from the entrance being served	Less than 120 ft.	Yes	
	When 4 or more spaces are required for a building with multiple entrances, the spaces shall be provided in multiple locations	All four spaces proposed in a single location	Yes	
	Spaces to be paved and the bike rack shall be inverted "U" design	inverted "U" design	Yes	
	Shall be accessible via 6 ft. paved sidewalk	6 ft. paved sidewalk	Yes	
Bicycle Parking Lot layout (Sec 5.16.6)	Parking space width: 6 ft. One tier width: 10 ft. Two tier width: 16 ft. Maneuvering lane width: 4 ft. Parking space depth: 2 ft. single, 2 ½ ft. double	Six bike spaces proposed	Yes	

Item	Required Code	Proposed	Meets Code	Comments
Loading Spaces (Sec. 5.4.1)	Loading area in the rear yard Loading area in interior side yard if it is adjacent to I, EXPO or EXO district	Proposed in the rear	Yes	Refer to Traffic review for more comments
Accessory Structures				
Dumpster (Sec 4.19.2.F)	<ul style="list-style-type: none"> - Located in rear yard - Attached to the building or - No closer than 10 ft. from building if not attached - Not located in parking setback - If no setback, then it cannot be any closer than 10 ft, from property line. - Away from Barrier free Spaces 	11 ft. from the building	Yes	
Dumpster Enclosure (Sec. 21-145. (c))	<ul style="list-style-type: none"> - Screened from public view - A wall or fence 1 ft. higher than height of refuse bin - And no less than 5 ft. on three sides - Posts or bumpers to protect the screening - Hard surface pad. - Screening Materials: Masonry, wood or evergreen shrubbery 	Unable to determine	Yes?	Provide dumpster elevations to verify conformance
Roof top equipment and wall mounted utility equipment (Sec. 4.19.2.E.ii)	All roof top equipment must be screened and all wall mounted utility equipment must be enclosed and integrated into the design and color of the building	Existing building	NA	
Roof top appurtenances screening	Roof top appurtenances shall be screened in accordance with applicable facade regulations, and shall not be visible from any street, road or adjacent property.	Existing building	NA	
I-2 District Required Conditions (Sec. 3.15)				
Outdoor Storage	Storage cannot extend to a greater height than the obscure on-site screen	8 foot chain-link fence along edge of parking	Yes	
Sidewalks and Pathways				
Article XI. Off-Road Non-Motorized Facilities	A 6 foot sidewalk is required along Grand River Avenue	Existing sidewalk	Yes	
Pedestrian Connectivity	Assure safety and convenience of both vehicular	Provided	Yes	

Item	Required Code	Proposed	Meets Code	Comments
	and pedestrian traffic both within the site and in relation to access streets			
Other Requirements				
Exterior lighting <i>(Sec. 5.7)</i>	Photometric plan and exterior lighting details needed at time of Final Site Plan submittal	A plan is provided	Yes?	Refer to comments provided later in the chart
Design and Construction Standards Manual	Land description, Sidwell number (metes and bounds for acreage parcel, lot number(s), Liber, and page for subdivisions).	Provided	Yes	
General layout and dimension of proposed physical improvements	Location of all existing and proposed buildings, proposed building heights, building layouts, (floor area in square feet), location of proposed parking and parking layout, streets and drives, and indicate square footage of pavement area (indicate public or private).	Mostly provided	Yes?	Refer to Traffic review for more comments
Economic Impact Information	- Total cost of the proposed building & site improvements - Number of anticipated jobs created (during construction & after building is occupied, if known)	Provided on page 8 of the narrative	Yes	
Development and Street Names	Development and street names must be approved by the Street Naming Committee before Preliminary Site Plan approval	Not Applicable. Project name is an established business name		
Development/ Business Sign	Signage if proposed requires a permit.	Building signage is indicated on the elevations.		<u>A sign permit is required.</u> For sign permit information contact Ordinance at 248-347-0438 .
Lighting and Photometric Plan (Sec.5.7)				
Intent <i>(Sec. 5.7.1)</i>	Establish appropriate minimum levels, prevent unnecessary glare, reduce spillover onto adjacent properties & reduce unnecessary transmission of light into the night sky	A plan is provided	Yes	
Lighting Plan <i>(Sec. 5.7.A.1)</i>	Site plan showing location of all existing & proposed buildings, landscaping, streets, drives, parking areas & exterior lighting fixtures			

Item	Required Code	Proposed	Meets Code	Comments
Building Lighting (Sec. 5.7.2.A.iii)	Relevant building elevation drawings showing all fixtures, the portions of the walls to be illuminated, luminance levels of walls and the aiming points of any remote fixtures.	Not provided	No	Please provide the photometric information on building facades, in addition to the site photometric plan.
Lighting Plan (Sec.5.7.A.2)	Specifications for all proposed & existing lighting fixtures	Provided		Hours of operation not provided Specification sheets for all proposed lighting are not provided. Please provide as required
	Photometric data	Provided		
	Fixture height	Provided (22 ft. to 25 ft.)		
	Mounting & design	Pole and wall mount LED		
	Glare control devices			
	Type & color rendition of lamps			
	Hours of operation			
Photometric plan illustrating all light sources that impact the subject site, including spill-over information from neighboring properties				
Required Conditions (Sec. 5.7.3.A)	Height not to exceed maximum height of zoning district (or 25 ft. where adjacent to residential districts or uses	25 ft. maximum	Yes	
Required Conditions (Sec. 5.7.3.B)	<ul style="list-style-type: none"> - Electrical service to light fixtures shall be placed underground - Flashing light shall not be permitted - Only necessary lighting for security purposes & limited operations shall be permitted after a site's hours of operation 	Notes are provided on sheet : Preliminary site plan	No	Please add these notes to photometric sheet P-1
Security Lighting (Sec. 5.7.3.H) Lighting for security purposes shall be directed only onto the area to be secured.	<ul style="list-style-type: none"> - All fixtures shall be located, shielded, and aimed at the areas to be secured. - Fixtures mounted on the building and designed to illuminate the facade are preferred. 	Not provided	No	Indicate what lights will be turned on past hours of operation for security reasons. A separate photometric plan is required for security lights only
Required Conditions (Sec.5.7.3.E)	Average light level of the surface being lit to the lowest light of the surface being lit shall not exceed 4:1	Does not exceed 4:1	Yes	
Required Conditions (Sec. 5.7.3.F)	Use of true color rendering lamps such as metal halide is preferred over high & low	LED	Yes	

Item	Required Code	Proposed	Meets Code	Comments
	pressure sodium lamps			
Min. Illumination <i>(Sec. 5.7.3.k)</i>	Parking areas: 0.2 min	All minimums are met	Yes	
	Loading & unloading areas: 0.4 min			
	Walkways: 0.2 min			
	Building entrances, frequent use: 1.0 min			
	Building entrances, infrequent use: 0.2 min			
Max. Illumination adjacent to Non-Residential <i>(Sec. 5.7.3.K)</i>	When site abuts a non-residential district, maximum illumination at the property line shall not exceed 1 foot candle	Maximum of 0.8 provided along sides that abut non-residential	Yes	
Cut off Angles <i>(Sec. 5.7.3.L)</i>	when adjacent to residential districts - All cut off angles of fixtures must be 90° - maximum illumination at the property line shall not exceed 0.5 foot candle	0 foot candles provided along property lines abutting residential	Yes	

NOTES:

1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards.
2. The section of the applicable ordinance or standard is indicated in parenthesis. Please refer to those sections in Article 3, 4 and 5 of the zoning ordinance for further details.
3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals.

ENGINEERING REVIEW



PLAN REVIEW CENTER REPORT

October 10, 2019

Engineering Review
Keford Collision and Towing
JSP18-0031

Applicant

Keford Properties, LLC

Review Type

Preliminary Site Plan

Property Characteristics

- Site Location: South side of Grand River Avenue, between Taft Road and Novi Road
- Site Size: 7.61 acres
- Plan Date: 09/12/2019
- Design Engineer: Alpine Engineering, Inc.

Project Summary

- Modifications to existing parking lot at existing building and addition of a tow-yard vehicle storage/parking area south of the existing building.
- Water service would be provided to the two existing buildings by two proposed domestic leads of unspecified diameter.
- Sanitary sewer service would be provided to the two existing buildings by two proposed 6-inch leads to an existing 18-inch main.
- Storm water would be collected on site, with bank full detention/pretreatment storage provided with restricted discharge to an off-site regional detention basin.

Recommendation

Approval of the Preliminary Site Plan is recommended, with items to be addressed at Final Site Plan submittal.

Comments:

The Preliminary Site Plan meets the general requirements of Chapter 11 of the Code of Ordinances, the Storm Water Management Ordinance and the Engineering Design Manual with the following exceptions, which shall be addressed at **Final Site Plan submittal**:

General

1. A right-of-way permit will be required from the City of Novi for work in the Grand River Avenue right-of-way.
2. A right-of-way permit will also be required from the Road Commission for Oakland County (RCOC) for work in the Grand River Avenue right-of-way.
3. The Non-domestic User Survey form shall be submitted to the City so it can be forwarded to Oakland County.
4. Show and label the existing 50-foot half right-of-way width for Grand River Avenue.
5. A license agreement will be required for fencing proposed within existing sanitary sewer easement.

Water Main

6. The applicant should be aware that additional hydrants may be required per Fire Department review, given the lack of existing hydrants on-site and the distance of the nearest existing hydrant northeast of the site in the Grand River Avenue right-of-way.

Storm Water Management Plan

7. The Storm Water Management Plan for this development shall be designed in accordance with the Storm Water Ordinance and Chapter 5 of the new Engineering Design Manual.
8. The storm water management plan proposes to maintain an existing condition of site drainage going into the Grand River right-of-way. Review and approval by the Road Commission for Oakland County will be required, and a variance from the Design and Construction Standards is required in any case where all drainage is not captured on-site.
9. Restricted discharge to an off-site regional detention basin is proposed. Bankfull storage will be provided on-site. Any applicable storm water detention tap fees will be pro-rated for bankfull detention storage provided on the site.
10. Provide a soil boring in the vicinity of the basin to determine soil conditions and to establish the high water elevation of the groundwater table.
11. One foot of freeboard shall be provided above the uppermost storage elevation, rather than the 0.4 foot shown on the plan.
12. A 25-foot vegetated buffer shall be provided around the storm water basin where any pavement runoff is directed toward the basin.
13. The maintenance access route to the basin outlet structure shall be a minimum of 15 feet wide for its entirety, including the proposed rip-rap and curb drop.
14. Provide a 5-foot wide stone bridge/access route allowing direct access to the standpipe from the bank of the basin during high-water conditions (i.e. stone 6-inches above high water elevation).

15. Provide an access easement for maintenance over the storm water detention system and the pretreatment structure. Also, include an access easement to the detention area from the public road right-of-way.
16. Provide manufacturer's details and sizing calculations for the pretreatment structure within the plans. Provide drainage area and runoff coefficient calculations specific to the area tributary to the treatment structure. The treated flow rate should be based on the 1-year storm event intensity (~1.6 In/Hr). Higher flows shall be bypassed.
17. Provide release rate calculations for the two design storm events addressed on this site (first flush and bankfull).
18. Due to maintenance concerns, each restricting orifice in the control structure shall be a minimum of 1 square-inch in size, even though this may result in a flow rate above that calculated.

Paving & Grading

19. In consideration of the City's waiver of the ordinance requirement that the rear storage area be paved, Developer shall:
 - a. Keep the access aisle as shown on the PRO plan free from parked vehicles or other obstructions so that there is fire truck access at all times (with the details of the Developer's plan to designate or demarcate the access aisle shown on the final approved site plan);
 - b. Undertake regular maintenance of the gravel storage area so as to prevent the migration of the gravel storage area to other areas of the Land or Development or adjacent properties or roadways or nearby waterbodies. To ensure compliance with these requirements, the City shall have a reasonable right to enter onto the Land or Development for purposes of inspection; provided, however, the City shall give 48 hours' notice of any expected inspection. If the City determines that corrective action is required to be taken, it shall issue a notice of corrective action, which shall include a time period for correction. Failure to comply shall be treated as a breach of this Agreement.
20. Design and Construction waivers of Section 11-239 of the City's Zoning Ordinance were authorized:
 - a. To allow gravel parking for storage yard in the rear;
 - b. For lack of curb and gutter within the rear yard storage area;
 - c. For lack of parking lot striping.
21. Two of the proposed barrier free ramps appear to exceed the absolute maximum longitudinal slope of 8.33%. These ramps are (1) the longest ramp at the northwest corner of the main existing building, and (2) the short ramp at the northeast corner of the paved parking lot.
22. Verify and label the slopes along the ingress/egress routing to the building from the barrier-free stalls. All barrier-free stalls shall comply with Michigan Barrier-Free regulations.
23. Detectable warning plates are required at all barrier free ramps, hazardous vehicular crossings and other areas where the sidewalk is flush with the adjacent drive or parking pavement. The barrier-free ramps shall comply

- with current MDOT specifications for ADA Sidewalk Ramps. Provide the latest version of the MDOT standard detail for detectable surfaces.
24. Label specific ramp locations on the plans where the detectable warning surface is to be installed.
 25. Specify the product proposed and provide a detail for the detectable warning surface for barrier free ramps. The product shall be the concrete-embedded detectable warning plates, or equal, and shall be approved by the Engineering Division. Stamped concrete will not be acceptable.
 26. Provide proposed contours for areas surrounding the rear yard gravel lot.
 27. Clarify all proposed contours by labeling contour elevations.
 28. Add pavement and curb grades to proposed parking along existing south building.
 29. Curbing and walks adjacent to the end of 17-foot stalls shall be reduced to 4-inches high (rather than the standard 6-inch height to be provided adjacent to 19-foot stalls). Some top-of-curb grades adjacent to 17-foot stalls continue to be 6 inches above top-of-pavement.
 30. Provide a plan view and cross-section of the retaining walls. Sheet with these details shall be signed and sealed by the design engineer responsible for the proposed retaining wall design and all associated calculations.

Soil Erosion and Sediment Control

31. An SESC permit is required. A full review has not been completed at this time. The review checklist detailing all SESC requirements is attached to this letter. Please submit an SESC permit application under separate cover. The application can be found on the City's website at <http://cityofnovi.org/Reference/Forms-and-Permits.aspx>.

Off-Site Easements

32. Any required off-site easements must be executed prior to final approval of the plans. Drafts shall be submitted at the time of the Final Site Plan submittal.

The following must be submitted at the time of Final Site Plan submittal:

33. A letter from either the applicant or the applicant's engineer must be submitted with the Final Site Plan highlighting the changes made to the plans addressing each of the comments listed above and indicating the revised sheets involved. **Additionally, a statement must be provided stating that all changes to the plan have been discussed in the applicant's response letter.**
34. An itemized construction cost estimate must be submitted to the Community Development Department for the determination of plan review and construction inspection fees. This estimate should only include the civil site work and not any costs associated with construction of the building or any demolition work. **The estimate must be itemized** for each utility (water, sanitary, storm sewer), on-site paving (square yardage), right-of-way paving (including proposed right-of-way), grading, and the storm water basin (basin construction, control structure, pre-treatment structure and restoration).

The following must be submitted with the Stamping Set:

(Please note that all documents must be submitted together as a package with the Stamping Set submittal with a legal review transmittal form that can be found on the City's website. Partial submittals will not be accepted.)

35. A draft copy of the Storm Drainage Facility Maintenance Easement Agreement (SDFMEA), as outlined in the Storm Water Management Ordinance, must be submitted to the Community Development Department. Once the agreement is approved by the City's Legal Counsel, this agreement will then be sent to City Council for approval/acceptance. The SDFMEA will then be recorded at the office of the Oakland County Register of Deeds. This document is available on our website.
36. A draft copy of the access easement to sanitary sewer monitoring manholes must be submitted to the Community Development Department. This document is available on our website.

The following must be addressed prior to construction:

37. A pre-construction meeting shall be required prior to any site work being started. Please contact Sarah Marchioni in the Community Development Department to setup a meeting (248-347-0430).
38. A City of Novi Grading Permit will be required prior to any grading on the site. This permit will be issued at the pre-construction meeting (no application required). No fee is required for this permit.
39. Material certifications must be submitted to Spalding DeDecker for review prior to the construction of any onsite utilities. Contact Ted Meadows at 248-844-5400 for more information.
40. Construction inspection fees must be paid to the Community Development Department.
41. Legal escrow fees must be deposited with the Community Development Department. All unused escrow will be returned to the payee at the end of the project. This amount includes engineering legal fees only. There may be additional legal fees for planning legal documents.
42. A storm water performance guarantee (equal to 120% of the cost required to complete the storm water management facilities) as specified in the Storm Water Management Ordinance must be posted at the Community Development Department.
43. Water and Sanitary Sewer Fees must be paid prior to the pre-construction meeting. Contact the Water & Sewer Division at 248-347-0498 to determine the amount of these fees.
44. A street sign financial guarantee (\$400 per traffic control sign proposed) must be posted at the Community Development Department. Signs must be installed in accordance with MMUTCD standards.

45. As described previously, a Soil Erosion Control Permit must be obtained from the City of Novi. Contact Sarah Marchioni in the Community Development Department, Building Division (248-347-0430) for forms and information. The financial guarantee and inspection fees will be determined during the SESC review.
46. A permit for all proposed work activities within the road right-of-way of Grand River Avenue must be obtained from the City of Novi. This application is available from the City Engineering Division or on the City website and can be filed once the Final Site Plan has been submitted. Please contact the Engineering Division at 248-347-0454 for further information. Please submit the cover sheet, standard details and plan sheets applicable to the permit only.
47. A permit for work within the road right-of-way of Grand River Avenue must be obtained from the Road Commission for Oakland County (RCOC). Please contact the RCOC (248-858-4835) directly with any questions. The applicant must forward a copy of this permit to the City. Provide a note on the plans indicating all work within the road right-of-way will be constructed in accordance with the RCOC standards.
48. An NPDES permit must be obtained from the MDEQ since the site is over 5 acres in size. The MDEQ may require an approved SESC plan to be submitted with the Notice of Coverage.
49. An inspection permit for the sanitary sewer taps must be obtained from the Oakland County Water Resource Commissioner (OCWRC).

The following must be addressed prior to issuance of a Temporary Certificate of Occupancy (TCO) approval for the development:

50. The amount of the incomplete site work performance guarantee for any outstanding site improvement items (limited to top course of pavement and other minor items), is calculated at 1.2 times the amount required to complete the site improvements (as specified in the Performance Guarantee Ordinance).
51. All easements and agreements referenced above must be executed, notarized and approved by the City Attorney and Engineering Division.
52. The City's consultant Engineer Spalding DeDecker will prepare the record drawings for this development. The record drawings will be prepared in accordance with Article XII, Design and Construction Standards, Chapter 11 of the Novi Code of Ordinances.
53. Submit an up-to-date Title Policy (dated within 90 days of City Council consideration of acceptance) for the purpose of verifying that the parties signing the Easement documents have the legal authority to do so. Please be sure that all parties of interest shown on the title policy (including mortgage holders) either sign the easement documents themselves or

provide a Subordination Agreement. Please be aware that the title policy may indicate that additional documentation is necessary to complete the acceptance process.

To the extent this review letter addresses items and requirements that require the approval of or a permit from an agency or entity other than the City, this review shall not be considered an indication or statement that such approvals or permits will be issued.

Please contact Victor Boron at (248) 735-5695 with any questions.

A handwritten signature in black ink that reads "Victor Boron". The signature is written in a cursive style and is positioned above a horizontal line.

Victor Boron
Civil Engineer

cc: Sri Komaragiri, Community Development
Angela Sosnowski, Community Development
Tina Glenn, Treasurers
Kristin Pace, Treasurers
Ben Croy, Engineering
T. Meadows, T. Reynolds; Spalding DeDecker

LANDSCAPE REVIEW



PLAN REVIEW CENTER REPORT

September 30, 2019

Preliminary Landscape Plan - Landscaping

Keford Towing

Review Type _____
Preliminary Landscape Plan Review

Job # _____
JSP18-0031

Property Characteristics

- Site Location: 45241 Grand River Ave.
- Site Acreage: 7.6 acres
- Site Zoning: I-1 Proposed rezone to I-2.
- Adjacent Zoning: North, East, West: I-1, South: RA, R-4
- Plan Date: 9/12/2019

Ordinance Considerations

This project was reviewed for conformance with Chapter 37: Woodland Protection, Zoning Article 5.5 Landscape Standards, the Landscape Design Manual and any other applicable provisions of the Zoning Ordinance. Items in **bold** below must be addressed and incorporated as part of the Final Site Plans. Please follow guidelines of the Zoning Ordinance and Landscape Design Guidelines. This review and the accompanying Landscape Chart is a summary and not intended to substitute for any Ordinance.

Recommendation

This plan **is recommended for Preliminary Site Plan approval**. Please make the indicated changes on Final Site Plans.

Landscape Deviations Granted by the Planning Commission on 9/26/2018:

1. Not meeting the minimum requirements for a 10 - 15 foot tall landscaped berm or not providing the minimum required screening trees between residentially zoned property and industrial. A berm approximately 7 feet in height is proposed south of the southeast corner of the storage lot, but not along the entire southern frontage, nor at the southwestern corner of the property (not including the preserved woodland);
2. Lack of interior canopy trees in the southern portion of the vehicular storage area due to conflict with truck turning patterns.
3. Lack of parking lot perimeter trees along 400 feet of the eastern edge of the property due to lack of room between drive and adjacent property
4. Parking lot perimeter trees planted more than 15 feet away from the vehicular storage area
5. Shortage of a total of 2980 square feet (37%) of required building foundation landscaping for the two buildings
6. Less than 75 percent of each building perimeter to be landscaped
7. Shortage of green scape along the building frontage facing Grand River (60% required, 54% proposed);
8. Lack of landscape islands every 25 spaces within the enclosed outside storage yard due to the nature of the proposed use

Please copy the above, including the meeting date, to Sheet L-1 in place of the other text regarding waivers.

Ordinance Considerations

Existing Soils (Preliminary Site Plan checklist #10, #17)

Provided

Existing and proposed overhead and underground utilities, including hydrants.(LDM 2.e.(4))

Provided

Existing Trees (Sec 37 Woodland Protection, Preliminary Site Plan checklist #17 and LDM 2.3 (2))

Provided

Adjacent to Residential - Buffer (Zoning Sec. 5.5.3.B.ii and iii)

1. The required 10-15' berm is not provided as required between the residential properties and the site.
2. A 7 foot tall berm is provided along the eastern 230' of the southern parking lot frontage, well south of the lot.
3. Most of the existing woodland at the southwest corner of the lot is being preserved.
4. A landscape deviation was granted for the lack of berm and landscaping for the parts of the southern frontage abutting residential property and for the lack of height of the proposed berm.
5. Additional evergreens have been added along the south border to provide additional screening for the residences to the south.

Required I-2 Screening/Outdoor Storage yards (4.55)

A landscape deviation was granted to allow the applicant to screen with evergreens and opaque fencing.

Adjacent to Public Rights-of-Way – Berm (Wall) & Buffer (Zoning Sec. 5.5.3.B.ii and iii)

The required berm and landscaping are provided.

Street Tree Requirements (Zoning Sec. 5.5.3.E.i.c and LDM 1.d.)

The RCOC sight vision requirements leave no room for any street trees along Grand River and none are provided.

Parking Lot Landscaping (Zoning Sec. 5.5.3.C.)

1. Based on the vehicular use areas, 3,019 sf of islands and 15 interior trees are required. 3,049 sf of islands and 15 trees are provided, all but 3 of which are located in the north part of the site.
2. Landscape deviations were granted for the lack of interior islands and interior landscaping in the south section (vehicular storage area) of the site.

Parking Lot Perimeter Canopy Trees (Zoning Sec. 5.5.3.C.(3) Chart footnote)

1. The site has a total of 2203 lf of parking lot perimeter, including access drives from Grand River, 412 lf of which are along the east edge where there is no room for trees. The applicant has not proposed deciduous canopy trees along the eastern property line and most of the southern vehicular storage lot perimeter.
2. Landscape deviations were granted for the lack of perimeter trees along the east accessway due to a lack of room, and to plant parking lot perimeter trees further than 15 feet from the edge of the parking lot.

Loading Zone screening (Zoning Sec. 3.14, 3.15, 4.55, 4.56, 5.5)

1. An eight-foot screening fence with opacity greater than 90% is proposed around the entire storage area of the site. This, along with the evergreen trees planted along the west side of the site, is acceptable.
2. Additional screening beyond the opaque fencing is not required along the east side of the site as it fronts on a regional detention pond zoned I-1, which has a large berm on the eastern end that screens the historic home from the site.

Building Foundation Landscape (Zoning Sec 5.5.3.D.)

1. A total of 6064 sf of foundation landscaping is required for the main building. Only 5339sf are provided. A landscape deviation was granted for a shortage in the landscaping.
2. A total of 2016 sf of foundation landscaping is required for the outbuilding and 2385 sf is provided.
3. Neither building meets the 75% minimum requirement of building perimeter with at least 4' strip of landscaping, which is a variation. A landscape deviation was granted for this deficiency.
4. 54% of the main building's frontage facing Grand River is landscaped, which is less than the 60% requirement. A landscape deviation was granted for this deficiency.

Plant List (LDM 2.h. and t.)

1. Provided
2. The tree diversity requirements of the Landscape Design Manual were exceeded by the evergreens.
3. **Please add Norway spruce (*Picea abies*) as an additional species of spruce to reduce the percentages of *Abies concolor* and *Picea glauca*.**
4. 15 of 22 species used (68%) are native to Michigan.

Planting Notations and Details (LDM)

Provided

Storm Basin Landscape (Zoning Sec 5.5.3.E.iv and LDM 1.d.(3))

Provided

Irrigation (LDM 1.a.(1)(e) and 2.s)

1. **The proposed landscaping must be provided with sufficient water to become established and survive over the long term. Please note how this will be accomplished if an irrigation plan is not provided.**
2. **Please provide the plan with Final Site Plans or Electronic Stamping Sets at the latest.**

Proposed topography. 2' contour minimum (LDM 2.e.(1))

Provided

Snow Deposit (LDM.2.q.)

Provided

Proposed trees to be saved (Sec 37 Woodland Protection 37-9, LDM 2.e.(1))

Provided

Corner Clearance (Zoning Sec 5.9)

Provided

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.735.5621 or rmeader@cityofnovi.org.



Rick Meader – Landscape Architect

LANDSCAPE REVIEW SUMMARY CHART – Preliminary Site Plan

Review Date: September 30, 2018
Project Name: JSP18 – 0031: Keford Collision & Towing
Plan Date: September 12, 2019
Prepared by: Rick Meader, Landscape Architect E-mail: rmeader@cityofnovi.org;
 Phone: (248) 735-5621

Items in **Bold** need to be addressed by the applicant on the Final Site Plan.

Landscape Deviations Granted by the Planning Commission on 9/26/2018:

1. Not meeting the minimum requirements for a 10 - 15 foot tall landscaped berm or not providing the minimum required screening trees between residentially zoned property and industrial. A berm approximately 7 feet in height is proposed south of the southeast corner of the storage lot, but not along the entire southern frontage, nor at the southwestern corner of the property (not including the preserved woodland);
2. Lack of interior canopy trees in the southern portion of the vehicular storage area due to conflict with truck turning patterns.
3. Lack of parking lot perimeter trees along 400 feet of the eastern edge of the property due to lack of room between drive and adjacent property
4. Parking lot perimeter trees planted more than 15 feet away from the vehicular storage area
5. Shortage of a total of 2980 square feet (37%) of required building foundation landscaping for the two buildings
6. Less than 75 percent of each building perimeter to be landscaped
7. Shortage of green scape along the building frontage facing Grand River (60% required, 54% proposed);
8. Lack of landscape islands every 25 spaces within the enclosed outside storage yard due to the nature of the proposed use

Please copy the above, including the meeting date, to Sheet L-1 in place of the other text regarding waivers.

Item	Required	Proposed	Meets Code	Comments
Landscape Plan Requirements (LDM (2))				
Landscape Plan <i>(Zoning Sec 5.5.2, LDM 2.e.)</i>	<ul style="list-style-type: none"> ▪ New commercial or residential developments ▪ Addition to existing building greater than 25% increase in overall footage or 400 SF whichever is less. ▪ 1"=20' minimum with proper North. Variations from this scale can be approved by LA ▪ Consistent with plans throughout set 	Scale 1"=50'	Yes	
Project Information	Name and Address	Yes	Yes	

Item	Required	Proposed	Meets Code	Comments
<i>(LDM 2.d.)</i>				
Owner/Developer Contact Information <i>(LDM 2.a.)</i>	Name, address and telephone number of the owner and developer or association	Yes – the address is on the cover sheet but not the Landscape plan.	Yes	
Landscape Architect contact information <i>(LDM 2.b.)</i>	Name, Address and telephone number of RLA/LLA	Yes	Yes	
Sealed by LA. <i>(LDM 2.g.)</i>	Requires original signature	Yes	Yes	<u>Original signature is needed on printed stamping sets.</u>
Miss Dig Note (800) 482-7171 <i>(LDM.3.a.(8))</i>	Show on all plan sheets	Yes	Yes	
Zoning <i>(LDM 2.f.)</i>	Include all adjacent zoning	<u>Parcel:</u> I-1 proposed rezone to I-2. <u>North, East, West:</u> I-1 <u>South:</u> RA, R-4	Yes	
Survey information <i>(LDM 2.c.)</i>	<ul style="list-style-type: none"> ▪ Legal description or boundary line survey ▪ Existing topography 	Topographic survey and legal description on Sheet 2	Yes	
Existing plant material Existing woodlands or wetlands <i>(LDM 2.e.(2))</i>	<ul style="list-style-type: none"> ▪ Show location type and size. Label to be saved or removed. ▪ Plan shall state if none exists. 	<ul style="list-style-type: none"> ▪ Existing trees and trees proposed to be removed shown on Sheet L-3. ▪ Tree chart and removal calculations also shown on Sheet L-3. 	Yes	See ECT review for more detailed discussion of woodlands and wetlands.
Soil types <i>(LDM.2.r.)</i>	<ul style="list-style-type: none"> ▪ As determined by Soils survey of Oakland county ▪ Show types, boundaries 	<ul style="list-style-type: none"> ▪ Sheet 5 ▪ Boundaries not shown, only Marlette mentioned. 	Yes	
Existing and proposed improvements <i>(LDM 2.e.(4))</i>	Existing and proposed buildings, easements, parking spaces, vehicular use areas, and R.O.W	Yes	Yes	
Existing and proposed utilities <i>(LDM 2.e.(4))</i>	Overhead and underground utilities, including hydrants	Yes	Yes	
Proposed grading. 2' contour minimum <i>(LDM 2.e.(1))</i>	Provide proposed contours at 2' interval	<ul style="list-style-type: none"> ▪ 7 foot tall berm is provided at southeast corner of property. It has 	Yes	

Item	Required	Proposed	Meets Code	Comments
		been extended to the east as requested to better screen the home southeast of the site. <ul style="list-style-type: none"> ▪ Section views of the property are provided. 		
Snow deposit <i>(LDM.2.q.)</i>	Show snow deposit areas on plan	Yes	Yes	
LANDSCAPING REQUIREMENTS				
Parking Area Landscape Requirements LDM 1.c. & Calculations (LDM 2.o.)				
General requirements <i>(LDM 1.c)</i>	<ul style="list-style-type: none"> ▪ Clear sight distance within parking islands ▪ No evergreen trees 	Yes	Yes	
Name, type and number of ground cover (LDM 1.c.(5))	As proposed on planting islands	Low prairie mix is indicated on islands, lawn in greenbelt and perimeter.	Yes	
General <i>(Zoning Sec 5.5.3.C.ii)</i>				
Parking lot Islands <i>(a, b. i)</i>	<ul style="list-style-type: none"> ▪ A minimum of 200 SF to qualify ▪ A minimum of 200sf unpaved area per tree planted in an island ▪ 6" curbs ▪ Islands minimum width 10' BOC to BOC 	All new islands in south section are just painted (on gravel).	No	A landscape deviation was granted for the painted islands in the vehicular storage area.
Curbs and Parking stall reduction <i>(c)</i>	Parking stall can be reduced to 17' and the curb to 4" adjacent to a sidewalk of minimum 7 ft.	Landscape island areas are provided.	Yes	
Contiguous space limit <i>(i)</i>	Maximum of 25 contiguous spaces	<ul style="list-style-type: none"> ▪ 31 is maximum bay length ▪ The applicant is not proposing the required islands in the south section in order to make it easier for their tow vehicles to maneuver around the site. 	No	A landscape deviation was granted for the lack of islands in the vehicular storage area.
Plantings around Fire Hydrant <i>(d)</i>	<ul style="list-style-type: none"> ▪ No plantings with matured height greater than 12' within 	None	Yes	

Item	Required	Proposed	Meets Code	Comments
	10 ft. of fire hydrants ▪ Trees should also be at least 5 feet from underground lines.			
Landscaped area (g)	Areas not dedicated to parking use or driveways exceeding 100 sq. ft. shall be landscaped	Yes	Yes	
Clear Zones (LDM 2.3.(5))	25 ft corner clearance required. Refer to Zoning Section 5.5.9	▪ RCOC clear vision zones are provided. ▪ They occupy all of the frontage.	Yes	1. No street trees are required due to the RCOC requirements. 2. This does not require a deviation as there is no room for the trees that would be required.
Category 1: For OS-1, OS-2, OSC, OST, B-1, B-2, B-3, NCC, EXPO, FS, TC, TC-1, RC, Special Land Use or non-residential use in any R district (Zoning Sec 5.5.3.C.iii)				
A = Total square footage of vehicular use areas up to 50,000sf x 7.5%	<ul style="list-style-type: none"> • $A = x \text{ sf} * 7.5 \% = A \text{ sf}$ • $50,000 * 7.5\% = 3750 \text{ sf}$ 	NA		
B = Total square footage of additional paved vehicular use areas (not including A or B) over 50,000 SF) x 1 %	<ul style="list-style-type: none"> • $B = x \text{ sf} * 1\% = B \text{ sf}$ • $(xxx - 50000) * 1\% = xx \text{ sf}$ 	NA		
Category 2: For: I-1 and I-2 (Zoning Sec 5.5.3.C.iii)				
A. = Total square footage of vehicular use area up to 50,000 sf x 5%	<ul style="list-style-type: none"> • $A = x \text{ sf} * 5\% = A \text{ sf}$ • $50000 * 5\% = 2500 \text{ sf}$ 			
B = Total square footage of additional paved vehicular use areas over 50,000 SF x 0.5%	<ul style="list-style-type: none"> ▪ $B = 0.5\% \times 0 \text{ sf} = B \text{ SF}$ ▪ $(153824-50000)*0.5\% = 519 \text{ sf}$ 			
All Categories				
C = A+B Total square footage of landscaped islands	$2500 + 519 = 3019 \text{ SF}$	3049 sf	Yes/No	A landscape deviation was granted for the lack of landscape islands in the vehicular storage area.
D = C/200 Number of canopy trees required	<ul style="list-style-type: none"> ▪ $3019/200 = 15 \text{ Trees}$ ▪ NOTE: The applicant cites the unique nature of their business as a justification for not providing the required 	15 trees	Yes/No	A landscape deviation was granted for the lack of landscape islands in the vehicular storage area.

Item	Required	Proposed	Meets Code	Comments
	interior or perimeter trees.			
Perimeter Green space	<ul style="list-style-type: none"> ▪ 1 Canopy tree per 35 lf ▪ (2203)/35 = 63 trees ▪ - 12 trees deviation = 51 canopy trees required ▪ Maximum of 25% evergreens can be used for parking lot perimeter trees. 	<ul style="list-style-type: none"> ▪ 8 canopy trees ▪ 7 subcanopy trees (4.7 canopy equivalent) ▪ 20 evergreen trees 	No	<ol style="list-style-type: none"> 1. A landscape deviation was granted to not provide perimeter trees along the east drive. 2. Another landscape deviation was granted to not require perimeter trees along the south and east sides of the vehicular storage area.
Accessway perimeter	<ul style="list-style-type: none"> ▪ 1 canopy tree per 35 lf on each side of road, less widths of access drives. ▪ (xx lf)/35 = xx trees 	The accessway calculation was included in the overall parking lot perimeter calculation.	Yes	
Parking land banked	▪ NA	No		
Berms, Walls and ROW Planting Requirements				
Berms				
<ul style="list-style-type: none"> ▪ All berms shall have a maximum slope of 33%. Gradual slopes are encouraged. Show 1ft. contours ▪ Berm should be located on lot line except in conflict with utilities. ▪ Berms should be constructed with 6" of top soil. 				
Residential Adjacent to Non-residential (Sec 5.5.3.A) & (LDM 1.a)				
Berm requirements <i>(Zoning Sec 5.5.A)</i>	Landscaped berm 10-15 feet high required along south property line facing residential property.	<ul style="list-style-type: none"> ▪ A 7 foot tall berm is proposed for approximately 210 lf of the southeastern frontage. ▪ No berm is provided for the 150 lf west of the berm and east of the woods. ▪ A wetland, at least 500 lf and existing trees separate the houses to the south from the property line. ▪ Evergreens are provided along 		A landscape deviation was granted for the lack of the 10-15' tall berm as distance and the provided opaque fence, evergreens and existing trees will provide sufficient screening.

Item	Required	Proposed	Meets Code	Comments
		the entire south frontage to screen the residences to the south from view of the site.		
Planting requirements (LDM 1.a.)	LDM Novi Street Tree List	NA		
Adjacent to Public Rights-of-Way (Sec 5.5.B) and (LDM 1.b)				
Berm requirements (Zoning Sec 5.5.3.A.(5))	An undulating berm a minimum of 3 feet high with a 3 foot wide crest is required along Grand River.	Proposed berms are provided.	Yes	
Cross-Section of Berms (LDM 2.j)				
Slope, height and width	<ul style="list-style-type: none"> ▪ Label contour lines ▪ Maximum 33% ▪ Min. 3 feet flat horizontal area ▪ Minimum 3 feet high ▪ Constructed of loam with 6' top layer of topsoil. 	Yes	Yes	
Type of Ground Cover		Lawn		
Setbacks from Utilities	Overhead utility lines and 15 ft. setback from edge of utility or 20 ft. setback from closest pole	NA		
Walls (LDM 2.k & Zoning Sec 5.5.3.vi)				
Material, height and type of construction footing	Freestanding walls should have brick or stone exterior with masonry or concrete interior	No walls are proposed.		
Walls greater than 3 ½ ft. should be designed and sealed by an Engineer		NA		
ROW Landscape Screening Requirements (Sec 5.5.3.B. ii)				
Greenbelt width (2)(3) (5)	Parking: 25 ft. No Pkg: 25 ft	102 ft	Yes	
Min. berm crest width	None	No	No	
Minimum berm height (9)	None	No	No	
3' wall	(4)(7)	No		
Canopy deciduous or	▪ Adjacent to Parking: 1	6 trees – 4 existing	Yes	

Item	Required	Proposed	Meets Code	Comments
large evergreen trees Notes (1) (10)	tree per 40 lf ▪ (294-54)/40 = 6 trees	and 2 perimeter trees.		
Sub-canopy deciduous trees Notes (2)(10)	▪ Adjacent to Parking: 1 tree per 35 lf ▪ (294-54)/35=7 trees	7 trees	Yes	
Canopy deciduous trees in area between sidewalk and curb (<i>Novi Street Tree List</i>)	▪ Parking & No Parking: 1 tree per 45 lf ▪ xx/45 = x trees	0 trees	Yes	The widths of the RCOC clear vision zones leave no room for any street trees along Grand River. No deviation is required.
Non-Residential Zoning Sec 5.5.3.E.iii & LDM 1.d (2) Refer to Planting in ROW, building foundation landscape, parking lot landscaping and LDM				
Interior Street to Industrial subdivision (<i>LDM 1.d.(2)</i>)	<ul style="list-style-type: none"> ▪ 1 canopy deciduous or 1 large evergreen per 35 l.f. along ROW ▪ No evergreen trees closer than 20 ft. ▪ 3 sub canopy trees per 40 l.f. of total linear frontage ▪ Plant massing for 25% of ROW 	NA		
Screening of outdoor storage, loading/unloading (<i>Zoning Sec. 3.14, 3.15, 4.55, 4.56, 5.5</i>)	Storage area shall be completely screened from view of adjacent residential or commercial districts.	<ul style="list-style-type: none"> • 8 foot tall screening fence is provided around entire southern portion of vehicular use area. • Evergreen trees or existing woods are provided along all of the south boundary. • Evergreen trees are provided along west property line to screen lot from adjacent I-1 properties. • A 7 foot tall berm is provided along eastern 210' of southern frontage 	Yes	
Transformers/Utility boxes (<i>LDM 1.e from 1 through 5</i>)	<ul style="list-style-type: none"> ▪ A minimum of 2ft. separation between box and the plants ▪ Ground cover below 4" is allowed up to pad. 	While no transformers are shown, a note stating that transformers must be screened per	Yes	

Item	Required	Proposed	Meets Code	Comments
	<ul style="list-style-type: none"> No plant materials within 8 ft. from the doors 	the detail provided on L-1 has been added.		
Building Foundation Landscape Requirements (Sec 5.5.3.D)				
Interior site landscaping SF	<ul style="list-style-type: none"> Equals to entire perimeter of the building, less paved access points, x 8 with a minimum width of 4 ft. Main bldg.: 758 * 8 ft = 6064 sf Outbldg: 252 * 8 ft = 2016 sf 	<ul style="list-style-type: none"> 5091 sf front building 2385 sf outbuilding 	No/Yes	A deviation was granted for the deficiency in foundation landscaping as the applicant is improving an existing site.
<i>Zoning Sec 5.5.3.D.ii. All items from (b) to (e)</i>	If visible from public street a minimum of 60% of the exterior building perimeter should be covered in green space	It appears that 54% of the building facing Grand River is landscaped.	No	A deviation was granted for the deficiency in foundation landscaping as the applicant is improving an existing site.
Detention/Retention Basin Requirements (Sec. 5.5.3.E.iv)				
Planting requirements (Sec. 5.5.3.E.iv)	<ul style="list-style-type: none"> Clusters of large native shrubs shall cover 70-75% of the basin rim area 10" to 14" tall grass along sides of basin Refer to wetland for basin mix 	<ul style="list-style-type: none"> 73% of the detention pond rim is landscaped with 3 species of native shrubs The pond is shown as being seeded. 	Yes	
Phragmites Control (Sec 5.5.6.C)	<ul style="list-style-type: none"> Any and all populations of Phragmites australis on site shall be included on tree survey. Treat populations per MDEQ guidelines and requirements to eradicate the weed from the site. 	A note has been added stating that there is no Phragmites on the site.	Yes	
LANDSCAPING NOTES, DETAILS AND GENERAL REQUIREMENTS				
Landscape Notes – Utilize City of Novi Standard Notes				
Installation date (LDM 2.I. & Zoning Sec 5.5.5.B)	Provide intended date	Fall or Spring 2019 or 2020	Yes	
Maintenance & Statement of intent (LDM 2.m & Zoning Sec 5.5.6)	<ul style="list-style-type: none"> Include statement of intent to install and guarantee all materials for 2 years. 	Yes	Yes	

Item	Required	Proposed	Meets Code	Comments
	<ul style="list-style-type: none"> Include a minimum one cultivation in June, July and August for the 2-year warranty period. 			
Plant source (LDM 2.n & LDM 3.a.(2))	Shall be northern nursery grown, No.1 grade.	Yes	Yes	
Irrigation plan (LDM 2.s.)	A fully automatic irrigation system or a method of providing sufficient water for plant establishment and survival is required on Final Site Plans.	No		1. Please add irrigation plan or information as to how plants will be watered sufficiently for establishment and long- term survival. 2. If xeriscaping is used, please provide information about plantings included.
Other information (LDM 2.u)	Required by Planning Commission	NA		
Establishment period (Zoning Sec 5.5.6.B)	2 yr. Guarantee	Yes	Yes	
Approval of substitutions. (Zoning Sec 5.5.5.E)	City must approve any substitutions in writing prior to installation.	Yes	Yes	
Plant List (LDM 2.h.) – Include all cost estimates				
Quantities and sizes	Refer to LDM suggested plant list	Yes	Yes	Please provide plant list on Preliminary Site Plans
Root type		No	No	
Botanical and common names		<ul style="list-style-type: none"> Evergreen species exceed the diversity percentages of the Landscape Design Manual 15/22 species used (68%) are native to Michigan 	No/Yes	Please add Norway Spruce (<i>Picea abies</i>) to reduce the numbers of <i>Abies concolor</i> and <i>Picea glauca</i>.
Type and amount of lawn		Yes	Yes	
Cost estimate (LDM 2.t)		For all new plantings, mulch and sod as listed on the plan	Yes	Yes
Planting Details/Info (LDM 2.i) – Utilize City of Novi Standard Details				
Canopy Deciduous Tree	Refer to LDM for detail drawings	Yes	Yes	
Evergreen Tree		Yes	Yes	

Item	Required	Proposed	Meets Code	Comments
Multi-stem Tree		Yes	Yes	
Shrub		Yes	Yes	
Perennial/ Ground Cover		Yes	Yes	
Tree stakes and guys. (Wood stakes, fabric guys)		Yes	Yes	
Tree protection fencing	Located at Critical Root Zone (1' outside of dripline)	Yes	Yes	
Other Plant Material Requirements (LDM 3)				
General Conditions <i>(LDM 3.a)</i>	Plant materials shall not be planted within 4 ft. of property line	Yes	Yes	
Plant Materials & Existing Plant Material <i>(LDM 3.b)</i>	Clearly show trees to be removed and trees to be saved.	Sheet L-3	Yes	
Landscape tree credit <i>(LDM3.b.(d))</i>	Substitutions to landscape standards for preserved canopy trees outside woodlands/ wetlands should be approved by LA. Refer to Landscape tree Credit Chart in LDM	No		
Plant Sizes for ROW, Woodland replacement and others <i>(LDM 3.c)</i>	2.5" canopy trees 6' evergreen trees	Yes	Yes	
Plant size credit <i>(LDM3.c.(2))</i>	NA	No		
Prohibited Plants <i>(LDM 3.d)</i>	No plants on City Invasive Species List	None used		
Recommended trees for planting under overhead utilities <i>(LDM 3.e)</i>	Label the distance from the overhead utilities	<ul style="list-style-type: none"> ▪ Overhead lines are clearly indicated. ▪ Subcanopy trees are proposed beneath the lines. 	Yes	
Collected or Transplanted trees <i>(LDM 3.f)</i>		None		
Nonliving Durable Material: Mulch <i>(LDM 4)</i>	<ul style="list-style-type: none"> ▪ Trees shall be mulched to 3" depth and shrubs, groundcovers to 2" depth ▪ Specify natural color, finely shredded hardwood bark mulch. 	Yes	Yes	

Item	Required	Proposed	Meets Code	Comments
	Include in cost estimate. ■ Refer to section for additional information			

NOTES:

1. This table is a working summary chart and not intended to substitute for any Ordinance or City of Novi requirements or standards.
2. The section of the applicable ordinance or standard is indicated in parenthesis. For the landscape requirements, please see the Zoning Ordinance landscape section 5.5 and the Landscape Design Manual for the appropriate items under the applicable zoning classification.
3. Please include a written response to any points requiring clarification or for any corresponding site plan modifications to the City of Novi Planning Department with future submittals.

TRAFFIC REVIEW



AECOM
 27777 Franklin Road
 Southfield
 MI, 48034
 USA
 aecom.com

Project name:
 JSP18-0031 Keford Towing and Collision
 Preliminary Site Plan Traffic Review

From:
 AECOM

Date:
 October 9, 2019

To:
 Barbara McBeth, AICP
 City of Novi
 45175 10 Mile Road
 Novi, Michigan 48375

CC:
 Sri Komaragiri, Lindsay Bell, Kate Richardson,
 Madeleine Kopko, Victor Boron

Memo

Subject: JSP18-0031 Keford Towing and Collision Preliminary Site Plan Traffic Review

The preliminary site plan was reviewed to the level of detail provided and AECOM recommends **approval** for the applicant to move forward with the condition that the comments provided below are adequately addressed to the satisfaction of the City.

GENERAL COMMENTS

1. The applicant, Keford Collision and Towing is proposing to utilize the existing 23,493 SF building and the existing 5,703 SF building on the 7.61 acres parcel on the south side of Grand River Avenue, east of Taft Road for the operation of a towing and automobile collision service.
2. Grand River Avenue is under the jurisdiction of the Road Commission for Oakland County (RCOC).
3. The site is currently zoned I-1, Light Industrial, and the applicant is requesting an I-2 planned rezoning overlay.
4. Summary of traffic-related waivers/variances:
 - a. **The applicant was granted a waiver to install painted end islands in lieu of raised end islands.**
 - b. **The applicant was granted a waiver for the lack of landscape islands every 15 consecutive spaces.**

TRAFFIC IMPACTS

1. AECOM performed an initial trip generation estimate based on the ITE Trip Generation Manual, 10th Edition, as follows. The ITE Code (Automobile Care Center) is the most closely-related land use available in the ITE Trip Generation Manual, even though it does not match the use of Keford Towing specifically. *Note that the 5,703 SF building is not included in the trip generation since the land use for that building has not yet been confirmed.*

ITE Code: 942 – Automobile Care Center
 Development-specific Quantity: 23,493 GSF
 Zoning Change: N/A

Trip Generation Summary				
	Estimated Trips	Estimated Peak-Direction Trips	City of Novi Threshold	Above Threshold?
AM Peak-Hour Trips	53	35	100	No

PM Peak-Hour Trips	73	38	100	No
Daily (One-Directional) Trips	Not Available	N/A	750	No

- The number of trips does not exceed the City's threshold of 100 trips per either the AM or PM peak hour. AECOM recommends performing the following traffic impact study in accordance with the City's requirements.

Trip Impact Study Recommendation	
Type of Study:	Justification
Rezoning Traffic Impact Statement	As part of the PRO process, the applicant is required to submit a rezoning traffic impact study. The applicant submitted a rezoning traffic impact study on July 16, 2018. AECOM review of the traffic impact study is discussed in a separate letter.

EXTERNAL SITE ACCESS AND OPERATIONS

The following comments relate to the external interface between the proposed development and the surrounding roadway(s).

- The applicant is not proposing any modifications to the external site access points at this time.

INTERNAL SITE OPERATIONS

The following comments relate to the on-site design and traffic flow operations.

- General Traffic Flow
 - The applicant has generally indicated 24 foot aisles throughout the site.
 - The applicant is proposing an 18 foot wide, one-way emergency access drive along the east side of the building. The applicant should provide further detail regarding the gate and signing requirements and should work with the Fire Marshal regarding the need for gates at either end of the drive.
 - The applicant should indicate the manner in which the drive aisles in the unstriped rear gravel lot will be maintained for clear fire access.**
 - The applicant was granted a deviation to install painted end islands in lieu of raised end islands.**
 - The applicant has included dimensions for the widths of the proposed painted end islands throughout the site that are generally in compliance with City requirements as stated in Section 5.3.12 of the Zoning Ordinance.
 - Note that all end islands shall be constructed three (3) feet shorter than the adjacent parking space.
 - The radii of the painted end islands are generally in compliance with City standards, with the exception of the following.
 - The applicant should provide justification for the irregular raised end island near the building canopy on the south side of the site or update to meet City standards.
 - Additionally, there is a painted end island proposed near the south side of the auxiliary building within the gated area that has an outside radius of 10 feet. This should be updated to a minimum of 15 feet.

- f. The applicant has proposed a trash receptacle in the rear yard. The trash receptacle is located in a position that, while during pick-up periods, may diminish the ability for vehicles to exit the nearest parking space; however, it is not expected to diminish accessibility beyond acceptable levels.
 - g. The applicant has indicated a loading ramp location and a 10' by 25' loading zone on the site plan.
 - i. The applicant provided truck travel patterns throughout the site and confirmed accessibility to/from the loading zone.
 - h. There are proposed parking bays that have more than 15 consecutive parking spaces. **The applicant has been granted a deviation for the lack of landscape islands every 15 spaces in accordance with the City's Zoning Ordinance, section 5.5.3.C.ii.i.**
2. Parking Facilities
- a. Refer to the Planning Letter for information about parking requirements and calculations.
 - b. The applicant has indicated 17 foot long parking spaces abutting four inch curbs, and 19 foot long parking spaces in all other areas.
 - c. The applicant has provided the width of the barrier-free parking spaces and aisles, which are in compliance with City standards.
 - i. One of the barrier-free parking spaces in proposed to be behind the gated portion of the parking lot. The applicant could consider moving this space to be within the main parking lot area.
 - d. The applicant has proposed a barrier-free parking space along the western side of the property and should strongly consider relocating this parking space to be closer to the building entrance which it is serving. The applicant indicated that the grading of the site limits the ability to move this space.
 - i. The applicant should provide a ramp to the sidewalk next to this accessible parking space.
 - e. The applicant should review the curb heights throughout the site to generally provide 6" curbs for all landscape areas, except when placed directly in front of a 17' parking space where the curb should be 4".
 - i. The grading plan and details are generally in compliance with this; however, there are locations throughout the site where further clarification is needed, such as along the existing building to the south.
 - ii. Note that when a 17' space is provided, there must be a clear 2' area to accommodate the vehicle overhang. The applicant has provided a detail showing the 2' overhang.
 - f. The applicant has provided four (4) bicycle parking spaces.
 - i. The bicycle parking layout details are in conformance with Section 5.16.5 of the Zoning Ordinance.
 - ii. The sidewalk in front of the bicycle parking is 8 feet, which is in compliance with City standards.
 - iii. The applicant should ensure that there is a barrier between the bicycle parking spaces and the drive aisle, such as a curb.
3. Sidewalk Requirements
- a. The applicant should dimension the width of all proposed sidewalks.
 - i. Sidewalks throughout the site are required to be a minimum of 5' wide.
 - ii. Note that when a 17' parking space abuts a sidewalk, the sidewalk shall be 4" in height and a minimum of 7' wide to accommodate a 2' vehicle overhand and provide 5' of unobstructed travel way for non-motorized users.
 - b. The applicant has labeled sidewalk ramps on the plans and should include the latest Michigan Department of Transportation (MDOT) detail.

SIGNING AND STRIPING

- 1. All on-site signing and pavement markings shall be in compliance with the Michigan Manual on Uniform Traffic Control Devices (MMUTCD). The following is a discussion of the proposed signing and striping.
- 2. The applicant has included a sign quantity table.
- 3. The applicant should provide the following notes related to proposed signing.

Memo

- a. Traffic control signs shall use the FHWA Standard Alphabet series.
4. The applicant should provide notes and details related to proposed pavement markings.
 - a. Detail the pavement markings for the end islands and other hatched areas, including color, striping width, etc.
 - b. Detail the pavement markings for crosswalks, including color and striping width.
 - i. The applicant should consider adding crosswalk markings at the northeast sidewalk ramp to the sidewalk that leads to Grand River Avenue.
 - c. A detail has been provided for the international symbol for accessibility.

Should the City or applicant have questions regarding this review, they should contact AECOM for further clarification.

Sincerely,

AECOM



Patricia Thompson, EIT
Traffic Engineer



Josh A. Bocks, AICP, MBA
Senior Transportation Planner/Project Manager

WETLANDS REVIEW



ECT Project No. 190657-0100
 October 7, 2019

Ms. Barbara McBeth, AICP
 City Planner
 Community Development Department
 City of Novi
 45175 W. Ten Mile Road
 Novi, Michigan 48375

Re: Keford Collision & Towing (JSP18-0031)
 Wetland Review of the Preliminary Site Plan (PSP19-0142)

Dear Ms. McBeth:

Environmental Consulting & Technology, Inc. (ECT) has reviewed the Preliminary Site Plan for the proposed Keford Collision & Towing project prepared by Alpine Engineering, Inc. dated September 12, 2019 and stamped "Received" by the City of Novi Community Development Department on September 13, 2019 (Plan). The Plan was reviewed for conformance with the City of Novi Wetland and Watercourse Protection Ordinance and the natural features setback provisions in the Zoning Ordinance.

ECT currently recommends approval of the Preliminary Site Plan for Wetlands. The Applicant shall address the items noted in the *Wetland Comments* Section of this letter prior to receiving Wetland approval of the Final Site Plan.

Item	Required/Not Required/Not Applicable
Wetland Permit (specify Non-Minor or Minor)	Required (Non-Minor)
Wetland Mitigation	Not Required
Wetland Buffer Authorization	Required
EGLE Permit	To Be Determined. It is the applicant's responsibility to contact EGLE in order to determine the need for a wetland use permit.
Wetland Conservation Easement	Not Required

The proposed project is located south of Grand River Avenue and east of Taft Road in Section 15. The Plan proposes the construction of proposed pavement and asphalt improvements around two (2) existing buildings to remain, associated storm sewer, and a stormwater detention basin.

Based on our review of the application, Novi aerial photos, Novi GIS, the City of Novi Official Wetlands and Woodlands Maps (see Figure 1, attached), and our wetland verification site inspection conducted on July 25, 2018 it appears as if this proposed project site contains three (3) areas of on-site wetlands.

Wetland Evaluation

Environmental Consulting & Technology, Inc. (ECT) conducted a wetland evaluation for the proposed project site on July 25, 2018. ECT's in-office review of available materials included the City of Novi Regulated Wetland and Watercourse map (see Figure 1), USGS topographic quadrangle map, NRCS soils

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map, USFWS National Wetland Inventory map, and historical aerial photographs (from Oakland County). The applicant has also provided a Wetland Delineation map (Figure 2) prepared by King & MacGregor Environmental, Inc. dated July 16, 2018. As noted, three (3) wetlands have been delineated, but not all of these wetland areas are indicated on the City's Regulated Wetlands Map. Based on our review of this information the overall proposed project parcel contains areas mapped as City-Regulated Wetlands/Watercourses. The site appears to contain wetland/watercourse areas that are regulated by the City of Novi as well as the Michigan Department of Environment, Great Lakes, and Energy (EGLE; formerly MDEQ).

The focus of the site inspection was to review site conditions in order to determine whether City-regulated wetlands are found on-site. King & MacGregor Environmental, Inc. (KME) completed a wetland delineation for this site. The Wetland Delineation map (Figure 2) is dated July 16, 2018. Pink wetland boundary flagging was in place at the time of this site inspection. ECT reviewed the flagging and agrees that the wetland boundaries were accurately flagged in the field. Based on the existing vegetation and topography, it is ECT's assessment that the on-site wetlands have been accurately delineated on-site.

The following is a brief description of the on-site wetland features as provided on the original Wetland Delineation map (see Figure 2 provided by KME):

Wetland A – Scrub shrub wetland located in the southwest portion of the site. Wetland A is listed as less than 0.01-acre. The dominant wetland vegetation includes common buckthorn (*Rhamnus cathartica*), green ash (*Fraxinus pennsylvanica*), and Virginia creeper (*Parthenocissus quinquefolia*). This wetland is a small, isolated wetland.

Wetland B – Scrub-shrub and emergent wetland located on the eastern and southern portions of the site. This wetland extends off-site to the east and south, however the on-site portion is listed as 0.32-acres. The dominant wetland vegetation includes sedges (*Carex spp.*), reed canary grass (*Phalaris arundinacea*), cattails (*Typha spp.*), eastern cottonwood (*Populus deltoides*), and box elder (*Acer negundo*).

Wetland C – Scrub shrub wetland located in the western portion of the site but extends off-site to the west. Wetland C is listed as less than 0.01-acre. The dominant wetland vegetation is similar to that of Wetland A.

Wetland Impact Review

As noted above, several areas of wetland have been confirmed on the subject property by the applicant's wetland consultant (KME) and ECT. Currently, the Plan indicates two (2) direct impacts to on-site wetlands. The Plan quantifies the areas of the proposed wetland impacts on Sheet 3 (*Preliminary Grading Plan*). The total amount of direct (i.e., fill or excavation) impact to on-site wetlands currently indicated is 0.065-acre. The current impact to Wetland A in the southwest portion of the site (Impact 'B' on the *Preliminary Grading Plan*) is for the purpose of parking lot construction. The impact to Wetland B on the east side of the site (Impact 'A' on the *Preliminary Grading Plan*) is for the purpose of constructing parking area/loading ramp and the stormwater detention basin. The Plan also proposes the discharge of pre-treated stormwater runoff to Wetland B on the east side of the site from the proposed pre-treatment detention basin.

The following table summarizes the proposed wetland impacts as listed on the *Preliminary Grading Plan* (Sheet 3):

Table 1. Proposed Wetland Impacts

Impact Area	Wetland Impacted	City Regulated?	MDEQ Regulated?	Impact Area (square feet)	Impact Area (acres)	Impact Volume (cubic yards)
A	B	Yes City Regulated /Essential	Likely	1,841	0.04	120
B	A	Yes City Regulated /Essential	Likely	973	0.02	15
C	C	Yes City Regulated /Essential	Likely	None	None	None
TOTAL	--	--	--	2,814	0.06	135

In addition to the proposed wetland impacts, the Plan proposes disturbance to on-site 25-foot wetland buffer areas. The applicant is urged to minimize impacts to all wetlands and 25-foot wetland setback areas to the greatest extent practicable. The City regulates wetland and watercourse buffers/setbacks. Article 24, Schedule of Regulations, of the Zoning Ordinance states that:

“There shall be maintained in all districts a wetland and watercourse setback, as provided herein, unless and to the extent, it is determined to be in the public interest not to maintain such a setback. The intent of this provision is to require a minimum setback from wetlands and watercourses”.

The proposed wetland buffer impacts appear to be to the entire setback of Wetland A in the southwestern portion of the site, a large portion of the Wetland C buffer on the western side of the site, and portion of the Wetland B buffer on the eastern side of the site. The Applicant shall indicate whether all proposed impacts to the 25-foot wetland buffer are permanent or temporary on subsequent plan submittals. All impacts on the *Preliminary Grading Plan* (Sheet 3) appear to be permanent, however a Low Prairie Seed Mix is proposed on the *Landscape Plan* (Sheet L-1). It is not clear where this particular seed mix is proposed to be installed.

The following table summarizes the proposed wetland setback impacts as listed on the Plan:

Table 2. Proposed 25-Foot Wetland Buffer Impacts

Impact Area	Wetland Buffer Impacted	Wetland Buffer Area		Buffer Impact Area		Purpose of Impact
		Square Feet	Acre	Square Feet	Acre	
A	B	6,194	0.14	4,987	0.11	Parking lot, loading ramp detention basin
B	A	5,464	0.13	5,464	0.13	Parking lot
C	C	1,992	0.05	1,598	0.04	Grading for storm sewer inlet
TOTAL		13,650	0.32	12,049	0.28	--

Regulatory Status - EGLE

ECT has evaluated the on-site wetlands and believes that they are all considered to be essential/regulated by the City of Novi as they meet one or more of the essentiality criteria (i.e., functions and values) outlined in the City of Novi Wetland and Watercourse Protection Ordinance and regulated by EGLE. As noted, the wetlands appear to accurately flagged in the field and appear to be generally indicated accurately on the *Wetland Delineation Map* provided by KME (Figure 2, attached).

EGLE generally regulates wetlands that are within 500 feet of an inland lake, pond, or stream, or within 1,000 feet of a Great Lake, Lake St. Clair, the St. Clair River, or the Detroit River. Isolated wetlands five (5) acres in size or greater are also regulated. EGLE may also exert regulatory control over isolated wetlands less than five acres in size "...if the department determines that protection of the area is essential to the preservation of the natural resources of the state from pollution, impairment, or destruction and the department has notified the owner". It appears as if a tributary to the Walled Lake Branch of the Middle Rouge River may be enclosed and flow through this site within an existing 30-inch stormsewer (appears to connect Wetlands B and C). It is the applicant's responsibility to contact EGLE in order to confirm the regulatory authority with respect to the on-site wetland areas. In a *Pre-Application Meeting Response to Review* letter dated September 12, 2019, the applicant's engineer notes that the wetland permitting is currently in process with EGLE.

Regulatory Status – City of Novi

The City of Novi Wetland and Watercourse Protection Ordinance (City of Novi Code of Ordinances, Part II, Chapter 12, Article V.; Division 2.) describes the regulatory criteria for wetlands and review standards for wetland permit applications. The City of Novi regulates wetlands that are: (1) contiguous to a lake, pond, river or stream, as defined in Administrative Rule 281.921; (2) two (2) acres in size or greater; or (3) less than two (2) acres in size but deemed essential to the preservation of the natural resources of the city under the criteria set forth in subsection 12-174(b). Wetlands deemed regulated by the City of Novi require the approval of a use permit for any proposed impacts to the wetland.

ECT has evaluated the areas of on-site wetland and believes that each wetland is regulated by the City's Wetland and Watercourse Protection Ordinance because all on-site wetlands appear to be either located within 500-feet of a regulating stream/drain or extend offsite and are 2 acres in size or greater.

It should be noted that in those cases where an activity results in the impact to wetland areas of 0.25-acre or greater that are deemed essential under City of Novi Ordinance subsection 12-174(b) mitigation shall be required. The applicant shall submit a mitigation plan which provides for the establishment of replacement wetlands at a ratio of 1:1 through 2:1 times the area of the natural wetland impaired or destroyed, if impacts meet or exceed the 0.25-acre threshold. In general, the MDEQ's threshold for the requirement of wetland mitigation is 0.3-acre of wetland impacts. Wetland mitigation does not appear to be a requirement of the current Plan.

As noted above, any proposed use of the wetlands will require a City of Novi *Wetland Use Permit* as well as an *Authorization to Encroach the 25-Foot Natural Features Setback* for any proposed impacts to the 25-foot wetland buffers. The applicant is urged to minimize impacts to on-site wetlands and wetland setbacks to the greatest extent practicable. The City regulates wetland buffers/setbacks. Article 24, Schedule of Regulations, of the Zoning Ordinance states that:

“There shall be maintained in all districts a wetland and watercourse setback, as provided herein, unless and to the extent, it is determined to be in the public interest not to maintain such a setback. The intent of this provision is to require a minimum setback from wetlands and watercourses”.

Finally, as proposed, the project will require a City of Novi Non-Minor Use wetland permit. The granting or denying of nonresidential minor use permits shall be the responsibility of the Community Development Department. A nonresidential minor use permit is a permit for activities consisting of no more than one (1) of the following activities which have a minimal environmental effect:

- a. Minor fills of three hundred (300) cubic yards or less and not exceeding ten thousand (10,000) square feet in a wetland area, providing the fill consists of clean, nonpolluting materials which will not cause siltation and do not contain soluble chemicals or organic matter which is biodegradable, and providing that any upland on the property is utilized to the greatest degree possible. All fills shall be stabilized with sod, or seeded, fertilized and mulched, or planted with other native vegetation, or riprapped as necessary to prevent soil erosion.
- b. Installation of a single water outfall provided that the outlet is riprapped or otherwise stabilized to prevent soil erosion.
- c. Watercourse crossings by utilities, pipelines, cables and sewer lines which meet all of the following design criteria:
 - i. The method of construction proposed is the least disturbing to the environment employable at the given site;
 - ii. The diameter of pipe, cable or encasement does not exceed twenty (20) inches;
 - iii. A minimum of thirty (30) inches of cover will be maintained between the top of the cable or pipe and the bed of the stream or other watercourse on buried crossings; and
 - iv. Any necessary backfilling will be of washed gravel.
- d. Extension of a wetland/watercourse permit previously approved by the Planning Commission.
- e. Replacement of a culvert of an identical length and size, and at the same elevation. If the proposed culvert is of a greater length or size than the existing culvert, or is a new culvert

altogether, it must meet the conditions of subpart c., above, to qualify for a nonresidential minor use permit.

- f. Temporary impacts where the encroachment into protected areas is less than five hundred (500) feet.

Because the project contains a proposed stormwater outfall as well as two (2) direct impacts to wetlands, a Non-Minor Wetland Permit (and approval of Planning Commission) shall be required.

Wetland and Watercourse Comments

The following are repeat comment from our Wetland Review of the PRO Concept Plan (PSP18-0107) letter dated August 7, 2018. The current status of each comment follows in *bold italics*. ECT recommends that the Applicant address the items noted below in subsequent site plan submittals:

1. The wetland and wetland buffer boundaries indicated on the Plan appear to be approximate. Subsequent site plan submittals shall include the actual, surveyed wetland boundary information that appears to be included on the Wetland Delineation map provided by KME (Figure 2). The wetland flag numbers shall also be provided on the Plan.

This comment has been satisfactorily addressed. This information has been included on the Topographic Survey (Sheet 2).

2. It is unclear if the proposed site work will impact Wetland C, however it does not appear likely. The boundaries of Wetland C are not currently shown on Sheet 1. This information should be provided/clarified on subsequent site plan submittals.

This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan (Sheet 3).

3. The applicant shall indicate, quantify and label all existing areas of wetland and 25-foot wetland buffers (square feet or acres) on the Plan.

This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan (Sheet 3).

4. The applicant shall indicate, quantify and label all proposed impacts to the wetlands (square feet or acres) including proposed volume of cut/fill (cubic feet or cubic yards).

This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan (Sheet 3).

5. The applicant shall indicate, quantify and label all proposed impacts to the 25-foot wetland setbacks (square feet or acres).

This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan (Sheet 3).

6. It appears as though a MDEQ Wetland Permit and a City of Novi Non-Minor *Wetland Use Permit* would be required for any proposed impacts to on-site wetlands, if applicable. A City of Novi *Authorization to Encroach the 25-Foot Natural Features Setback* would be required for any proposed impacts to on-site 25-foot wetland or watercourse buffers.

This comment has been partially addressed. In a Pre-Application Meeting Response to Review letter dated September 12, 2019, the applicant's engineer notes that the wetland permitting is currently in process with EGLE. The Applicant should provide a copy of the EGLE Wetland Use Permit application to the City (and our office) for review and a copy of the approved permit upon issuance. A City of Novi Wetland Permit cannot be issued prior to receiving this information.

7. It should be noted that it is the Applicant's responsibility to confirm the need for a Permit from the MDEQ for any proposed wetland or floodplain impacts. Final determination as to the regulatory status of any on-site wetlands (if applicable) shall be made by MDEQ. The Applicant should provide a copy of the MDEQ Wetland Use Permit application to the City (and our office) for review and a copy of the approved permit upon issuance. A City of Novi Wetland Permit cannot be issued prior to receiving this information.

This comment has been partially addressed. See Comment No. 6, above.

8. The Plan should address how any temporary impacts to wetland or 25-foot wetland buffers shall be restored, if applicable. Subsequent Plan submittals shall include specifications for any proposed seed mixes proposed for use within these areas. Sod or common grass seed will not be acceptable to restore temporary impacts to wetlands or 25-foot wetland buffers.

This comment has not been addressed. All impacts on the Preliminary Grading Plan (Sheet 3) appear to be permanent, however a Low Prairie Seed Mix is proposed on the Landscape Plan (Sheet L-1). It is not clear where this particular seed mix is proposed to be installed. Please review and revise the Plan as necessary.

9. The applicant should ensure that any proposed snow storage areas are located such that any runoff will not directly affect any on-site wetlands, or the Walled Lake Branch of the Middle Rouge River (if applicable).

This comment has been satisfactorily addressed. The proposed snow storage areas have been indicated on the Landscape Plan (Sheet 1). It appears as though the runoff from the snow melt will be routed through the proposed detention basin prior to discharge to existing Wetland B on the eastern side of the site.

10. ECT suggests that any proposed stormwater management plan be reviewed by the City of Novi Engineering Department to ensure that they meet the City of Novi design requirements.

This comments still applies.

Keford Collision & Towing (JSP18-0031)
Wetland Review of the Preliminary Site Plan (PSP19-0142)
October 7, 2019
Page 8 of 12

Wetland Conclusion

The project site appears to contain wetlands/watercourse that are regulated by both the City of Novi and EGLE. Any proposed impacts to on-site wetlands will require a permit from EGLE, a City of Novi *Wetland and Watercourse Non-Minor Use Permit*, and an *Authorization to Encroach the 25-Foot Natural Features Setback* for any proposed impacts to the 25-foot wetland buffers.

Recommendation

ECT currently recommends approval of the Preliminary Site Plan for Wetlands. The Applicant shall address the items noted in the *Wetland Comments* Section of this letter prior to receiving Wetland approval of the Final Site Plan.

If you have any questions regarding the contents of this letter, please contact us.

Respectfully submitted,
ENVIRONMENTAL CONSULTING & TECHNOLOGY, INC.



Pete Hill, P.E.
Senior Associate Engineer

cc: Lindsay Bell, City of Novi Planner
Sri Komaragiri, City of Novi Planner
Madeleine Kopko, City of Novi Planning Assistant
Rick Meader, City of Novi Landscape Architect

Attachments: Figure 1 – City of Novi Regulated Wetland and Woodland Map
Figure 2 – Wetland Delineation Map
Site Photos

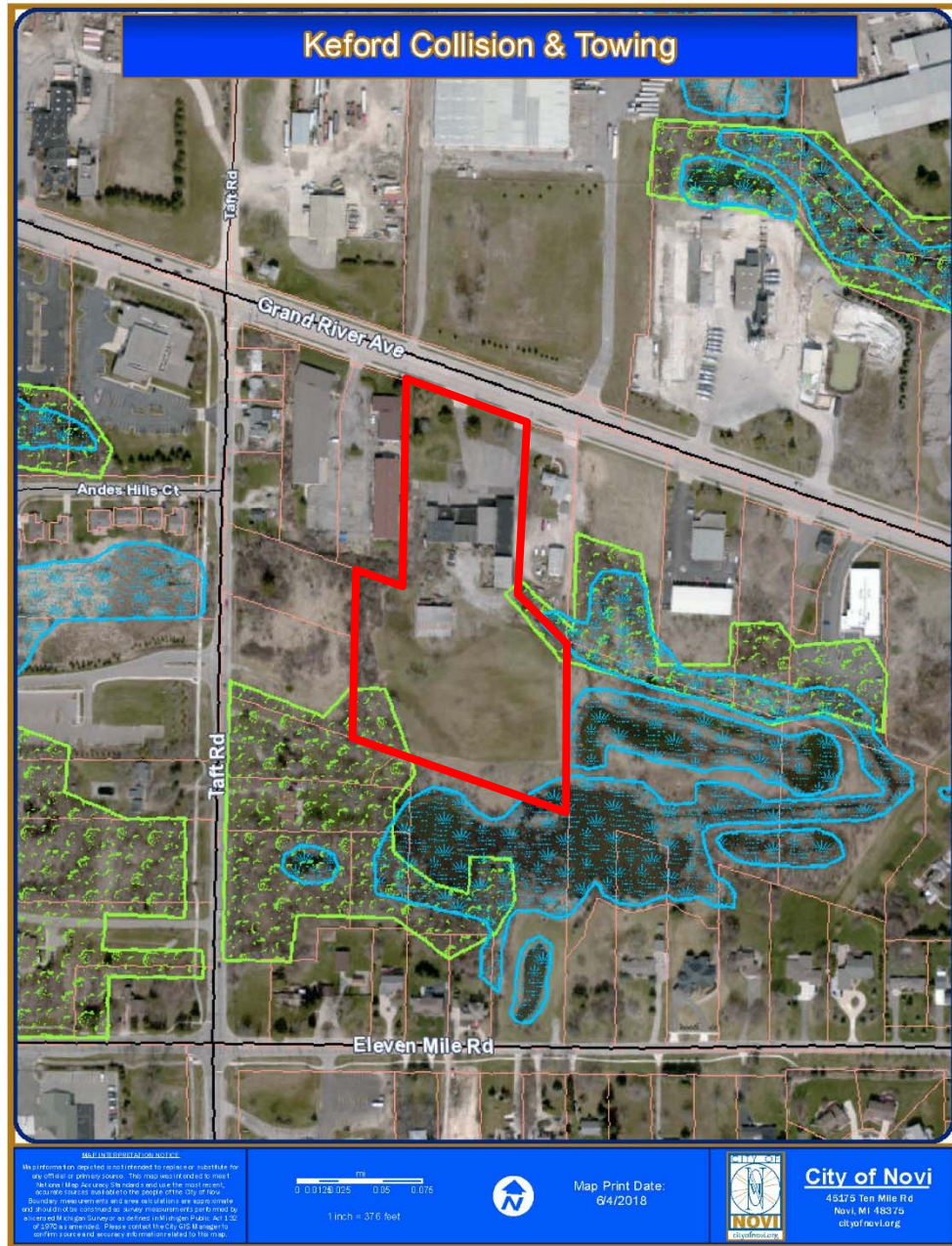
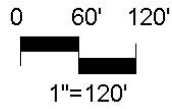


Figure 1. City of Novi Regulated Wetland & Woodland Map (approximate parcel boundary shown in red). Regulated Woodland areas are shown in green and Regulated Wetland areas are shown in blue.



Please be advised the information provided by KME, Inc. regarding wetland boundaries is an estimate of the wetland boundary. The ultimate decision on wetland boundary locations and jurisdictions thereof rests with the MDEQ and, in some cases, the Federal government. As a result, there may be adjustments to boundaries based upon review of a regulatory agency. An agency determination can vary, depending on various factors including, but not limited to, experience of the agency representative making the determination and the season of the year. In addition, the physical characteristics of the site can change with time, depending on the weather, vegetation patterns, drainage and management activities on adjacent parcels or other events. Any of these factors can change the nature/extent of wetlands on site. This wetland determination, as defined by the boundary flags depicted on this drawing, is valid for one growing season from the date flagged. There is no assurance given herein or otherwise implied that the KME, Inc. wetland boundary will be accepted by any regulatory agency. Reliance on KME, Inc.'s opinion is at the client's risk. Further, it has been our experience that site conditions are likely to change over the course of one year. Therefore, KME, Inc. strongly recommends that the client have no reliance on our opinion after one growing season. Be aware the manufacturer of the G.P.S. (Global Positioning System) used by KME, Inc. has advised that the equipment has, at best, sub-meter accuracy. The location of the actual wetland boundaries may therefore vary somewhat if a professional survey of the wetland flags is conducted.



PROJECT NUMBER 18199	SHEET NAME Wetland Boundary Map	SHEET NUMBER 1	DATE 07/16/18	DRAWN BY MM	CHECKED BY MM	SCALE AS SHOWN	PROJECT TITLE Wetland Delineation For: 45237 Grand River Avenue City of Novi, Oakland County, Michigan		

Figure 2. Wetland Delineation Map (provided by King & MacGregor Environmental, Inc., dated July 16, 2018).

Site Photos



Photo 1. Looking south at Wetland B (near flag B-12) along the eastern portion of the site (ECT, July 25, 2018).



Photo 2. Looking southeast at open water area of Wetland B located off-site to the south (ECT, July 25, 2018).



Photo 3. Looking west at scrub-shrub Wetland B located in the southwest portion of the site (ECT, July 25, 2018).



Photo 4. Looking north towards Wetland C located in the western portion of the site (ECT, July 25, 2018).

FAÇADE REVIEW



October 17, 2019

City of Novi Planning Department
 45175 W. 10 Mile Rd.
 Novi, MI 48375- 3024

Façade Review Status Summary:
Approved - Full compliance.
Section 9 Waiver not required.

Re: FACADE ORDINANCE REVIEW – Preliminary Site Plan
Keford Collision & Towing, JSP18-31, PSP18-0107
 Façade Region: 1, Zoning District: I-1,

Dear Ms. McBeth;

The following is the Façade Review for the Preliminary Site Plan of the above referenced project. This review is based on the drawings dated 10/17/19, prepared Cityscape Architects of Novi, Michigan. A sample board with renderings and proposed colors was also provided. The percentages of materials proposed for each façade are as shown on the table below. The maximum percentages of materials allowed by Façade Ordinance are shown in the right hand column. Note that the façade materials on the side and rear elevations are not proposed to be changed.

	North (Front)	East	West	South	Ordinance Maximum (Minimum)
Brick (existing 8" x 8" natural fired clay tile)	47%	UN	UN	UN	30% Minimum
Tile (Proposed 8" x 8", blue color)	25%	UN	UN	UN	25%
Flat Metal (Canopy fascia)	3%	UN	UN	UN	50%
Flat Metal (horizontal louver feature, "real red")	25%	UN	UN	UN	50%
UN - Unaltered					

Recommendation – This project is considered a façade alteration as regulated by Section 5.15.6 of the Ordinance. The applicant has revised the color of the proposed louver feature to a more subdued tone (BM-1315). The color is now consistent with Section 5.15.2 of the Façade Ordinance which prohibits intense colors. The existing 8” x 8” tile is a natural fired clay product. This material may be considered brick with respect to the Ordinance, providing that it is not painted. Although the façade materials on the side and rear facades are not proposed to be altered these facades are proposed to be painted to generally match the color of front façade (BM1616). Therefore, it is our recommendation that the design is consistent with the Façade Ordinance and the PRO Agreement. A Section 9 Waiver is not required for this project.

Notes to the Applicant:

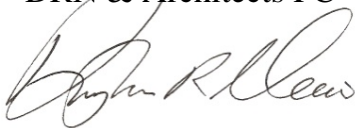
1. It should be noted that all roof top equipment must be screened from view from all vantage points both on-site and off-site using materials in compliance with the Façade Ordinance.

2. Inspections – The Façade Ordinance requires inspection(s) for all projects. Materials displayed on the approved sample board (in this case the adjacent existing material) will be compared to materials to be installed. It is the applicant’s responsibility to request the inspection of each façade material at the appropriate time. Inspections may be requested using the Novi Building Department’s Online Inspection Portal with the following link. Please click on “Click here to Request an Inspection” under “Contractors”, then click “Façade”.

<http://www.cityofnovi.org/Services/CommDev/OnlineInspectionPortal.asp>.

If you have any questions regarding this review, please do not hesitate to call.

Sincerely,
DRN & Architects PC

A handwritten signature in black ink, appearing to read 'Douglas R. Necci', written over a faint circular stamp.

Douglas R. Necci, AIA

FIRE REVIEW



September 24, 2019

TO: Barbara McBeth- City Planner
Sri Ravali Komaragiri- Plan Review Center
Lindsay Bell-Plan Review Center
Madeleine Kopko-Planning Assistant

CITY COUNCIL

Mayor
Bob Gatt

Mayor Pro Tem
Dave Staudt

Andrew Mutch

Laura Marie Casey

Kelly Breen

Ramesh Verma

Doreen Poupard

City Manager
Peter E. Auger

Director of Public Safety
Chief of Police
David E. Molloy

Director of EMS/Fire Operations
Jeffery R. Johnson

Assistant Chief of Police
Erick W. Zinser

Assistant Chief of Police
Scott R. Baetens

RE: Keford Towing

Project Description:

Build a 23493 S.Q.F.T. structure off of Grand River.

Comments:

- **All** fire hydrants **MUST** be installed and operational prior to any combustible material is brought on site. **IFC 2015 3312.1**
- Water-main sizes and fire hydrant locations **MUST** be put on the plans for review.
- **MUST** provide a Knox Lock for the gates X2.
- **Existing fire lane Corrected**. Fire access lane on the east side of the structure **MUST** be 20' wide. NOT 18.6'. **IFC 2015 503.2.1**
- Turning radius doesn't meet city standards, 50' outside and 30' inside turning radius. Locations are northwest corner and southeast corner of the structure.

Recommendation:

APPROVED WITH CONDITIONS

Sincerely,

A handwritten signature in black ink, appearing to read 'KSP', with a horizontal line extending to the right.

Kevin S. Pierce-Fire Marshal
City of Novi – Fire Dept.

cc: file

Novi Public Safety Administration
45125 Ten Mile Road
Novi, Michigan 48375
248.348.7100
248.347.0590 fax

cityofnovi.org

APPLICANT RESPONSE LETTER



46892 West Road, Suite 109
Novi, Michigan 48377
Phone: (248) 926-3701
Fax: (248) 926-3765
Web: www.alpine-inc.net

October 22, 2019

via email

Sri Ravali Komaragiri
City of Novi Community Development Department
45175 West 10 Mile Road
Novi, MI 48375

**Re: Keford Towing
Response to PSP Review Comments
Alpine Engineering Inc. Project #17-504
JSP18-31**

Dear Sri

On behalf of our client, Keford Towing, please find the following information for your distribution and review:

- PDF copy of the Preliminary Site Plan (dated 09-12-2019)
- PDF copy of the Demarcation Plan (dated 10-22-2019)

Please find the following responses to the reviews received on October 11, 2019 via email for the above referenced project.

PLANNING REVIEW LETTER (DATED OCTOBER 7, 2019)

- 1. Comment:** Please add the PRO conditions and deviations listed in this letter on the site plan sheet for reference. Please refer to additional comments that need to be addressed to comply with the PRO Conditions listed in next section.

Response: The PRO conditions and deviations will be listed on the plans for the Final Site Plan submittal.
- 2. Comment:** It appears that there is no proposed use or a prospective tenant for the out building. The applicant should provide an update. A Planning Commission approval is required for any proposed use. The applicant should contact Community Development department once a prospective tenant is determined.

Response: The Applicant will provide this information when it is determined.
- 3. Comment:** The plan should clearly label the 10 dedicated spaces for car rental, dedicated spaces for staging.

Response: Additional notation will be provided to provide clarity for the dedicated parking spaces.
- 4. Comment:** Please provide the photometric information on building facades. In addition to the site photometric plan.

Response: Additional photometric information will be provided on the Final Site Plan.
- 5. Comment:** Indicate what lights will be turned on past hours of operation for security reasons. A separate photometric plan is required for security lights only.

Response: Additional photometric information will be provided on the Final Site Plan.
- 6. Comment:** It appears that there is no proposed use or a prospective tenant for the out building. The applicant should provide an update.

Response: The Applicant will provide this information when it is determined.
- 7. Comment:** It appears that there is name change to the business. It is unclear if the ownership changed as well. The applicant should clarify.

Response: The Applicant will provide this information.
- 8. Comment:** The plans do not show clear demarcation as required. Refer to Traffic review for more details.

Response: A Demarcation Plan has been included with this submittal for review.

PLANNING REVIEW CHART (DATED OCTOBER 7, 2019)

9. **Comment:** *The applicant should contact Community Development department once a prospective tenant is determined.*
Response: Understood.
10. **Comment:** Clearly label rental car parking on the plan. The applicant also referred to moving them inside for repair. Please clearly label dedicated spaces used for staging.
Response: The rental car parking including spaces proposed for staging will be labeled on the Final Site Plan.
11. **Comment:** Review to the traffic review for more comments.
Response: Refer to the traffic review responses below.
12. **Comment:** Provide dumpster elevations to verify conformance.
Response: A dumpster elevation will be provided on the Final Site Plan.
13. **Comment:** Exterior Lighting. Refer to comments provided later in the chart.
Response: Refer to the photometrics review responses below.
14. **Comment:** Refer to traffic review for more comments.
Response: Refer to the traffic review responses below.
15. **Comment:** A sign permit is required. For sign permit information contact Ordinance at 248-347-0438.
Response: Understood.
16. **Comment:** Please provide the photometric information on building facades. In addition the site photometric plan.
Response: Additional photometrics information will be provided on the Final Site Plan.
17. **Comment:** Hours of operation not provided.
Response: The hours of operation will be provided on the Final Site Plan.
18. **Comment:** Specification sheets for all proposed lighting are not provided. Please provide as required.
Response: The cut sheets for the proposed lights will be provided on the Final Site Plan.
19. **Comment:** Please add these notes to photometric sheet P-1.
Response: The notes indicated will be added on the Final Site Plan.
20. **Comment:** Indicate what lights will be turned on past hours of operation for security reasons. A separate photometric plan is required for security lights only.
Response: Additional information regarding security lighting will be provided on the Final Site Plan.

ENGINEERING REVIEW (DATED OCTOBER 10, 2019)

1. **Comment:** *A right-of-way permit will be required from the City of Novi for work in the Grand River Avenue right-of-way.*
Response: A City of Novi right-of-way permit will be applied for at the time of the Final Site Plan and a note indicating this is located on the plans. Refer to Note #6 on the "Preliminary Site Plan".
2. **Comment:** *A right-of-way permit will also be required from the Road Commission for Oakland County (RCOC) for work in the Grand River Avenue right-of-way.*
Response: An Oakland County right-of-way permit will be applied for at the time of the Final Site Plan and a note indicating this is located on the plans. Refer to Note #6 on the "Preliminary Site Plan".
3. **Comment:** *The Non-domestic User Survey form shall be submitted to the City so it can be forwarded to Oakland County.*
Response: Per discussion with the Planning Department on September 9th, 2019, the Non-domestic User Survey will be provided with the Final Site Plan submittal.
4. **Comment:** *Show and label the existing 50-foot half right-of-way for Grand River Avenue.*
Response: The existing 50-foot half right-of-way line will be shown and labeled on the Final Site Plan.
5. **Comment:** *A license agreement will be required for fencing proposed within existing sanitary sewer easement.*
Response: Understood. We have contacted the City Engineering Department to obtain the process for acquiring this agreement.
6. **Comment:** *The applicant should be aware that additional hydrants may be required per Fire Department review, given the lack of existing hydrants on-site and the distance of the nearest existing hydrant northeast of the site in the Grand River Avenue right-of-way.*
Response: Understood.

7. **Comment:** *The Storm Water Management Plan for this development shall be designed in accordance with the Storm Water Ordinance and Chapter 5 of the new Engineering Design Manual.*
Response: Understood.
8. **Comment:** *The storm water management plan proposes to maintain an existing condition of site drainage going into the Grand River right-of-way. Review and approval by the Road Commission for Oakland County will be required, and a variance from the Design and Construction Standards is required in any case where all drainage is not captured on-site.*
Response: Understood, the plans will be submitted to the RCO. We respectfully request a variance from the Design and Construction Standards for maintaining existing site drainage draining to the Grand River Avenue Right-of-Way.
9. **Comment:** *Restricted discharge to an off-site regional detention basin is proposed. Bankfull storage will be provided on-site. Any applicable storm water detention tap fees will be pro-rated for bankfull detention storage provided on the site.*
Response: No comment.
10. **Comment:** *Provide a soil boring in the vicinity of the basin to determine soil conditions and to establish the high water elevation of the groundwater table.*
Response: A soil boring in the proposed basin area will be submitted at the time of the Final Site Plan submittal.
11. **Comment:** *One foot of freeboard shall be provided above the uppermost storage elevation, rather than the 0.4 foot shown on the plans.*
Response: The plans currently show one (1) foot of freeboard. Additional information will be provided on the Final Site Plan to provide clarity.
12. **Comment:** *A 25-foot vegetated buffer shall be provided around the storm water basin where any pavement runoff is directed toward the basin.*
Response: A twenty-five (25)-ft vegetated buffer has been provided along the west side of the basin. Refer to the "Preliminary Storm Water Management Plan".
13. **Comment:** *The maintenance access route to the basin outlet structure shall be a minimum of 15 feet wide for its entirety, including the proposed rip-rap and curb drop.*
Response: This information will be provided on the Final Site Plan.
14. **Comment:** *Provide a 5-foot wide stone bridge/access route allowing direct access to the standpipe from the bank of the basin during high-water conditions (i.e. stone 6-inches above high water elevation).*
Response: This information will be provided on the Final Site Plan.
15. **Comment:** *Provide an access easement for maintenance over the storm water detention system and the pretreatment structure. Also, include an access easement to the detention area from the public road right-of-way.*
Response: An access easement for storm water detention system maintenance will be provided at the time of the Final Site Plan submittal.
16. **Comment:** *Provide manufacturer's details and sizing calculations for the pretreatment structure within the plans. Provide drainage area and runoff coefficient calculations specific to the area tributary to the treatment structure. The treated flow rate should be based on the 1-year storm event intensity (~1.6 in/hr). Higher flows shall be bypassed.*
Response: Currently, the design is utilizing a wet detention basin to accommodate water quality treatment. Additional information regarding the basin will be provided on the Final Site Plan.
17. **Comment:** *Provide release rate calculations for the two design storm events addressed on this site (first flush and bankfull).*
Response: Additional calculations will be provided on the Final Site Plan.
18. **Comment:** *Due to maintenance concerns, each restricting orifice in the control structure shall be a minimum of 1 square-inch in size, even though this may result in a flow rate above that calculated.*
Response: Additional detail for the outlet control structures will be provided on the Final Site Plan.
19. **Comment:** *In consideration of the City's waiver of the ordinance requirements that the rear storage are be paved. Developer shall:*
 - a. **Comment:** *Keep the access aisle as shown on the PRO plan free from parked vehicles or other obstructions so that there is fire truck access at all times (with the details of the Developer's plan to*

designate or demarcate the access aisle shown on the final approved site plan);

Response: A Demarcation Plan has been provided for the PC meeting as requested by the Planning Department.

- b. **Comment:** Undertake regular maintenance of the gravel storage areas so as to prevent the migration of the gravel storage area to other areas of the waterbodies. To ensure compliance with these requirements, the City shall have a reasonable right to enter onto the Land or Development for purposes of inspection; provided, however, the City shall give 48 hours' notice of any expected inspection. If the City determines that corrective action is required to be taken, it shall issue a notice of corrective action, which shall include a time period for correction. Failure to comply shall be treated as a breach of this Agreement.

Response: Understood.

20. **Comment:** Design and Construction waivers of Section 11-239 of the City's Zoning Ordinance were authorized:

- a. **Comment:** To allow gravel parking for storage yard in the rear

Response: No comment.

- b. **Comment:** For lack of curb and gutter within the rear yard storage area

Response: No comment.

- c. **Comment:** For lack of parking lot striping.

Response: No comment.

21. **Comment:** Two of the proposed barrier free ramps appear to exceed the absolute maximum longitudinal slope of 8.33%. These ramps are (1) the longest ramp at the northwest corner of the main existing building, and (2) the short ramp at the northwest corner of the paved parking lot.

Response: Additional detail will be provided on the Final Site Plan.

22. **Comment:** Verify and label the slopes along the ingress/egress routing to the building from the barrier-free stalls. All barrier-free stalls shall comply with Michigan Barrier-Free regulations.

Response: Additional detail will be provided on the Final Site Plan.

23. **Comment:** Detectable warning plates are required at all barrier free ramps, hazardous vehicular crossings and other areas where the sidewalk is flush with the adjacent drive or parking pavement. The barrier-free ramps shall comply with current MDOT specifications for ADA Sidewalk Ramps. Provide the latest version of the MDOT standard detail for detectable surfaces.

Response: Additional detail will be provided on the Final Site Plan.

24. **Comment:** Label specific ramp locations on the plans where the detectable warning surface is to be installed.

Response: Additional detail will be provided on the Final Site Plan.

25. **Comment:** Specify the product proposed and provide a detail for the detectable warning surfaces for barrier free ramps. The product shall be the concrete warning surface for barrier free ramps. The product shall be the concrete embedded detectable warning plates, or equal, and shall be approved by the Engineering Division. Stamped concrete will not be acceptable.

Response: Additional detail will be provided on the Final Site Plan.

26. **Comment:** Provide proposed contours for areas surrounding the rear yard gravel lot.

Response: Additional detail will be provided on the Final Site Plan.

27. **Comment:** Clarify all proposed contours by labeling contour elevations.

Response: Additional detail will be provided on the Final Site Plan.

28. **Comment:** Add pavement and curb grades to proposed parking along existing south building.

Response: Additional detail will be provided on the Final Site Plan.

29. **Comment:** Curbing and walks adjacent to the end of 17-foot stalls shall be reduced to 4-inches high (rather than the standard 6-inch heights to be provided adjacent to 19-foot stalls). Some top-of-curb grades adjacent to 17-foot stalls continue to be 6 inches above top-of-pavement.

Response: Grading corrections will be provided on the Final Site Plan.

30. **Comment:** Provide a plan view and cross-sections of the retaining walls. Sheet with these details shall be signed and sealed by the design engineer responsible for the proposed retaining wall design and all associated calculations.

Response: Additional detail will be provided on the Final Site Plan.

31. **Comment:** An SESC permit is required. A full review has not been completed at this time. The review checklist

detailing all SESC requirements is attached to this letter. Please submit an SESC permit application under separate cover.

Response: A SESC permit will be applied for at the time of the Final Site Plan.

32. **Comment:** Any required off-site easements must be executed prior to final approval of the plans. Drafts shall be submitted the time of the Final Site Plan submittal.

Response: Understood.

33. **Comment:** A letter from either the applicant or the applicant's engineer must be submitted with the Final Site Plan highlighting the changes made to the plans addressing each of the comments listed above and indicating the revised sheets involved. Additionally, a statement must be provided stating that all changes to the plan have been discussed in the applicant's response letter.

Response: Understood.

34. **Comment:** An itemized construction cost estimate must be submitted to the Community Development Department for the determination of plan review and construction inspection fees. This estimate should only include the civil site work and not any costs associated with construction of the building or any demolition work. The estimate must be itemized for each utility (water, sanitary, storm sewer), on-site paving (square yardage), right-of-way paving (including proposed right-of-way), grading, and the storm water basin (basin construction, control structure, pre-treatment structure and restoration).

Response: Understood.

35. **Comment:** A draft copy of the Storm Drainage Facility Maintenance Easement Agreement (SDRMEA), as outline in the Storm Water Management Ordinance, must be submitted to the Community Development Department. Once the agreement is approved by the City's Legal Counsel, this agreement will then be sent to City Council for approval/acceptance. The SDFMEA will then be recorded at the office of the Oakland County Register of Deeds.

Response: Understood.

36. **Comment:** A draft copy of the access easement to sanitary sewer monitoring manholes must be submitted to the Community Development Department. This document is available on our website.

Response: Understood.

37. **Comment:** A pre-construction meeting shall be required prior to any site work being started. Please contact Sarah Marchioni in the Community Development Department to setup a meeting (248-347-0430)

Response: Understood.

38. **Comment:** A City of Novi Grading Permit will be required prior to any grading on the site. This permit will be issued at the pre-construction meeting (no application required). No fee is required for this permit.

Response: Understood.

39. **Comment:** Material certifications must be submitted to Spalding DeDecker for review prior to the construction of any onsite utilities. Contact Ted Meadows at 248-844-5400 for more information.

Response: Understood.

40. **Comment:** Construction inspection fees must be paid to the Community Development Department.

Response: Understood.

41. **Comment:** Legal escrow fees must be deposited with the Community Development Department. All unused escrow will be returned to the payee at the end of the project. This amount includes engineering legal fees only. There may be additional legal fees for planning legal documents.

Response: Understood.

42. **Comment:** A storm water performance guarantee (equal to 120% of the cost required to complete the storm water management facilities) as specified in the Storm Water Management Ordinance must be posted at the Community Development Department.

Response: Understood.

43. **Comment:** Water and Sanitary Sewer Fees must be paid prior to the pre-construction meeting. Contact the Water & Sewer Division at 248-347-0498 to determine the amount of these fees.

Response: Understood.

44. **Comment:** A street sign financial guarantee (\$400 per traffic control sign proposed) must be posted at the Community Development Department. Signs must be installed in accordance with MMUTCD standards.

Response: Understood.

45. As described previously, a Soil Erosion Control Permit must be obtained from the City of Novi.
Response: Understood.
46. **Comment:** A permit for all proposed work activities within the road right-of-way of Grand River Avenue must be obtained from the City of Novi.
Response: Understood.
47. **Comment:** A permit for work within the road right-of-way of Grand River Avenue must be obtained from the Road Commission for Oakland County (RCOC).
Response: Understood.
48. **Comment:** An NPDES permit must be obtained from the MDEQ since the site is over 5 acres in size. The MDEQ may require an approved SESC plan to be submitted with the Notice of Coverage.
Response: Understood.
49. **Comment:** An inspection permit for the sanitary sewer taps must be obtained from the Oakland County Water Resource Commissioner (OCWRC).
Response: Understood.
50. **Comment:** The amount of the incomplete site work performance guarantee for any outstanding site improvement items, is calculated at 1.2 times the amount required to complete the site improvements.
Response: Understood.
51. **Comment:** All easement and agreements referenced above must be executed, notarized and approved by the City Attorney and Engineering Division.
Response: Understood.
52. **Comment:** The City's consultant Engineer Spaulding DeDecker will prepare the record drawings for this development. The record drawings will be prepared in accordance with Article XII, Design and Construction Standards, Chapter 11 of the Novi Code of Ordinances.
Response: Understood.
53. **Comment:** Submit an up-to-date Title Policy (dated within 90 days of City Council consideration of acceptance) for the purpose of verifying that the parties signing the Easement documents have the legal authority to do so. Please be sure that all parties of interest shown on the title policy (including mortgage holders) Either sign the easement documents themselves or provide a Subordination Agreement. Please be aware the at the title policy may indicate that additional documentation is necessary to complete the acceptance process.
Response: Understood.

ECT – WETLANDS & WOODLAND REVIEW (DATED OCTOBER 9, 2019)

Wetland Review Comments

1. **Comment:** *The wetland and wetland buffer boundaries indicated on the Plan appear to be approximate. Subsequent site plan submittals shall include the actual, surveyed wetland boundary information that appears to be included on the Wetland Delineation map provided by KME (figure 2). The wetland flag numbers shall also be provided on the plans. **This comment has been satisfactorily addressed. This information has been included on the Topographic Survey (Sheet 2)***
Response: No comment.
2. **Comment:** *It is unclear if the proposed site work will impact Wetland C, however it does not appear likely. The boundaries of Wetland C are not currently shown on sheet 1. This information should be provided/clarified on subsequent site plan submittals. **This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan (Sheet 3).***
Response: No comment.
3. **Comment:** *The applicant shall indicate, quantity and label all existing areas of wetland and 25-foot wetland buffers (square feet or acres) on the Plan. **This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan. (Sheet 3).***
Response: No comment.
4. **Comment:** *The applicant shall indicate, quantify and label all proposed impacts to the wetlands (square feet or acres) including proposed volume of cut/fill (cubic feet or cubic yards). **This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan (sheet 3).***
Response: No comment.

5. **Comment:** *The applicant shall indicate, quantify and label all proposed impacts to the 25-foot wetland setbacks (square feet or acres). This comment has been satisfactorily addressed. This information has been included on the Preliminary Grading Plan (Sheet 3).*

Response: No comment.

6. **Comment:** *It appears as though a MDEQ Wetland Permit and a City of Novi Non-Minor Wetland Use Permit would be required for any proposed impacts to on-site wetlands, if applicable. A City of Novi Authorization to encroach the 25-foot Natural Features Setback would be required for any proposed impacts to on-site 25-foot wetland or watercourse buffers. This comment has been partially addressed. In a Pre-Application Meeting Response to Review letter dated September 12, 2019, the applicant's engineer notes that the wetland permitting is currently in process with EGLE. The Applicant should provide a copy of the EGLE Wetland Use Permit application to the City (and our office) for review and a copy of the approved permit upon issuance. A City of Novi Wetland Permit cannot be issued prior to receiving this information.*

Response: Wetland permitting is currently in process. The application will be forwarded once ready.

7. **Comment:** *It should be noted that it is the Applicant's responsibility to confirm the need for a Permit from the MDEQ for any proposed wetland impact. Final determination as to the regulatory status of each of the on-site wetlands shall be made by MDEQ. The Applicant should provide a copy of the MDEQ Wetland Use Permit application to the City (and our office) for review and a copy of the approved permit upon issuance. A City of Novi Wetland Permit cannot be issued prior to receiving this information. This comment has been partially addressed. See Comment No. 6, above.*

Response: This information is currently being investigated and will be submitted under separate cover.

8. **Comment:** *The Plan should address how any temporary impacts to wetland or 25-foot wetland buffers shall be restored, if applicable. Subsequent Plan submittals shall include specifications for any proposed seed mixes proposed for use within these areas. Sod or common grass seed will be acceptable to restore temporary impacts to wetlands or 25-foot wetland buffers. This comment has not been addressed. All impacts on the Preliminary Grading Plan (Sheet 3) appear to be permanent, however Low Prairie Seed Mix is proposed on the Landscape Plan (Sheet L-1). It is not clear where this particular seed mix is proposed to be installed. Please review and revise the Plan as necessary.*

Response: Additional information will be provided on the Final Site Plan.

9. **Comment:** *The applicant should ensure that any proposed snow storage areas are located such that any runoff will not directly affect any on-site wetlands or the Walled Lake Branch of the Middle Rouge River (if applicable)*

Response: Additional information has been provided. Refer to the "Landscape Plan". **This comment has been satisfactorily addressed. The proposed snow storage areas have been indicated on the Landscape Plan (Sheet 1). It appears as though the runoff from the snow melt will be routed through the proposed detention basin prior to discharge to existing Wetland B on the eastern side of the site.**

Response: No comment.

10. **Comment:** *ECT suggests that any proposed stormwater management plan be reviewed by the City of Novi Engineering Department to ensure that they meet the City of Novi design requirements. This comment still applies.*

Response: No comment.

Woodland Review Comments

1. **Comment:** *ECT currently recommends approval of the PRO Concept Plan for Woodlands. No further woodland review of the proposed project is necessary.*

A review letter for the woodlands was not provided within the October 11th, 2019 review package.

Response: No comment.

AECOM – TRAFFIC REVIEW (DATED OCTOBER 9, 2019)

Internal Site Operations

1. **General Traffic Flow**

- a. **Comment:** *The applicant has generally indicated 24 foot aisles throughout the site.*

Response: Additional dimensions will be included on the Final Site Plan.

- b. **Comment:** *The applicant is proposing an 18 foot wide, one-way emergency access drive along the east side of the building. The applicant should provide further detail regarding the gate and signing*

requirements, and should work with the Fire Marshal regarding the need for gates at either end of the drive.

Response: Additional information will be provided on the Final Site Plan.

- c. **Comment:** The applicant should indicate the manner in which the drive aisles in the unstriped rear gravel lot will be maintained for clear fire access.

Response: A Demarcation Plan has been prepared to address this item. Refer to the "Demarcation Plan".

- d. **Comment:** The applicant was granted a deviation to install painted end islands in lieu of raised end islands.

Response: No comment.

- e. **Comment:** The applicant has included dimensions for the widths of the proposed painted end islands throughout the site that are generally in compliance with City requirements as stated in Section 5.3.12 of the Zoning Ordinance.

- i. **Comment:** Note that all end islands shall be constructed three (3) feet shorter than the adjacent parking space.

Response: Additional dimensions will be added to provide clarity on the Final Site Plan.

- ii. **Comment:** The radii of the painted end islands are generally in compliance with City standards, with the exception of the following.

1. **Comment:** The applicant should provide justification for the irregular raised end island near the building canopy on the south side of the site or update to meet City standards.

Response: Additional information to provide clarity will be provided on the Final Site Plan.

2. **Comment:** Additionally, there is a painted end island proposed near the south side of the auxiliary building within the gated area that has an outside radius of 10 feet. This should be updated to a minimum of 15 feet.

Response: Additional information to provide clarity will be provided on the Final Site Plan.

- f. **Comment:** The applicant has proposed a trash receptacle in the rear yard. The trash receptacle is located in a position that, while during pick-up periods, may diminish the ability for vehicles to exist the nearest parking space; however, it is not expected to diminish accessibility beyond acceptable levels.

Response: No comment.

- g. **Comment:** The applicant has indicated a loading ramp location and a 10' by 25' loading zone on the site plan.

- i. **Comment:** The applicant provided truck travel patterns throughout the site and confirmed accessibility to/from the loading zone.

Response: No comment.

- h. **Comment:** There are proposing parking bays that have more than 15 consecutive parking spaces. The applicant has been granted a deviation for the lack of landscape islands every 15 spaces in accordance with the City's Zoning Ordinance, section 5.5.3.C.ii.i.

Response: No comment.

2. Parking Facilities

- a. **Comment:** Refer to the Planning Letter for information about parking requirements and calculations.

Response: No comment.

- b. **Comment:** The applicant has indicated 17 foot long parking spaces abutting four inch curbs, and 19 foot long parking spaces in all other areas.

Response: No comment.

- c. **Comment:** The applicant has provided the width of the barrier free parking spaces and aisles, which are in compliance with City standards.

- i. **Comment:** One of the barrier-free parking spaces is proposed to be behind the gated portion of the parking lot. The applicant could consider moving this space to be within the main parking lot area.

Response: No comment.

- d. **Comment:** *The applicant has proposed a barrier-free parking space along the western side of the property and should strongly consider relocating this parking space to be closer to the building entrance which it is serving. The applicant indicated that the grading of the site limits the ability to move this space.*
 - i. **Comment:** *The applicant should provide a ramp to the sidewalk next to this accessible parking space.*
Response: Additional grading detail will be provided on the Final Site Plan.
- e. **Comment:** *The applicant should review the curb heights throughout the site to generally provide 6" curbs for all landscape areas, except when placed directly in front of a 17' parking space where the curb should be 4".*
 - i. **Comment:** *The grading plan and details are generally in compliance with this; however, there are locations throughout the site where further clarification is needed, such as along the existing building to the south.*
Response: Grading will be reviewed and corrected where necessary for the Final Site Plan.

3. Sidewalk Requirements

- a. **Comment:** *The applicant should dimension the width of all proposed sidewalks.*
 - i. **Comment:** *Sidewalk throughout the site are required to be a minimum of 5' wide.*
Response: Additional dimensions will be included on the Final Site Plan.
 - ii. **Comment:** *Note that when a 17' parking space abuts a sidewalk, the sidewalk shall be 4" in height and a minimum of 7' wide to accommodate a 2' vehicle overhang and provide 5' of unobstructed travel way for non-motorized users.*
Response: Grading will be reviewed and corrected where necessary for the Final Site Plan.
- b. **Comment:** *The applicant should label sidewalk ramps on the plans and include the latest Michigan Department of Transportation (MDOT) detail.*
Response: MDOT details for ADA barrier free ramps will be included on the Final Site Plan.
- c. **Comment:** *The applicant should provide a sidewalk ramp at the north bay of parking that connects the sidewalk from Grand River Avenue to the site.*
Response: Grading will be reviewed and corrected where necessary for the Final Site Plan.

Signing and Striping

- 1. **Comment:** *All on-site signing and pavement markings shall be in compliance with the Michigan Manual on Uniform Traffic Control Devices (MMUTCD). The following is a discussion of the proposed signing and striping.*
Response: No comment.
- 2. **Comment:** *The applicant has included a sign quantity table*
Response: No comment.
- 3. **Comment:** *The applicant should provide notes and details related to proposed signing.*
 - a. **Comment:** *Traffic control signs shall use the FHWA Standard Alphabet series*
Response: Additional information will be provided on the Final Site Plan.
- 4. **Comment:** *The applicant should provide notes and details related to proposed pavement markings.*
 - a. **Comment:** *Detail the pavement markings for the end islands and other hatched areas, including color, striping width, etc.*
Response: Additional information will be provided on the Final Site Plan.
 - b. **Comment:** *Detail the pavement markings for crosswalks, including color and striping width.*
 - i. **Comment:** *The applicant should consider adding crosswalk markings at the northeast sidewalk ramp to the sidewalk that leads to Grand River Avenue.*
Response: The Applicant will consider additional crosswalk markings.
 - c. **Comment:** *A detail has been provided for the international symbol for accessibility.*
Response: No comment.

CITY OF NOVI FIRE DEPARTMENT REVIEW (DATED SEPTEMBER 24, 2019)

- 1. **Comment:** *All fire hydrants must be installed and operational prior to any combustible material is brought on site.*
Response: A note indicating this will be provided on the Final Site Plan.

2. **Comment:** *Water main sizes and fire hydrant locations must be put on the plans for review.*
Response: No comment.
3. **Comment:** Must provide a Knox lock for the gates
Response: A note indicating this will be provided on the Final Site Plan.
4. **Comment:** Existing fire lane Corrected. Fire access lane on the east side of the structure must be 20' wide. Not 18.6'
Response: East access lane is an existing condition.
5. **Comment:** Turning radius doesn't meet city standards, 50' outside and 30' inside turning radius. Locations are northwest corner and southeast corner of the structure.
Response: Additional information will be provided on the Final Site Plan.

DRN & ASSOCIATES, ARCHITECT, PC REVIEW (DATED AUGUST 7, 2019)

Refer to the response to review letter prepared by Cityscape and dated October 16, 2019.

LANDSCAPE REVIEW LETTER (DATED SEPTEMBER 30, 2019)

Refer to the response to review letter prepared by Allen Design and dated October 21, 2019.

If you have any questions/comments, please feel free to contact me at (248) 926-3701 or shiloh@alpine-inc.net.

Thank you.

Sincerely,
Alpine Engineering, Inc.



Shiloh Dahlin
Senior Project Engineer

October 21, 2019

Mr. Rick Meader, Landscape Architect
City of Novi Community Development
45175 West 10 Mile
Novi, MI 48375

RE: Keford Collison and Towing

Dear Mr. Meader:


Below are our responses to your review of plans dated September 12, 2019.

Landscape Comments:

- The landscape waivers, including the meeting date, will be shown on Sheet L-1.
- An additional evergreen tree species will be used.
- An irrigation plan will be submitted with Stamping Sets.

If you have any questions or comments regarding this response, please contact me at your convenience.

Sincerely,



James C. Allen
Allen Design L.L.C.



October 16, 2019

Mr. Sri Ravali Komaragiri
City of Novi Planning Department
45175 West Ten Mile Road
Novi, MI 48375

RE: Façade Ordinance Review – Preliminary Site Plan
Keford Collision & Towing, JSP18-31, PSP18-0107
Façade Region1, Zoning District I-1

Dear Sri,

On behalf of our client, Keford Towing, please find the following architectural updated elements in response to Doug Necci's Façade Review letter dated August 7, 2018.

- PDF files of revised architectural documents dated 10/17/19, including Shts #T-001, A-110, A-111, A-211, and one colored rendering.
- Color elevation rendering on Sht T-001 has been revised.
- Sht A-110 and A-111 revision date has been added to title block.
- Sht A-211 color PT-3 has been added to material schedule and color PT-1 has been revised to a more subdued tone of red.
- Note has been added in reference to the fact that no rooftop mechanical units exist on the roof; therefore, we are not planning to add any new rooftop units (RTU) as part of this proposed design.

Please let us know if you have any questions or require any further information from our firm

Regards,

A handwritten signature in black ink, appearing to read "Tony Dellicolli".

Tony Dellicolli, RA, AIA
Principal

A handwritten signature in black ink, appearing to read "Derrick J. Mroz".

Derrick J. Mroz, RA, LEED AP
Principal

Cc: Tom Herrington, Keford Towing
Shiloh Dahlin, Alpine Engineering
David Landry, Landry, Mazzeo & Dembinski, PC

Enclosure

□ □ □

MEMORANDUM



TO: MEMBERS OF THE PLANNING COMMISSION
FROM: SRI RAVALI KOMARAGIRI, PLANNER
THROUGH: BARBARA MCBETH, AICP, CITY PLANNER
SUBJECT: PUBLIC HEARING FOR TEXT AMENDMENT 18.290
DATE: OCTOBER 24, 2019

On August 28, 2019 Staff presented a draft amendment to reduce parking for hotel uses in Novi and requested Planning Commission to set a public hearing date. Planning Commission has set the date to October 30 and asked the staff to look into identifying ways to convert the area reduced in parking into usable green space. The intent of the amendment is to reduce the impervious footprint and gain pervious area on the site. This would help with storm water run-off and reduce development costs to the developer.

Initial comments from Commission members at the last discussion seemed to indicate concurrence with the idea of reducing the number of required parking spaces for these uses, but there was also a sentiment in favor of having the reduction result in more open space, as opposed to simply a bigger allowable footprint for building improvements. In response to those comments, staff is now proposing a two-point approach.

- 1) **Reduce parking from 1 space per room to 0.85 spaces per room, with no change to employee parking.** This reduction would apply to all hotels and motels irrespective of being part of a mixed use development or a shared parking agreement. The proposed reduction is based on the estimated 72% occupancy rate listed in ITE Parking manual (4th edition) and the increasing trend towards using ride sharing services as noted in our research. Parking demand as per ITE parking manual ranges from 0.43 to 0.86 spaces per each room depending on the type of hotel. Refer to Table 2 on page 5 for more details.
- 2) **Require 30 square feet of usable open space per each room subject to conditions listed in sec. 4.28.6 in draft amendment.** Refer to section 'Required usable open space for hotel use' on page 2 for more details on how staff has derived the square footage requirement. This proposed open space is in addition to the ordinance requirements for parking lot setbacks, building foundation plantings, and other landscaping requirements. This would not apply to TC, TC-1 and PD-2 districts where there are open space regulations in place.

This amendment also includes minor changes which are detailed in the draft ordinance and as noted below:

1. Clarifying definitions for a Hotel and Motel.
2. Reclassifying Business Motels as Motels and Transient Residential Uses as Hotels and making appropriate updates in related sections.

Background and Memo Attachments:

There are currently sixteen built hotels in Novi and one under construction. In the last five years, the Planning Commission has approved two hotels. In 2019, City Council has approved a development that included two hotels as part of a Planned Rezoning Overlay development, located at the intersection of Novi Road and I-96 express ramp. One of the hotels has applied for a reduction of minimum parking required and noted that *'the advent of transportation networking services/ridesharing services (Uber, Lyft, etc.) have an impact on parking trends. It is anticipated that guests will utilize the aforementioned services for business and recreational purposes and will further reduce the demand on parking'*. **Based on recent discussion, the Planning Commission has advised the staff to look into effects of ride-sharing services on the parking demand for hotels.** Staff studied available articles and contacted local hotel managers to gather data. This memo includes summary of the research and conclusions in the following documents attached to this memo.

1. Required usable open space for Hotel use
2. Research Summary on impacts of ride sharing services on Parking demand
3. Parking comparison with surrounding communities
4. Summary of proposed Text Amendments
5. A Draft Ordinance Amendment
6. A map showing the existing hotels and hotels currently under review/construction
7. A map showing the Current Zoning districts that allows Hotels as a permitted/special land use/limited use
8. A map showing future land uses that would allow Hotels as a permitted/special land use/limited use
9. Articles studied for this Amendment

The Planning Commission is asked to review the proposed amendments and if acceptable, make a recommendation to the City Council on the proposed ordinance amendments.

We note that the version that is now being provided is somewhat different from the version that the Commission saw when it set the public hearing. The changes primarily relate to the required open space concept and applying parking reduction for all hotels and motels irrespective of being part of a mixed use development or a shared parking agreement. The attached staff version of the proposed amendment is subject to review and changes by City staff and/or the City Attorney's Office. Please contact Sri Ravali Komaragiri 248.735.5607 or skomaragiri@cityofnovi.org with any questions or concerns.

REQUIRED OPEN SPACE FOR HOTELS

The requirement of minimum open space for a hotel use is not an uncommon. Especially with extended stay hotels where a majority of users are families with kids, having that extra space for passive recreation is useful. Our surrounding communities currently do not have standards, but a quick research revealed a few comparable requirements.

- o *City of Santa Ana, CA has a minimum of 1,000 square feet of common open space shall be provided at a 50 square feet per guest unit ratio up to a total of 7,500 square feet.*
- o *Town of Watertown, MA has a minimum open space requirement of ten percent (10%) of total site area.*
- o *Marietta, GA has a minimum open space requirement of 25% of the lot area dedicated to either active or passive open space for extended stay hotels/motels must have and an active recreation area at a ratio of 5 square feet per room with a minimum provision of 750 square feet.*

In order to achieve our intent to reduce parking to increase green space, we propose adding a requirement of minimum usable open space. **We determined that approximately 30 square feet of usable open space per each proposed room (AS) is close to 250 square feet of area per each reduced parking space (OS).** Please see below for more details. This would not apply to hotel uses in TC, TC-1 and PD-2 as they have a minimum open space requirement.

Approximately, a minimum of 250 square feet per parking space is required to build a parking lot. We took an example scenario of a hotel with 200 rooms to calculate the area of impervious area reduced with the proposed 15%. We did not take employee parking into account, since a change to the requirement is not proposed.

Table 1: Minimum Parking Requirements in Surrounding Communities		
	Current Ordinance	Proposed Ordinance
Number of Rooms = 200	R: 200 spaces (1 per each room)	170 spaces (0.85 per each room)
15% of parking required per current ordinance (15% of 200)		30
Area saved in square foot by proposed reduction <i>(Approximately 250square feet per parking space including drives etc.)</i>		250 sf x 30 spaces = AS = 7,500 SF
Minimum usable open space required (@ <i>30 square feet per each room)</i>		30 square feet x 200 rooms = OS = 6,000 SF

Given that the ride-sharing mode of transportation is an emerging idea, there is not enough data available that could be used to identify parking demands for specific land use. Most of the data available is collected at the airport or downtown parking garages and do not necessarily apply to a suburban setting similar to Novi. However, a particular study completed by University of Colorado, published in the Journal of Transport and land use (see end of page for citation) provides a better insight in the bi-directional relationship between parking and ride-sharing service. The study results suggest that:

- i) *Ride-hailing is replacing driving trips and could reduce parking demand, particularly at land uses such as airports, event venues, restaurants, and bars;*
- ii) *Parking stress is a key reason respondents chose not to drive; and*
- iii) *Respondents are generally willing to pay more for reduced parking time and distance. Conversely, parking supply, time, and cost can all influence travel behavior and ride hailing use. This study provides insight into potential benefits and disadvantages of ride-hailing as related to parking.*

This study also noted that 26.4% of Uber/Lyft riders would have driven and needed a parking space if the ride-hailing services did not exist. The users would have driven a car if parking is accessible and available. Residents and visitors to Novi have access to large surface parking lots, so availability of parking is not yet an issue in Novi. Novi does not have public transportation as an alternate mode of transportation; one should either own a car or use services like Uber/Lyft to travel.

Another article on the Hotels News Now, by George Jordan, states that 'ride-sharing works well in dense major metropolitan areas but not quite so well in ex-urban locations, where distances and wait times compel some form of ownership. The article also states the cost of renting a car and parking often exceeds the cost of overnight hotel accommodations in larger Cities, for example, \$ 70 a night in Chicago. Looking further into this theory, a room at a hotel in Novi, rents out to be approximately within \$65 to \$135 depending on the type of the hotel. A single day car rental at the airport starts at \$35a day. An Uber ride one way typically starts at \$35 from the airport. The owner of Hyatt Regency a Suburban showplace indicated that most of the residents at that hotel are conference attendees and a majority of them use services like Uber or Lyft, for the convenience aspect of it.

Based on the research, the probability of Uber/Lyft users is more for hotels associated with a conference use. Users of extended stay hotels may most likely rent a car.

[1] HENAO, Alejandro; MARSHALL, Wesley E.. *The impact of ride hailing on parking (and vice versa)*. *Journal of Transport and Land Use*, [S.l.], v. 12, n. 1, feb. 2019. ISSN 1938-7849. Available at: <https://www.jtlu.org/index.php/jtlu/article/view/1392>.

PARKING COMPARISON WITH SURROUNDING COMMUNITIES

Staff compared the minimum parking requirements for a Hotel in Novi with the surrounding communities and the ITE Parking manual (4th edition). A summary of findings is listed below:

Table 2: Minimum Parking Requirements in Surrounding Communities		
	per guest room	per each employee
Novi	1	1
Lyon Township	1	1
City of Farmington	1	0.25
Northville	1	1
Wixom	1	0
Commerce Township	1	1
Rochester Hills	1.1	0
Southfield	1.3	0
Suburban Hotel <i>(ITE Land use)</i>	1.2 per occupied room (At 72% Occupancy translates to 0.86 for each room)	0
Business Hotel <i>(ITE Land use)</i>	0.6 per occupied room (At 72% Occupancy translates to 0.43 for each room)	0
Motel <i>(ITE Land use)</i>	0.71 per occupied room (At 72% Occupancy translates to 0.51 for each room)	0

1. All communities require additional parking for accessory uses are per respective Zoning Ordinances.
2. Wixom, Farmington, Rochester Hills and Southfield did not specify a minimum requirement for number of employees, most likely due to access to public transportation.
3. ITE parking manual did not account for parking for employees as well.
4. **ITE parking manual lists parking demand based on 'occupied room' as opposed 'occupancy'**. It indicates that it is important to collect data on occupied rooms as well as total rooms in order to accurately estimate parking generation characteristics for the site.
5. ITE manual also acknowledges that parking demand at a hotel may be related to the presence of supporting facilities such as convention facilities, restaurants, meeting/banquet space and retail facilities.

SUMMARY OF PROPOSED TEXT AMENDMENTS

City of Novi Zoning Ordinance identifies four types of Hotel uses: Transient Residential Uses, Hotels, Business Motels and Motels as defined in Section 2.2 of our Zoning Ordinance. **Please refer to the draft Amendment for changes to the definitions for these four types.** A majority of the land uses are located along Grand River Avenue, Novi Road and Haggerty Road. The memo includes three maps that show the current hotels in Novi, current Zoning map and future land uses that would allow Hotels as a permitted/special land use/limited use. The text amendment would be applicable for the zoning districts listed in Table 3. Most of the districts would likely have a shared use.

It should be noted that the ITE provides numbers that indicate parking demand based on existing facilities survey. For example, the requirement for 1.2 per space is the demand for each occupied room. It is based on the estimation that the average occupancy rate for hotel is 72% at its maximum during a year. **Novi and surrounding jurisdictions require minimum parking for an estimated 100% occupancy, which is usually not the case for suburban hotels. It also did not account for Uber and Lyft.**

Staff contacted the surrounding communities around Novi to inquire if they considered the impact of Uber/Lyft on hotel parking. Even though they did not start the research, they agreed that it is something worth pursuing. The limited available data does not provide a clear direction on recommended reduction in the data based on ride sharing services. Some assumptions need to be made. In summary, parking requirements for hotels in Novi can be reconsidered. As these services become more popular, it also presents Planners an opportunity to consider the following in the future:

1. Lowering parking generation rates, thus reducing parking demand;
2. Reconsider the minimum parking requirements for certain uses such as restaurants, bars and conference centers;
3. Replacing parking spaces with different land uses and provide economic development opportunities.

Staff is proposing the following changes to the current parking and loading requirements, and simplifying the hotel categories as currently listed in our Zoning Ordinance, as noted in table 3 and 4.

Table 3: Comparison of Existing Requirement and Proposed Amendment		
	Existing Requirement	Proposed Amendment
Employee Parking <i>Section 5.2.12.C</i>	1 per employee	No change. The reduction in employee parking is not having any significant impact on minimum required calculations. Hotels now-a-days are transporting cleaning crews from one facility to the other.
Guest Parking <i>Section 5.2.12.C</i>	1 per occupancy room	0.85 per room for hotels or for all hotels and motels including extended stay. There is no maximum requirement in our ordinance. The applicant can always choose to provide additional parking. The proposed reduction is based on the 72% occupancy rates estimation provided in ITE Parking manual (4 th edition) and the increasing trend towards using ride sharing services as noted in our research.
Pick-up/Drop-off <i>Section 5.2.12.C</i>	No specific requirement	1 space for Pick-up to facilitate Uber/Lyft cars waiting for pick-up/drop-offs.
Accessory uses <i>Section 5.2.12.C</i>	Parking for accessory uses can be calculated based on individual requirements set forth on our Zoning Ordinance	Parking for accessory uses can be calculated based on individual requirements set forth on our Zoning Ordinance. Reduction subject to Planning Commission's approval of shared parking study or parking demand study, subject to requirements of Section 5.2.7.
Usable Open Space <i>Sec. 4.28.5</i>	None Town Center and Town Center One districts have 15% open space requirement for all uses including hotels.	30 square feet of usable open space for each room subject to size and type conditions listed in sec. 4.28.5. This would allow converting all the impervious area saved by reducing parking into usable green space (pervious). This would allow reduction of storm water run-off and construction of additional parking improvements.
Loading Zone <i>Sec. 4.28.6</i>	Within the B, GE, FS, RC, NCC, TC and TC-1 districts, a ratio of ten (10) square feet of loading area is required for each front foot of building; Within the OS districts, in the ratio of five (5) square feet of loading area is required per front foot of building up to a total area of three-hundred sixty (360) square feet per building.	The Planning Commission may permit reduction in minimum loading area requirements listed in Section 5.4. Such reduction may be considered by the Planning Commission only after reviewing relevant data provided by the applicant of the largest truck that will access the site conditions listed in sec.4.28.6.

Table 4: Changes to Hotel Terminology per Zoning District

Zoning District	Use	Section No.	Current Ordinance	Proposed Text change
P: Permitted; SLU: Special Land use				
B-2: Community Business	P	3.1.11.B.xiv	Hotels and motels §4.28 <i>(Permitted when the site does not abut residential use)</i>	No change
B-3: General Business	SLU	3.1.11.C.ii.	Motel §4.28	No change
C: Conference	SLU	3.1.13.C.ii	Hotels and motels <i>(not subject to Sec. 4.28)</i>	Hotels §4.28.5
EXPO: Expo	P	3.1.14.ii	Hotels and motels <i>(if included with an exposition facility)</i>	No change
EXO: Exposition Overlay	P	3.1.15.B.v	Hotels and business motel §4.28.4 <i>(when part of a development)</i>	Hotels §4.28.4
EXO: Exposition Overlay	SLU	3.1.15.C.ii	Hotels and motels when part of an Exposition, conference, and convention facilities §3.25	No change
FS: Freeway Service	P	3.1.17.iv	Motels, hotel and transient lodging facilities §4.28	Hotels and motels §4.28
I-2: Heavy Industrial District	SLU	3.1.19.B.xxxi.c	Eating and drinking establishments and motels §4.49 <i>Motels subject to restrictive conditions, to serve limited needs of an industrial district</i>	No change
I-1: Light Industrial District	SLU	3.1.18.C.ii.c		No change
OSC: Office Service Commercial	P	3.1.22.B.xi	Transient residential uses	Hotels §4.28.5
OST: Office Service Technology	P	3.1.23.B.v	Hotels and business motels §4.28.4 <i>(when part of a development)</i>	Hotels §4.28.4 <i>(when part of a development)</i>
RC: Regional Center	P	3.1.24.xiii	Transient residential uses	Hotels §4.28.5
TC Town Center	P	3.1.25.B.xix	Hotels and Transient Housing	Hotels §4.28.5
TC-1: Town Center-1	P	3.1.26.B.xix	Hotels and Transient Housing	Hotels §4.28.5
PD-2: Planned Development	SLU	3.31.7.B.i	Convention centers including motels, motor hotels	Convention centers including motels and hotels
Master Plan City west	P		Recommended permitted use of Hotels in 2015 Master Plan for Land Use	To be determined

A Draft Ordinance Amendment

DRAFT
STATE OF MICHIGAN
COUNTY OF OAKLAND
CITY OF NOVI
ORDINANCE NO. 18.290

AN ORDINANCE TO AMEND THE CITY OF NOVI ZONING ORDINANCE AT ARTICLE 2.0 DEFINITIONS, SECTION 2.2 DEFINITIONS; ARTICLE 3.0 ZONING DISTRICTS, SECTION 3.1 DISTRICTS ESTABLISHED, TO UPDATE THE HOTEL TERMINOLOGY IN APPLICABLE SECTIONS OF B-2, B-3, C, EXPO, EXO, FS, I-2, I-1, OSC, OST, RC, TC, TC-1 AND PD-2 ZONING DISTRICTS; ARTICLE 4.0 USE STANDARDS, SECTION 4.28 HOTELS, MOTELS AND TRANSIENT LODGING FACILITIES; ARTICLE 5.0 SITE STANDARDS, SECTION 5.2.12 OFF-STREET PARKING SPACES IN ORDER TO CLARIFY THE DEFINITION OF HOTEL, REVISE THE MINIMUM PARKING AND LOADING AREA REQUIREMENTS FOR HOTEL USE;

THE CITY OF NOVI ORDAINS:

Part I.

That the City of Novi Zoning Ordinance, as amended, Article 2, Definitions, Section 2.2. Definitions, is hereby amended to include the following definitions:

Section 2.2 Definitions

~~Business Motel: A building or part of a building, as to which the primary form of access to at least seventy five (75) percent of individual rooms is through a common entrance or entrances, and in which there is a series of attached, semi-detached or detached rental units containing a minimum of a bedroom, bathroom and closet space. Units shall provide for overnight lodging and are offered to the public for compensation, and shall cater primarily to the business traveler.~~

Motel: A series of attached, semidetached or detached rental units containing a bedroom, bathroom and closet space. Buildings may contain exterior corridors accessing the rooms. Units shall provide for overnight lodging and are offered to the public for compensation, have limited supporting facilities and shall cater primarily to the public traveling by motor vehicle.

Hotel: A building or part of a building, with a common entrance or entrances, in which the ~~dwelling units or rooming~~ units are used primarily for transient occupancy including extended stay, and in which one or more of the following services are offered: maid service, furnishing of linen, ~~telephone, secretarial, or desk service, and bellboy service,~~ breakfast service, room service and a business center. A hotel may include a restaurant or cocktail lounge, public banquet halls, ballrooms, or meeting rooms.

~~Transient Residential Uses: Uses such as hotels, motels and facilities used primarily for transient occupancy.~~

Part II.

That the City of Novi Zoning Ordinance, as amended, Article 4, Use Standards, Section 4.28 Hotels, Motels, and Transient Lodging Facilities, is hereby amended to read as follows:

Section 4.28 Hotels, and Motels, ~~and Transient Lodging Facilities~~

1. In the B-2 district, hotels and motels are a permitted use provided the site does not abut a residential district.
2. In the B-3 district, motels are permitted as a special land use subject to the following conditions:
 - A. Access shall be provided so as not to conflict with the adjacent business uses or adversely affect traffic flow on major thoroughfare
 - B. Each unit shall contain not less than two-hundred and fifty (250) square feet of floor area.
 - C. No unit shall be occupied as a permanent residence.
3. In the FS district, motels ~~and~~, hotels ~~and transient lodging facilities~~ but not including tent sites and campgrounds are permitted uses, provided that each living unit shall not contain less than two-hundred fifty (250) square feet of floor area, and provided further that no unit shall be occupied as a permanent residence.
4. In the OST district and EXO Overlay district, hotels ~~and business motels~~ are a permitted use when such are designed to be an integral part of an overall design of an OST district development under Section 3.1.23.B, which shall be constructed at the same time as or after one (1) of the principal permitted use
5. All hotels and motels in all districts except TC, TC-1 and PD-2 districts, shall provide a minimum of 30 square feet of usable open space for each room subject to the following conditions:
 - A. Usable open space shall include an unobstructed portion of a site which has a minimum dimension of ten feet in any direction, is landscaped and developed for active and passive recreational use, and is conveniently located and accessible;
 - B. The usable open space may include amenities such as pavers, benches and other landscape design elements with pervious surface materials. All such areas shall be for the benefit of the hotel patrons;
 - C. The usable open space shall not be counted toward any minimum landscaping or other open space requirements of the Zoning Ordinance and required parking setback area; or
6. The Planning Commission may approve a reduction in the minimum loading area requirements of Section 5.4 for all hotels and motels, subject to the Planning Commission making the following findings:
 - A. The proposed loading area is sufficient for the largest truck that will access the site, and considering any accessory uses proposed such as banquet halls, restaurants and gathering spaces such as conference spaces;
 - B. The proposed location shall provide a safe and convenient access to related uses inside the building in order to prevent the use of existing maneuvering lanes as a temporary loading area;
 - C. The reduction in the minimum loading areas shall not negatively affect the site or surrounding properties.

Part III.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, is hereby amended in the following Sections to read as follows:

C: Conference

3.1.13. C. ii. Special Land Uses

Hotels and motels S4.28.5

EXO: Exposition Overlay

3.1.15. B .v. Permitted Uses

| Hotels ~~and business motel~~ §4.28.4

FS: Freeway Service

3.1.17. B. iv. Permitted Uses

| ~~Hotels and~~ motels, ~~hotel and transient lodging facilities~~ §4.28

OSC: Office Service Commercial

3.1.22. B. xi. Permitted Uses

| ~~Transient residential uses~~ Hotels §4.28.5

OST: Office Service Technology

3.1.23. B. v. Permitted Uses

| Hotels ~~and business motels~~ §4.28.4

RC: Regional Center

3.1.24. xiii. Permitted Uses

| ~~Transient residential uses~~ Hotels §4.28.5

TC Town Center

3.1.25. B. xix. Permitted Uses

| ~~Hotels and Transient Housing~~ Hotels §4.28.5

TC-1: Town Center-1

3.1.26. B. xix. Permitted Uses

| ~~Hotels and Transient Housing~~ Hotels §4.28.5

PD-2: Planned Development

3.31.7. B. i. Permitted Uses

| Convention centers including ~~motels, motor~~ hotels

Part IV.

That the City of Novi Zoning Ordinance, as amended, Article 5, Site Standards, Section 5.2.12, Off-Street Parking Spaces is hereby amended to read as follows:

Section 5.2.12. Off-Street Parking Spaces

| Sec. 5.2.12.C Motels, ~~and~~ hotels ~~or other commercial lodging establishments~~

| ~~One (1) 0.85 space~~ for each one (1) occupancy unit plus one (1) for each one (1) employee, ~~plus parking for accessory uses. Minimum required spaces for other accessory uses such as restaurants, conference facilities shall be calculated per requirements at Section 5.2.12 in addition to the minimum required for a hotel.~~

Part V.

That the City of Novi Zoning Ordinance, as amended, Article 5, Site Standards, Section 5.2.12, Off-Street Parking Spaces is hereby amended to read as follows:

Section 5.4. Off-street Loading and Unloading

Items 1 through 5 unchanged

6. For hotels and motels, the Planning Commission may permit reduction in minimum loading area requirements listed in this section 4.28. 6.

PART VI.

Severability. Should any section, subdivision, clause, or phrase of this Ordinance be declared by the courts to be invalid, the validity of the Ordinance as a whole, or in part, shall not be affected other than the part invalidated.

PART VII.

Savings Clause. The amendment of the Novi Code of Ordinances set forth in this Ordinance does not affect or impair any act done, offense committed, or right accruing, accrued, or acquired or liability, penalty, forfeiture or punishment, pending or incurred prior to the amendment of the Novi Code of Ordinances set forth in this Ordinance.

PART VIII.

Repealer. All other Ordinance or parts of Ordinance in conflict herewith are hereby repealed only to the extent necessary to give this Ordinance full force and effect.

PART IX.

Effective Date: Publication. Public hearing having been held hereon pursuant to the provisions of Section 103 of Act 110 of the Public Acts of 2006, as amended, the provisions of this Ordinance shall be published within fifteen (15) days of its adoption by publication of a brief notice in a newspaper circulated in the City of Novi stating the date of enactment and effective date, a brief statement as to its regulatory effect and that a complete copy of the Ordinance is available for public purchase, use and inspection at the office of the City Clerk during the hours of 8:00 A.M. to 5:00 P.M., Local Time. The provisions of this Ordinance shall become effective seven (7) days after its publication.

MADE, PASSED, AND ADOPTED BY THE CITY COUNCIL OF THE CITY OF NOVI, OAKLAND COUNTY, MICHIGAN, ON THE ____ DAY OF _____, 2019.

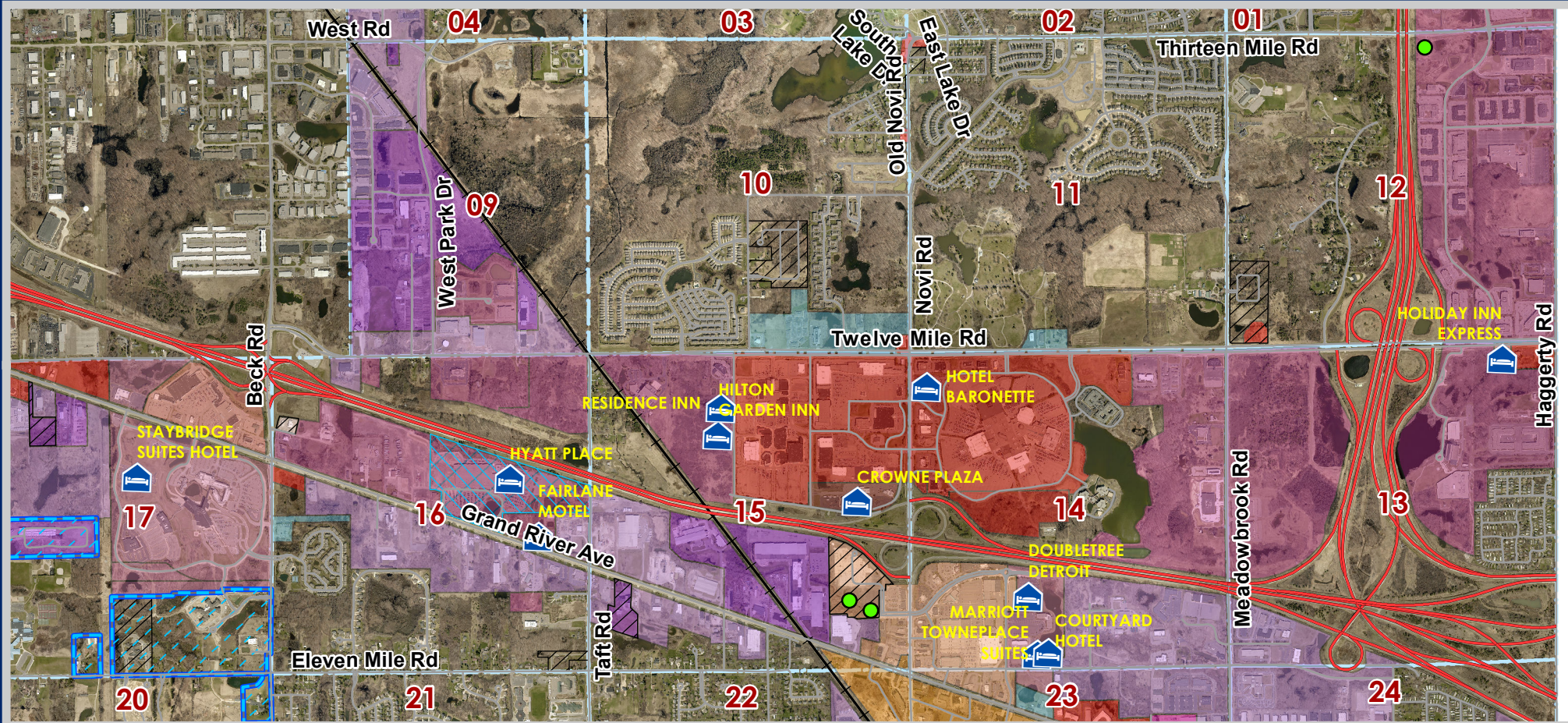
ROBERT J. GATT, MAYOR

CORTNEY HANSON, CITY CLERK

Ayes:
Nays:
Abstentions:
Absent:

A map showing the existing hotels and hotels currently under review/construction

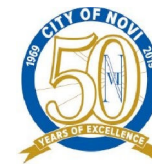
Existing and Proposed Hotels in Novi



LEGEND

- | | |
|------------------------------------|-------------------------------------|
| Exposition (EXO) | FS: Freeway Service District |
| Planned Suburban Low-Rise (PSLR) | I-1: Light Industrial District |
| Planned Rezoning (PRO) | I-2: General Industrial District |
| B-1: Local Business District | NCC: Non-Center Commercial District |
| B-2: Community Business District | OS-1: Office Service District |
| B-3: General Business District | OSC: Office Service Commercial |
| C: Conference District | OST: Office Service Technology |
| EXO: OST District with EXO Overlay | RC: Regional Center District |
| EXPO: EXPO District | TC: Town Center District |
| GE: Gateway East District | TC-1: Town Center -1 District |
| | Existing Hotels |
| | Hotels under construction/review |

Map Author: Sri Ravali Komaragiri
Date: August 15, 2019



City of Novi

Dept. of Community Development
City Hall / Civic Center
45175 W Ten Mile Rd
Novi, MI 48375
cityofnovi.org

0 950 1,900 3,800 Feet

Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Pleas contact the City GIS Manager to confirm source and accuracy information related to this map.



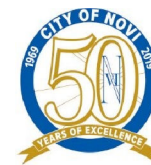
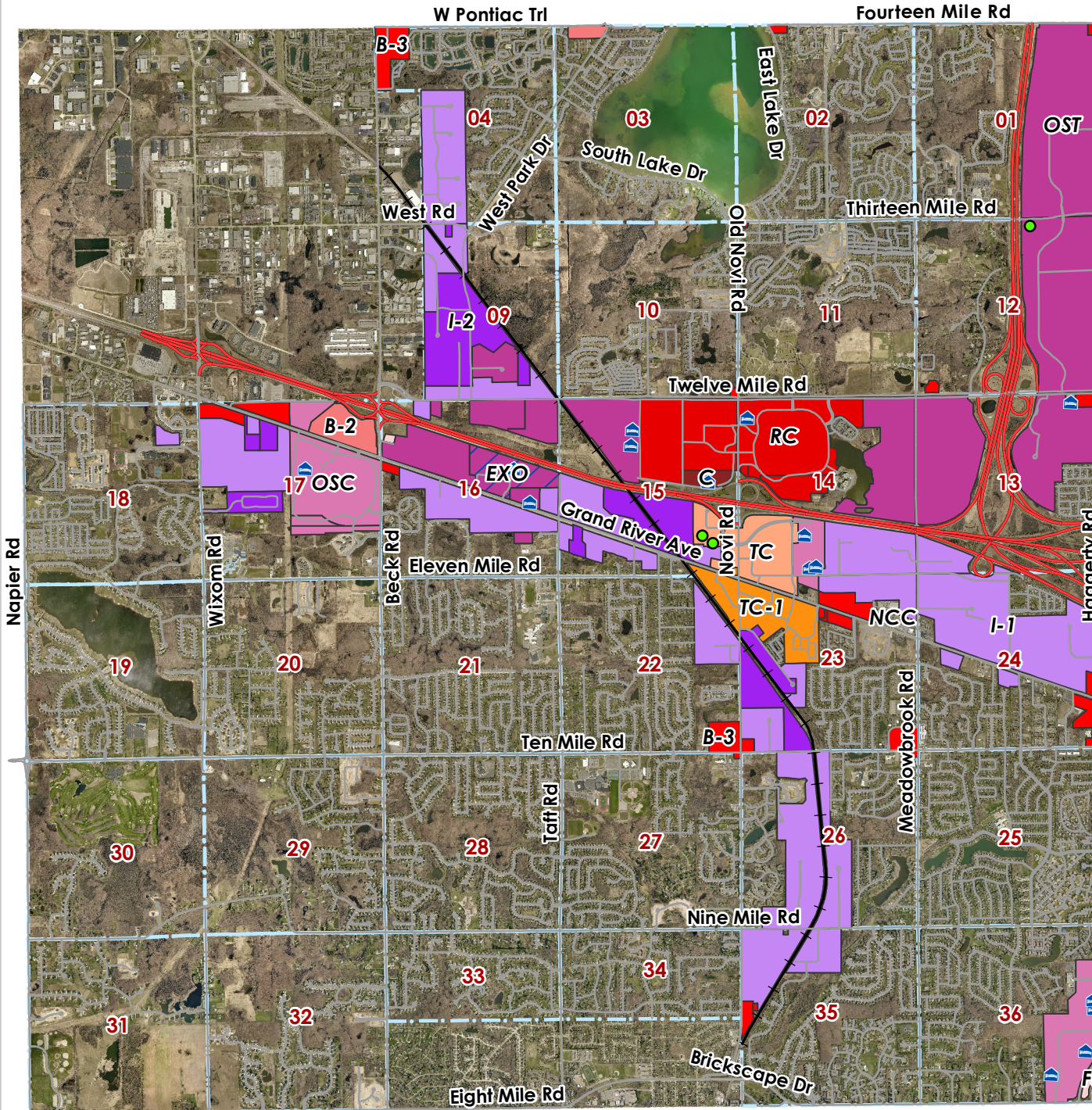
A map showing the Current Zoning districts that allows Hotels as a permitted/special land use/limited use

Current zoning Hotel use

Legend

- B-2: Community Business District
- B-3: General Business District
- C: Conference District
- EXO: OST District with EXO Overlay
- EXPO: EXPO District
- FS: Freeway Service District
- I-1: Light Industrial District
- I-2: General Industrial District
- OSC: Office Service Commercial
- OST: Office Service Technology
- RC: Regional Center District
- TC: Town Center District
- TC-1: Town Center -1 District
- Existing Hotels
- Hotels under construction/review

t t t g s t t s



City of Novi
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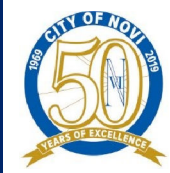
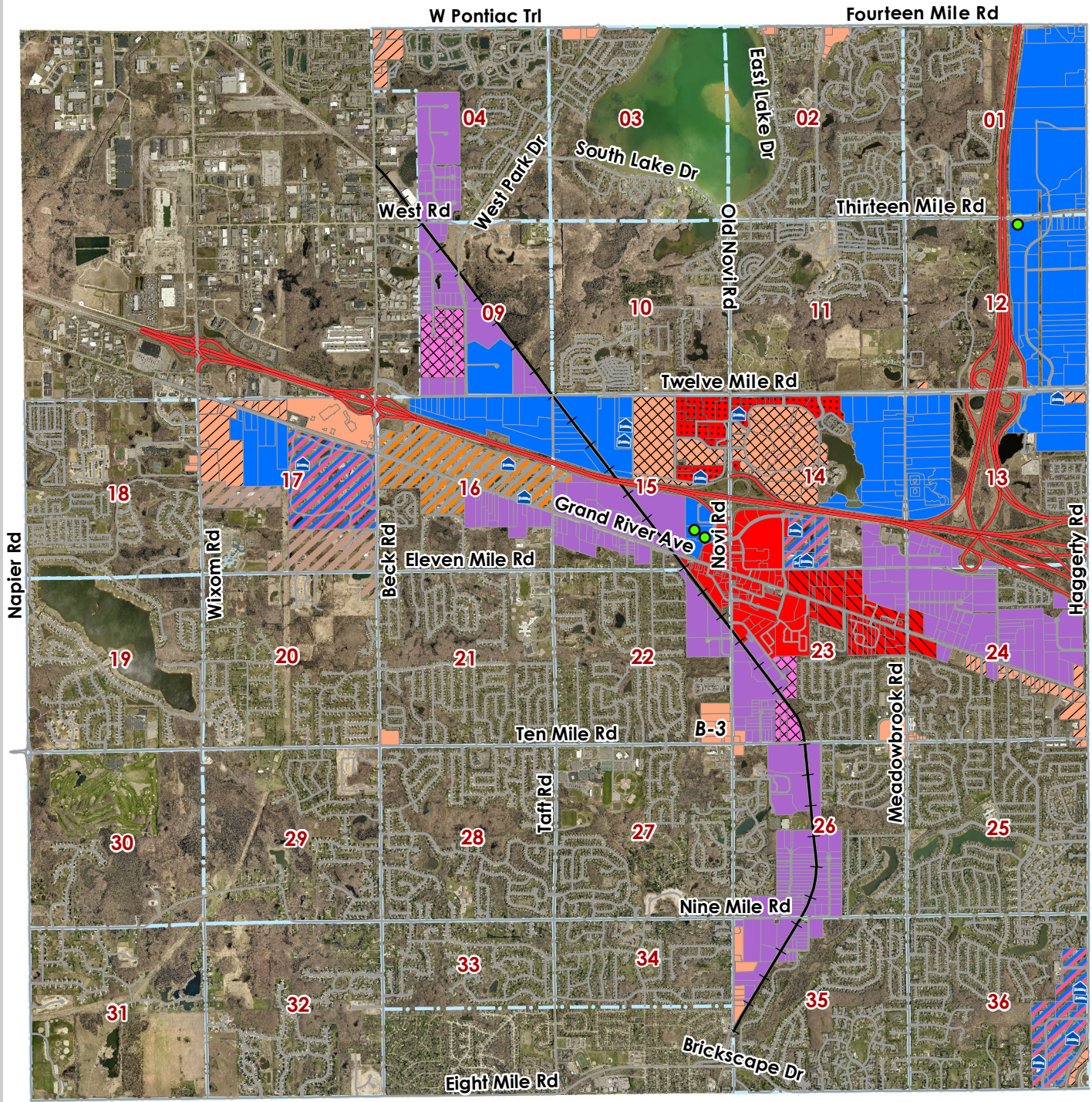
A map showing future land uses that would allow Hotels as a permitted/special land use/limited use

Future Land Use Categories: Hotel Use

Legend

-  Suburban Low-Rise
-  Office Research Development Technology
-  Office Commercial
-  Industrial Research Development Technology
-  Heavy Industrial
-  Local Commercial
-  Community Commercial
-  Regional Commercial
-  City West
-  TC Commercial
-  TC Gateway
-  PD2
-  Existing Hotels
-  Hotels under construction/review

t t t g s t t s



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Articles studied for this Amendment

Opinions

What the rise of Uber means for hotel parking lots

25 JANUARY 2017 1:13 PM

What could Uber's potential impact on transportation mean for hotel guests' need for their cars? The hotel parking lot might be living on borrowed time.



By **George Jordan**
george.jordan@ohrllc.com

Recently, I was bemoaning to a colleague about how I often struggle to find a relevant topic to write about for this column. Angie said, "You should write about Uber." And I thought to myself, well you are "uber-duber-whack-a-doodle-doesky. ... What does that have to do with hotels?" It turns out, quite a bit, and Angela is one smart lady.

Uber and other ride-sharing services—and the rise of social media applied to a smartphone—translates into a highly diminished desire for Generation Z citizens (and others) to own and drive a car. Indeed, lots of chatter online recently makes a very valid case that for the most part, private auto ownership is one of the worst capital investments anyone could make.

The very expensive car sits mostly un-utilized most hours of the day. It's parked in a garage overnight, is driven to/from work, or potentially shopping, and these activities may chew up a few hours a day of actual drive time. Otherwise, it sits idle. Factor in the recurring costs of ownership, and Gen Z has figured out it's cheaper and less stressful to "Uber it" to the next location.

With the added benefit of being able to "text and ride" and communicate via social media as a passenger, why drive when you can ride? Car ownership, and parking utilization, may have peaked in urban communities.

Ride-sharing works well in dense major metropolitan areas but not quite so well in ex-urban locations, where distances and wait times compel some form of car ownership. Finally, the cost of renting a car and parking (whether valet or self-park) often exceeds the cost of overnight hotel accommodations in larger cities—e.g. \$70 per night in Chicago.

The end result of these burgeoning trends is a precipitous drop in parking demand. New hotels are reducing the amount of parking stalls or eliminating parking altogether. In some major urban markets, residential parking ratios are extremely low, and cities are requiring fewer parking spots in order to encourage greater use of public transportation.

Autonomous vehicles make headlines on a daily basis. Every car manufacturer is in deep research and development and is looking forward to the day when vehicles are effectively robotic transports controlled by computers. Watch out Uber drivers: Uber will remain, but soon enough, the human driver won't.

The day is coming when citizenry doesn't drive, they ride. They do not park, they exit. Traffic jams and gridlock will be stricken from the urban dictionary. Vehicles will be computerized and unattended means of conveyance that are nearly fully utilized capital investments. Parking may become an afterthought, and its revenue streams like the telephone department deleted. You remember landlines, right?

All of this buildup is to point out a major trend at urban hotels: parking is on the decline, no qualifiers. For hotel owners and managers, this means "right-sizing" parking garages, moving to valet operations, and otherwise creating value for those that still do park. (Squeegee the windshield clean upon departure, perhaps?) It means less land requirements for new builds, and greater return on investment when sized right.

But none of this happens overnight or even in the next five years. So in the short term, what are you doing to maximize convenience and parking revenues to enhance the guest experience? Cleaning the windshield, offering a bottle of water on a hot summer day, combining door/bell services with valet are a means to improve the guest experience and the bottom line.

Short term, we find a new form of gridlock in the urban core, "Uberlock." The ride-share drivers have activated their idle capital (cars) and are circling the city blocks like vultures in the desert, waiting for something to die. And it will; it's called parking.

George Jordan is senior vice president – operations for Oxford Hotels & Resorts, overseeing a cluster of three-, four-, and 4 ½ -star hotels, both operating and under development. Mr. Jordan has worked in hotels for over 30 years including the Arizona Biltmore, The St. Paul, The Marquette, The Drake, Raffaello Hotel, Hotel Felix, and most recently The Godfrey Hotel Chicago. New openings currently orchestrated by Mr. Jordan include the Godfrey Hotel Boston, and LondonHouse Chicago. Mr. Jordan rose through the ranks while attending college at University of Southern California and Arizona State University, where he obtained a B.S. in finance. George has served as area food and beverage director for Hilton International, based out of the Drake Hotel Chicago, and also as hotel

manager at the Drake. George joined the Oxford team in 2009 as area general manager; he was promoted to senior vice president in 2012. His daily duties include oversight of Hotel Felix, Hotel Cass, Godfrey Hotel, and contributes his operational and marketing expertise to acquisition activities. George is a well-respected leader and a member of many Chicago civic organizations including The Magnificent Mile Association, CCTB, DLC and serves on the board of directors for Lawson House YMCA and on the advisory council of De Paul University's School of Hospitality. Mr. Jordan writes a quarterly column for Hotel News Now and is slated to be a cast member in an upcoming reality TV series.

The opinions expressed in this column do not necessarily reflect the opinions of Hotel News Now or its parent company, STR and its affiliated companies. Columnists published on this site are given the freedom to express views that may be controversial, but our goal is to provoke thought and constructive discussion within our reader community. Please feel free to comment or contact an editor with any questions or concerns.

Change Language: ▼

The Uber Effect

The “Uber Effect” refers to the influence of mobility sharing services, such as Uber and Lyft, on traditional transportation generators like commercial establishments, entertainment complexes, airports and hotels.

Ride-hailing, mobility-on-demand, ride-sharing: Whatever you call it, the media have extolled these app-based services as “disrupters” and “category killers.” The taxi and parking industries appear to be the “disrupted” and “killed” in these dire-future forecasts.

Outside this media echo chamber, there is evidence where Uber and Lyft are affecting the parking industry in certain markets. Uber doesn't release financials. Lyft has yet to make a dime. Nonetheless, parking facility owners, managers and other industry experts see ride-hailing companies affecting several specific demand sectors.

Are ride-hailing apps a trend? A spreading contagion dooming parking? Or just a fad, a blip in the nonstop, 100-year growth curve of commercial parking?

Airports Handle Ride Apps Differently

Uber and Lyft entered the Richmond, Va. market in August 2014. Richmond International Airport's (RIC) chief financial officer Doug Blum is closely monitoring the situation, but says, thus far, the “effect” has been minimal.

“Our enplanements have been growing the last four years and our parking income has also grown apace,” he notes. “The ride shares are taking a good bit of business from taxis, but the taxis are surviving. I think an overlooked part of the Uber traffic is from people that would have otherwise asked a friend, relative or co-worker to drive them.”

For airport administrators, an early warning of storms ahead might be diminishing terminal curbside availability.

As fewer vehicles exit airport roadways to park, more will destinate at the airport's front door. Competition for parking at airport curbsides has always been challenging, but now, with ride-hailing vehicles in the mix, congestion has worsened.

BUR Ride-Sharing Drivers Pick Up in Parking Lot

Hollywood Burbank Airport (BUR) has addressed curbside congestion and concern over potential lost parking revenues. The airport requires ride-hailing drivers to pick up their arriving passengers in the short-term parking lot.

This has added an estimated \$2,700 per month in parking fees, according to reports in the Los Angeles Times. It's not clear whether these new fees offset parking revenue losses created by the on-demand services spiriting away their customers.

LAX Drivers Pay Airport Access Fees

Also according to the LA Times, Los Angeles International Airport (LAX), has reached an agreement with Uber and Lyft that treats the on-demand services more like taxis, which pay commercial vehicle access fees to the airport.

LAX has created special waiting areas and curbside pick-up zones for the ride hailers. In return, Uber and its peers have each agreed to guarantee LAX a minimum of \$25,000 per month in commercial vehicle fees.

To date, these fees have greatly exceeded set minimums, but it's unknown if these gains offset potential parking revenue losses.

End of the Designated Driver?

Worldwide, restaurants, bars and concert venues are attributing a drop in parking demand directly to Uber and its competitors.

In São Paulo, Brazil, city managers blame Uber for a 40 percent decline in the parking tax income flowing from parking demand.

Recent articles from a variety of sources, such as the New York Times, indicate that app-users are partying longer and harder, freed from concerns about parking hassles and driving under the influence.

In Chicago, a Crain's Business Weekly article, "Will Uber and driverless cars turn the parking biz into roadkill?," identified one garage near the famed Rush Street nightlife zone citing a revenue decline of five percent. (Yes, the garage is still in business.)

NPA stalwarts John Hammerschlag of Hammerschlag Parking and SpotHero's Mark Lawrence were also cited in the Crain's article.

Hammerschlag noted the "Uber Effect" had produced "some impact" in certain locales, but that overall, his year-to-year traffic volumes were up.

Lawrence observed his nighttime customers were still driving, but perhaps more likely to park once, then Uber between hotspots.

Getting a Lyft to the Hotel

The hospitality parking sector has been hammered as guests, especially those from out-of-town who previously might have rented a car, choose to be driven to their hotel destination.

In an article in the Hotel News entitled, "What the Rise of Uber Means for Hotel Parking Lots," George Jordan, senior vice president for operations at Oxford Hotels & Resorts wrote recently, "[A] major trend at urban hotels: parking is on the decline, no qualifiers. For hotel owners and managers, this means 'right-sizing' parking garages, moving to valet operations, and otherwise creating value for those that still do park. . . It means less land requirements for new builds, and greater return on investment when sized right."

Annapolis-based Towne Park is a parking management leader in the hospitality sector. Chuck Heskett, president of the firm, agrees that on-demand services have reduced hospitality parking needs, but adds a caveat.

"Our hotel business has been negatively impacted. That's the bad news. The good news is the impact has been modest. The most severe loss in vehicle volume counts is occurring on the coasts and in larger event and entertainment hotels."

Most Commuters Still Prefer to Drive Themselves

Jerry Skillett of New York-based Citizens Parking has been quoted on numerous occasions as not fearing the onset of ride hailing and driverless cars.

In a recent article in the Atlanta Journal & Constitution, Skillett recounted that 90 percent of the vehicles arriving in his premium-rate, Manhattan garages are commuters piloted by a single occupant. Most observers agree that for commuting, Uber is not competitive. . . yet.

To date, ride hailing has not noticeably penetrated the suburban retail sector, known for its ubiquitous parking acreages. However, it has enlivened a few downtown residential and specialty shopping areas.

Condominiums have been listed for sale in San Francisco with no parking, but a year's worth of Uber rides. Leaders of Washington, DC's 14th Street retail corridor credit Uber with the area's revival. Medical markets have also not been significantly impacted.

Towne Park's Chuck Heskett reports, "Our healthcare business has been immune to the ride hailing apps."

Apps Growing, But So is Parking

Heskett keeps competitive concerns over the "Uber Effect" in perspective. "We have data that show the percent of annual passenger miles traveled using these apps (Uber, Lyft, etc.) going from 1.9 percent today to 5.3 percent by 2030," Heskett says.

"Overall, the parking business still grows. In absolute terms, we believe we will still be parking more cars as time goes on. The adoption rate of people that use ride-sharing apps seems to have leveled off at most of our affected locations as well."

Something's in the air with the "Uber Effect". But it's too early to tell whether it's burning rubber or simply hot exhaust.

Charles R. "Charlie" Munn III, CPP is the co-founder and CEO of the H2H2H Foundation. He is a former commercial parking executive. Email him at cmunn@h2h2h.org.

[VIEW ALL ARTICLES](#)

Parking Demand Trends: The Impact of Transportation Network Cos.

Walker Consultants Vice President Mary Smith discusses the impact of TNCs and autonomous vehicles on parking demand and how some sectors are affected by this growing industry.

By Adina Marcut (/author/adina-marcut/)

APR 02, 2018



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demand-
trends-
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impact-
of-
transportation-
network-
cos (/author/adina-marcut/)

Ride-sharing services such as Uber and Lyft are disrupting the status quo by offering cheaper, more flexible transportation, changing the way we get around. On one hand, these companies present challenges to traditional county revenue streams, while on the other hand, they provide new opportunities to improve county planning, mobility and service models. Walker Consultants' Vice President of Parking Consulting Mary Smith spoke with *Commercial Property Executive* about how Transportation Network Companies (TNCs) are affecting parking demand and how that impact could play out in the next few years.



(https://www.cpeexecutive.com/wp-content/uploads/2018/04/Mary-Smith-Walker-Consultants.jpg)

Mary Smith

Where do you currently have projects under development?

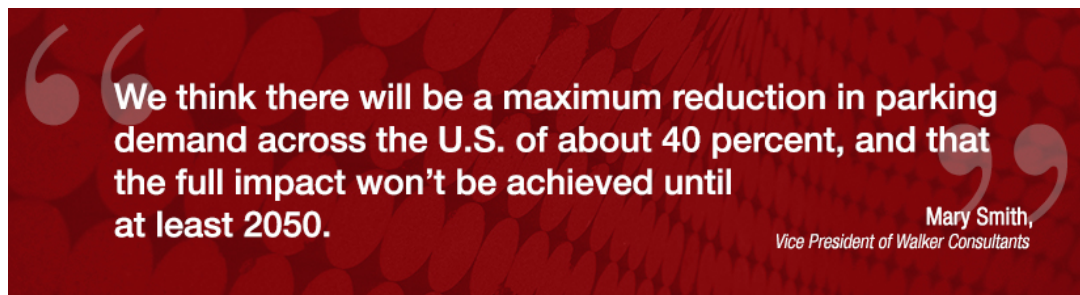
Smith: Personally, I am working on projects in Doha, Cairo, Dubai as well as in Atlanta, Los Angeles and New Jersey.

Do you think TNCs are a positive or a negative? Why?

Smith: There are many positives, including providing additional mobility options, and for urban dwellers, supporting a car-free lifestyle. Someone can use transit for most trips and TNCs when transit doesn't work well. However, studies are finding TNCs are negatively impacting transit, walking, biking and car sharing, proportionately more than driving and parking. A study by UC Davis found that roughly half of the trips by TNCs would have been made otherwise by transit, walking or biking or not going at all. They found that TNC use reduces bus ridership by 6 percent and light-rail usage by 3 percent, but increases heavy rail transit by 3 percent. Other studies have similar findings. So there are legitimate concerns about TNCs' impact on traffic and congestion, transit etc.

Which businesses are impacted the most by TNCs and how are they affected?

Smith: Aside from parking, the biggest issue for TNC rides right now is the impact of passenger loading. Airports are finding increased congestion at the curb and are moving pickup for TNCs inside parking facilities. Sports and event facilities are dealing with problems staging vehicles for pickup after events, with the volumes still growing rapidly year over year.



(<https://www.cpexecutive.com/wp-content/uploads/2018/04/Interview-quote-CPE-Mary-Smith.jpg>)

How do TNCs impact cities?

Smith: Cities are beginning to have to turn on-street parking to passenger loading zones. Over time, they will lose parking revenue if TNC use outpaces parking development growth.

How are ride-sharing services impacting parking demand?

Smith: From a parking perspective, airport parking transactions per enplanement are down by 5-20 percent with parking by business travelers appearing to be most impacted. It depends upon the parking rates and convenience of parking at the specific airport. Moreover, the impact is even greater on taxis and rental cars at airports and the fees that airports receive from those transactions. In turn, hotels are seeing up to a 70 percent decline in parking by business travelers, although there is much less impact on leisure traveler parking, as well as banquet and local event parking. Restaurants and bars, particularly those with valet parking, are seeing up to an 80 percent reduction in parking, apparently due to concerns both for convenience and cost of parking, and to avoid drinking and driving. Sports and events facilities are seeing a 3-6 percent reduction in parking from a few years ago.

How will driverless cars impact parking demand?

Smith: While many in the planning community project as much as a 90 percent reduction in parking demand in the U.S. within a decade or so due to autonomous vehicles (AVs), we believe it will be slower and much less impactful. We simply don't believe that 90 percent of Americans can or will give up cars and use **driverless cars** (<https://www.cpexecutive.com/post/4-big-trends-that-will-shape-cre-a-futurists-guide/>) instead, particularly shared-ride providers like UberPool and Lyft Line, which are necessary to get to the 90 percent figure cited in most articles.

How will parking demand change in the next years?

Smith: About one-third of Americans live in areas with a population less than 200,000 people, where shared TNC rides are unlikely to be nearly as cost-effective and convenience and comfort will play a bigger role. Further, we have 260 million non-automated vehicle (AV) cars on the road today, and millions more that will be sold in the next decade (before AVs are available to consumers). We think there will be a maximum reduction in parking demand across the U.S. of about 40 percent, and that the full impact won't be achieved until at least 2050. Where a parking facility serves activities that grow with population, like airports, downtowns and universities, the parking demand will continue to rise through about 2030 and then come back down to the demand today around 2050. Certainly, the impact will be much higher than a 40 percent reduction in the urban core areas, but it will be lower in suburbs and much lower in rural areas and smaller cities and towns.

How do you think self-driving vehicles will impact parking planning?

Smith: In addition to the reduction of parking due to driverless TNC rides, "autonomous parking" by privately owned AVs, will allow passengers to be dropped at the door and then the car will go and park itself, perhaps at a lower cost parking facility a few blocks away. Wherever they park, they can park closer together, because car doors don't need to be opened at the parking stall. As a result, the capacity of parking facilities may go up at the same time parking demand goes down. We need to plan for significantly increased passenger loading zones in the future for most parking structures designed today.

There is significant potential for the seas of asphalt surrounding most suburban developments to be redeveloped with office, residential, hotels, restaurants and even retail that would share with existing **parking recourses** (<https://www.cpexecutive.com/post/right-sizing-your-parking/>).

Can you name a few metros that are experiencing parking issues?

Smith: We are hearing the above referenced reductions to hotels, airports, bars/restaurants are pretty consistent in major metro areas across the US. Las Vegas is having enough problems with TNC loading that they are starting to turn on-street parking into loading zones.

What are the future plans regarding parking demand?

Smith: The impacts on parking in downtowns, universities and others land uses that have multiple parking facilities, will be absorbed by the market over time; surface lots in prime locations will be developed with little or no new parking, and older deteriorated garages will be torn down and redeveloped as well. While many talk about designing new parking facilities to be completely converted to other uses in the future, we haven't found a single client willing to pay any significant premium to do much more than provide more floor-to-floor height now.

And if you don't take other design steps now, like strengthening the structure for the heavier loads of office, retail and apartment uses and/or providing a façade that is easily converted, it will cost much more to convert in the future, while you will end up with a space that is probably significantly compromised compared to what the future market wants and needs.

Image courtesy of Walker Consultants

[Walker Consultants \(/tag/walker-consultants/\)](#)

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Excerpt from Planning Commission Meeting Minutes
August 28, 2019



PLANNING COMMISSION MINUTES

CITY OF NOVI

Regular Meeting

August 28, 2019 7:00 PM

Council Chambers | Novi Civic Center
45175 W. Ten Mile (248) 347-0475

CALL TO ORDER

The meeting was called to order at 7:03 PM.

ROLL CALL

Present: Member Avdoulos, Member Gronachan, Member Lynch, Member Maday

Absent: Member Anthony, Member Ferrell, Chair Pehrson

Also Present: Barbara McBeth, City Planner; Sri Komaragiri, Planner; Lindsay Bell, Planner; Victor Boron, Staff Engineer; Kate Richardson, Staff Engineer; Thomas Schultz, City Attorney

PLEDGE OF ALLEGIANCE

Member Lynch led the meeting attendees in the recitation of the Pledge of Allegiance.

APPROVAL OF AGENDA

Moved by Member Lynch and seconded by Member Gronachan.

VOICE VOTE TO APPROVE THE AUGUST 28, 2019 AGENDA MOTION MADE BY MEMBER LYNCH AND SECONDED BY MEMBER GRONCHAN.

Motion to approve the August 28, 2019 Planning Commission Agenda. Motion carried 4-0.

AUDIENCE PARTICIPATION

Dorothy Duchesneau, 125 Henning, said given where we are today, August of 2019, and with Robertson Brothers PRO approved for Lakeview, and it's not August of 2018 anymore when citizen input was given at the Pavilion Shore Park workshop, I don't understand why the entire six acres mentioned today as the proposed rezoning of Pavilion Shore Village needs to be rezoned.

Robertson Brothers was approved this past spring as a PRO development with an overlay. The new development does not even need to be part of the Pavilion Shore footprint. It seems like an overlay on top of an overlay on top of an already existing subdivision overlay, which is getting redundant. This portion of the neighborhood, as per the 2016 Master Plan, is now being developed and it's being fixed. The properties that were sold to Robertson Brothers were part of two subdivisions that were platted and date back ninety years. They

- ii. **A maximum of 4.5 feet of on-ground projection shall be allowed as shown in the 'Wing wall/planter projection area' exhibit on revised PRO Concept plan dated 07-25-19, subject to the City's Façade Consultant approval at the time of building permit review;**
- d. **The elevations shall comply with the Ordinance requirements and conditions of the PRO agreement, subject to the limitations set forth in the Master Deed as determined at the time of individual building permit review;**
- e. **The applicant shall update the PRO agreement with the following updates:**
 - i. **The ITC comfort station shall be completed within 6 months of Dec. 15, 2019 or within 6 months from the substantial completion date of the ITC trail at Nine Mile and Garfield intersection;**
 - ii. **The compensating cut areas in the approved PRO Agreement shall be updated to be consistent with the MDEGLE permit approval dated 04-09-19;**

This motion is made because the proposed amendment is proposing changes that are consistent with the intent of the original PRO plan and Agreement with additional modification as noted. *Motion carried 4-0.*

MATTERS FOR CONSIDERATION

1. **INTRODUCE TEXT AMENDMENT 18.290– Updates to standards for Hotel developments**
Set a public hearing for Text Amendment 18.290 to update at various sections, in order to bring ordinance language up to date, and update standards for minimum parking and loading requirements for Hotel developments.

Planner Komaragiri said thank you. Recently the Community Development department has seen a considerable interest in hotel development in Novi. There are about sixteen built hotels in Novi and one under construction. In the last five years, the Planning Commission has approved two hotels and in 2019, the City Council had approved a development that included two hotels as part of a planned rezoning overlay development. One of these hotels, which was part of the PRO, applied for the reduction in minimum parking and noted that the increase use of networking services and ridesharing services like Uber and Lyft had an impact on parking trends and anticipated that the guests will utilize those services and requested a reduction in parking counts.

Also, based on recent discussion, the Planning Commission has advised the staff to look into the effects of ride sharing services on the parking demand for hotels. We have studied available articles online and then contacted local hotel managers and we also checked in with other planners in the neighboring communities to see if they considered research on a similar amendment. The memo that was a part of the packet included a summary of the research and the conclusions and staff's recommendations. Staff had made some recommendations to reduce the minimum required parking for hotels and made some updates to the categories that are available in the text amendment and then allowed an option for Planning Commission to approve the reduction in parking and the loading area for hotels as part of their preliminary site plan approval.

The Planning Commission is asked to review the proposed amendment and if acceptable set a public hearing at a later meeting. Following the public hearing, the Planning Commission will be asked to make a recommendation to the City Council on the proposed ordinance amendment. Staff will get into additional detail at the public hearing about the actual changes. If you have any questions, I would be glad to answer them.

Member Avdoulos said thank you Sri, any additional discussion from the Planning Commissioners?

Member Lynch said first of all, I want to thank you for doing all of this. I know I asked for it a while ago, and I think the goal, when we had the discussion earlier, it was not just to give the hotel more opportunity to put more stuff in, but to reduce asphalt and replace it with some sort of foliage, we even talked about a land bank. If for some reason, we mess up and thirty years from now Uber and Lyft are gone and people are back driving cars, I would like to have the option of saying, right now we have this wonderful land bank and we have less asphalt, well now it looks like we messed up and have to go back to the old ordinance. It would give the hotels an opportunity to take that land bank and turn it back into asphalt or concrete. My understanding of the discussion was that we want to be reasonable, but we wouldn't want to give that stuff away, we just want to replace asphalt with greenery. Is that where this text amendment is headed?

Planner Komaragiri said not exactly. The text amendment looked at a possible reduction of the minimum parking counts because we looked at the ITE parking generation and everyone noted that the average maximum occupancy is between 70-80%. So we were looking in terms of that and we provided an opportunity to reduce the minimum parking counts and then there's always a shift in employee ratios within hotels. In response to your land banking comment, the current ordinance still gives that as an option for the applicants. It does not necessarily through this amendment, if any hotel applicant wants to come in and propose land banking, they can still do so by providing a parking study justifying the land bank and then Planning Commission can approve that based on the current ordinance requirements.

Member Lynch said in other words, right now the way the ordinance is written, I have a hotel and then I have parking, and that creates a footprint, so what I'm saying is, I want the footprint to stay the same, but the parking portion of that footprint is smaller and the green portion, where it would have been asphalt, is now a green space. Is that what we're doing?

Planner Komaragiri said not with this amendment, but the current ordinance would allow giving that option to the applicant if they want to.

Member Lynch said what if the applicant doesn't want to?

Planner Komaragiri said that's something we may have to look into.

Planner McBeth said right, so with Planning Commission's recommendation we can certainly add that component to the ordinance and say that a certain number of reduction of parking spaces can be allowed with the Planning Commission's approval for hotels provided that the spaces are land banked.

Member Lynch said you're much better at this than I am, I agree with you said.

Planner McBeth said and then there were several other changes included in the ordinance as well. Some of the research that was done indicated that perhaps we do have a little bit of a high parking calculation for hotels based on the other communities. So, allowing it to be green space instead of asphalt is a good idea.

Member Lynch said OK, when I make the motion it is going to be in accordance with what you just said Barbara.

Member Maday said that's what I was thinking too. Why have this huge parking lot if it's not going to be used? But I don't want to make it so people build a bigger building; I want it to be used for green. But this equation is going to change again, in my opinion, in the next ten years, everything's going to be augmented for ride sharing, so I think we're going to be visiting this several times, sooner than later. I like that were working on it and putting a foot forward on it.

Member Avdoulos said I think we're heading towards the direction of minimizing parking as much as we can and I know here we have accessory uses under the hotels so, for business type hotels, I can tell you I travel a lot around the country so when there's four of us going to a meeting and were staying overnight we'll have four rooms but we'll use one vehicle and we see that a lot with our consultants too. The accessory uses I'm thinking more hotels with banquet facilities will need to provide enough parking for their guests overnight so that's where I see it getting tricky, where you want to make sure you have enough parking to satisfy that. So I think this is a step in the right direction and I think Member Maday is correct. We'll revisit the ordinance so we make sure it is applicable and it's up to date. So those are my comments. Do we have a motion on that?

ROLL CALL VOTE TO SET A PUBLIC HEARING FOR TEXT AMENDMENT 18.290 MADE BY MEMBER LYNCH AND SECONDED BY MEMBER GRONACHAN.

Motion to set a public hearing for the text amendment 18.290 with staff to make to make revisions to the draft ordinance as discussed in the meeting. Motion carried 4-0.

2. INTRODUCE TEXT AMENDMENT 18.288 – UPDATES TO THE B-2 AND B-3 ZONING DISTRICTS

Set a public hearing for Text Amendment 18.288 to update the uses permitted as of right and the uses permitted as special land uses in the B-2, Community Business District and B-3, General Business District, and various other modifications.

Planner Bell said in your packet you have a number of proposed text amendments to the Zoning Ordinance as proposed by staff. City Council's Ordinance Committee recently met and requested that staff review land uses which may be more suited to be considered as a Special Land Use under the Special Land Use permit guidelines. That committee's input has been incorporated into the proposed text. The suggested changes at this time are primarily in the B-2, Community Business District and the B-3, General Business District. Those changes include amending the definitions of the Retail Business Service Uses and Retail Business Uses, and adding a definition for Smoke Shops, reclassifying theaters and other places of assembly as Special Land Uses in the B-2 and the B-3 Districts, clarifying the types of retail businesses allowed in the B-3 District, the

MEMORANDUM



TO: MEMBERS OF THE PLANNING COMMISSION
FROM: LINDSAY BELL, PLANNER *LB*
THROUGH: BARBARA MCBETH, AICP, CITY PLANNER
SUBJECT: SET PUBLIC HEARING FOR TEXT AMENDMENT 18.288
DATE: OCTOBER 23, 2019

Attached, please find a number of proposed text amendments to the City of Novi Zoning Ordinance as proposed by staff. City Council's Ordinance Review Committee met and requested that staff review land uses which may be more suited to be considered for a Special Land Use permit. Staff has completed this review, and the suggested changes at this time are primarily in the B-2, Community Business District and the B-3, General Business District. The limited scope of this review is intended to provide a manageable number of changes for ease of review by the Planning Commission and the City Council. Staff has incorporated items deemed necessary, including:

1. Amending the definitions of the Retail Business Service Uses and Retail Business Uses, and adding a definition for Smoke Shops:
 - a. The list of uses provided in the definition of Retail Business Service Uses has been updated to include establishments that provide technology repair, such as cell phone or electronic device repair.
 - b. Minor modifications are proposed to the wording in the definitions to improve clarity.
2. Reclassifying theaters and other places of assembly as Special Land Uses in the B-2 and the B-3 Districts. This addition is intended to allow the Planning Commission an opportunity to review any future theaters or places of assembly under the criteria provided in the ordinance for Special Land Use consideration, offering additional discretion for approval, and allowing for a public hearing on such requests.
3. Clarifying the types of retail businesses allowed in the B-3 District. The language proposes reclassifying tattoo parlors as a Special Land Use (from Principal Permitted Uses), and adding Smoke Shops to the list of Special Land Uses in the B-3 District. Again, this would allow the Planning Commission to review these uses under the Special Land Use criteria of the Zoning Ordinance.
4. Adding Massage Establishments as a Special Land Use only in the B-3 District. The Zoning Ordinance does not currently provide clear guidance on the permitted

location of such uses, as defined in the City Code:

Massage establishment means any establishment having a source of income or compensation derived from the practice of massage, and which has a fixed place of business where any person engages in or carries on any of the activities defined as massage. This definition does not include a regularly licensed hospital or dispensary, a place where massage of the face is for cosmetic or beautifying purposes, a place providing massage in the course of prescribed medical treatment by a physician or a health club, as defined herein, where massage is offered as an accessory service. Massage shall be considered an accessory service if the revenue received from the massage activities comprise no more than five (5) per centum of the gross receipts of the business location. A health club that offers massage as an accessory service shall be exempt from the permitting requirements, but is still required to annually submit an income statement to the police chief for review by the finance department to verify that the gross receipts for massage do not exceed five (5) per centum.

This amendment will allow the Planning Commission to hold a public hearing and consider requests for new massage establishments under the Special Land Use criteria of the ordinance prior to City Council's consideration of granting a license for such uses.

5. In the use standards for Places of Worship (Section 4.10), clarifying the districts where such use is considered a principal permitted use and where it is a Special Land Use.
6. Allowing the Planning Commission to modify the outdoor recreation requirements for day cares in the use standards for commercial districts.

On August 28, the Planning Commission reviewed the draft ordinance language and recommended that the matter be set for a public hearing.

The Planning Commission is asked to hold the Public Hearing and make a recommendation to the City Council for reading and adoption. If any commissioner has any questions related to this request, do not hesitate to contact Lindsay Bell at 248.347.0484.

DRAFT ORDINANCE AMENDMENT
STRIKE-THROUGH VERSION

DRAFT

STATE OF MICHIGAN

COUNTY OF OAKLAND

CITY OF NOVI

ORDINANCE NO. 18.288

AN ORDINANCE TO AMEND THE CITY OF NOVI ZONING ORDINANCE AT THE FOLLOWING LOCATIONS: ARTICLE 2, DEFINITIONS; ARTICLE 3, SECTION 3.1.11 "B-2 COMMUNITY BUSINESS DISTRICT"; ARTICLE 3, SECTION 3.1.12 "B-3 GENERAL BUSINESS DISTRICT"; ARTICLE 4, SECTION 4.10 "PLACES OF WORSHIP"; ARTICLE 4, SECTION 4.12 "GROUP DAY CARE HOMES, DAY CARE CENTERS, AND ADULT DAY CARE CENTERS"; ARTICLE 4, SECTION 4.27 "RETAIL BUSINESS AND SERVICE ESTABLISHMENTS"; IN ORDER TO RECLASSIFY CERTAIN USES AS SPECIAL LAND USES IN THE B-2 AND B-3 DISTRICTS, CLARIFY ORDINANCE LANGUAGE, AND OTHER ITEMS DEEMED NECESSARY.

THE CITY OF NOVI ORDAINS:

Part I.

That the City of Novi Zoning Ordinance, as amended, Article 2, Definitions, is hereby amended to amend two entries and include a new entry as follows:

Retail Business Service Uses: Personal service establishments which perform services on the premises, such as, but not limited to: barber shops, beauty shops, copy center, florist shops, locksmiths, home furnishings, photo finishing services, stationers, technology repair, and shoe repair shops.

Retail Business Uses: Generally recognized retail business which supply commodities on the premises, such as, but not limited to: bakeries ~~who's where~~ products are sold only at retail on premises, book stores, ~~news stands newsstands~~, drug stores, dry cleaning/laundry outlets dealing directly with consumers, food stores, jewelry stores, sporting goods stores, and studios: photography, art, music, and dancing, ~~and sporting goods stores~~.

Smoke Shop: A retail establishment where 50 percent or more of the retail area, defined as wall to wall, is used for the display, promotion, sale or use of products listed below; or an establishment where the sale of products listed below constitute greater than 50 percent of the establishment's merchandise:

- a. Cigarettes, e-cigarettes, vapor, nicotine/alternative nicotine products, cigars and packaged tobacco;
- b. Tobacco smoking and e-cigarette paraphernalia products, including, but not limited to: pipes for smoking tobacco and nicotine products, cigarette holders, pens and electronic devices used for smoking tobacco, vapor products, and other nicotine or alternative nicotine products, and cigarette rolling papers.

Part II.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, Section 3.1.11, is hereby amended to read as follows:

Section 3.1.11 B-2 Community Business District

A. [unchanged]

B. Principal Permitted Uses

- i. Retail business uses §4.27
- ii. Retail business service uses §4.27
- iii. Business establishments which perform services on the premises
- iv. Dry cleaning establishments, or pick-up stations, dealing directly with the consumer §4.24
- v. Professional services
- vi. Instructional centers
- vii. Service establishments of an office, showroom, or workshop nature §4.27
- viii. Restaurants (sit-down), banquet facilities or other places serving food or beverage §4.27
- ~~ix. Theaters, assembly halls, concert halls, museums, or similar places of assembly §4.27~~
- ~~ix.~~ Business schools and colleges or private schools operated for profit §4.27
- ~~xi.~~ Day care centers, and adult day care centers §4.12.2
- ~~xii.~~ Private clubs, fraternal organizations, and lodge halls
- ~~xii.~~ Places of worship §4.10
- ~~xiii.~~ Hotels and motels §4.28
- ~~xiii.~~ Professional and medical offices, including laboratories
- ~~xiv.~~ Other uses similar to the above uses
- ~~xv.~~ Accessory structures and uses §4.19, customarily incident to the above permitted uses

C. Special Land Uses

- i. Fueling station §4.29
- ii. Sale of produce and seasonal plant materials outdoors §4.30
- iii. Veterinary hospitals, or clinics §4.31
- ~~iv. Places of worship §4.10~~
- ~~v. Theaters, assembly halls, concert halls, museums, or similar places of assembly §4.27~~

D. [unchanged]

Part III.

That the City of Novi Zoning Ordinance, as amended, Article 3, Zoning Districts, Section 3.1.12, is hereby amended to read as follows:

Section 3.1.12 B-3 General Business District

A. [unchanged]

B. Principal Permitted Uses

- i. Retail businesses ~~uses or service establishments §4.27~~
- ~~ii. Retail business service uses~~
- ~~ii.~~ ~~iii.~~ Dry cleaning establishments, or pick-up stations, dealing directly with the consumer §4.24
- ~~iii.~~ ~~iv.~~ Business establishments which perform services on the premises
- iv. Professional services
- ~~v. Restaurants (sit-down), banquet facilities or other places serving food or beverage §4.27~~
- ~~vi. Instructional centers~~
- ~~vi. Retail business or retail business service establishments §4.27~~

- vii. Professional and medical offices, including laboratories
- viii. Fueling station §4.29
- ~~ix. Sale of produce and seasonal plant materials outdoors §4.30~~
- ix. Auto wash §4.32
- xi. Bus passenger stations
- xii. New and used car salesroom, showroom, or office
- xiii. Other uses similar to the above uses
- ~~xiv. Tattoo parlors~~
- ~~xiii.xv.~~ Publicly owned and operated parks, parkways and outdoor recreational facilities
- ~~xiv.xvi.~~ Accessory structures and uses §4.19 customarily incident to the above permitted uses
- xvii. Public or private health and fitness facilities and clubs §4.34
- xviii. Microbreweries §4.35
- ~~xvii.xix.~~ Brewpubs §4.35
- ~~xviii.xx.~~ Day care centers, and adult day care centers §4.12.2

C. Special Land Uses

- i. Outdoor space for exclusive sale of new or used automobiles, campers, recreation vehicles, mobile homes or rental of trailers or automobiles §4.36
- ii. Motel §4.28
- iii. Business in the character of a drive-in or open front store §4.37
- iv. Veterinary hospitals or clinics §4.31
- v. Plant materials nursery §4.6
- vi. Public or private indoor and private outdoor recreational facilities §4.38
- vii. Mini-lube or oil change establishments §4.39
- viii. Sale of produce and seasonal plant materials outdoors §4.30
- ix. Restaurant in the character of a fast food carryout drive-in fast food drive through, or fast food sit-down §4.40
- ~~x. Massage Establishments, as defined in Section 20 of the City Code~~
- ~~xi. Smoke Shops~~
- ~~xii. Tattoo Parlors~~
- ~~xiii. Places of Worship §4.10~~
- ~~xiii. Theaters, assembly halls, concert halls, museums, or similar places of assembly §4.27~~

D. [unchanged]

Part IV.

That the City of Novi Zoning Ordinance, as amended, Article 4, Use Standards, Section 4.10, is hereby amended to read as follows:

Section 4.10 Places of Worship

A. In the OS-1, OSC, RC, TC, and TC-1 districts, churches and other facilities normally incidental thereto are a permitted use.

B. In the RA, R-1, R-2, R-3, R-4, B-2, B-3, C, PSLR, and NCC districts, churches and other facilities normally incidental thereto are permitted as a special land use subject to the following conditions:

1-7. [unchanged]

Part V.

That the City of Novi Zoning Ordinance, as amended, Article 4, Use Standards, Section 4.12, is hereby amended to read as follows:

Section 4.12 Group Day Care Homes, Day Care Centers, and Adult Day Care Centers

1. [unchanged]

2. Day care centers and adult day care centers are a permitted use in the B-2, B-3, OST, TC, TC-1 districts and EXO Overlay district and a special land use in the OS-1, OSC, and PSLR districts, all subject to the following:

- A. In the B-2, B-3, OST, OS-1, OSC, TC, TC-1, PSLR districts and EXO Overlay district:
 - i. Outdoor recreation areas shall be provided, consisting of at least one - hundred fifty (150) square feet for each person cared for, unless modified by the Planning Commission based on reasonable justification provided by the applicant, with a minimum total area of three-thousand five-hundred (3,500) square feet. All such outdoor recreation areas shall be fenced with self-closing gates. The recreation area may extend into an exterior side yard up to twenty-five (25) percent of the distance between the building facade and the property line.
 - ii. The hours of operation shall be limited to the period between 6 a.m. and 7 p.m. for those facilities abutting residential zoning districts.
 - iii. Facilities shall be located either within a permitted office, or commercial structure, or in a freestanding building on a site coordinated with surrounding development (i.e., traffic flow, parking access, drop off areas, architecture and relationship to other buildings).
 - iv. Screening and landscaping of outdoor recreation areas, recreation area fences and parking lots shall comply with Section 5.5.
 - v. Off-street parking shall comply with Section 5.2.12. and Section 5.3.
- B. [unchanged]

3. [unchanged]

Part VI.

That the City of Novi Zoning Ordinance, as amended, Article 4, Use Standards, Section 4.27, is hereby amended to read as follows:

4.27 Retail Business or Service Establishments

- 1. In the B-2, B-3, TC and TC-1 districts, all retail business or service establishments are permitted as follows:
 - A. Any retail business whose principal activity is the sale of merchandise in an enclosed building.
 - B. Any service establishment of an office, showroom or workshop nature of a decorator, dressmaker, tailor, bridal shop, art gallery, interior designer or similar establishment that requires a retail adjunct.

- C. Restaurants (sit down), banquet facilities or other places serving food or beverage, except those having the character of a drive-in or having a drive-through window, unless otherwise permitted herein.
 - ~~D. Personal service establishments which perform services on the premises, such as, but not limited to: barber shops, beauty shops, copy center, florist shops, locksmiths, home furnishings, photo finishing services, stationers, technology repair, and shoe repair shops.~~
 - ~~D. Theaters, assembly halls, concert halls, museums or similar places of assembly when conducted completely within enclosed buildings.~~
 - E. Business schools and colleges or private schools operated for profit.
2. In the FS district, retail establishments to serve the needs of the highway travelers, including such facilities as, but not limited to, gift shops and restaurants, not including drive-ins are permitted uses.
- ~~3. In the TC and TC-1 districts, the following shall be permitted as follows: Theaters, assembly halls, concert halls, museums or similar places of assembly when conducted completely within enclosed buildings.~~

PART VII.

Severability. Should any section, subdivision, clause, or phrase of this Ordinance be declared by the courts to be invalid, the validity of the Ordinance as a whole, or in part, shall not be affected other than the part invalidated.

PART VIII.

Savings Clause. The amendment of the Novi Code of Ordinances set forth in this Ordinance does not affect or impair any act done, offense committed, or right accruing, accrued, or acquired or liability, penalty, forfeiture or punishment, pending or incurred prior to the amendment of the Novi Code of Ordinances set forth in this Ordinance.

PART IX.

Repealer. All other Ordinance or parts of Ordinance in conflict herewith are hereby repealed only to the extent necessary to give this Ordinance full force and effect.

PART X.

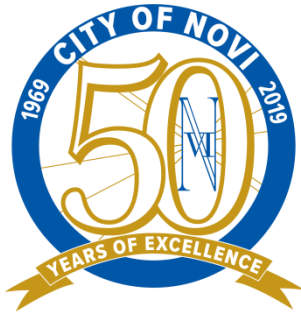
Effective Date: Publication. Public hearing having been held hereon pursuant to the provisions of Section 103 of Act 110 of the Public Acts of 2006, as amended, the provisions of this Ordinance shall be published within fifteen (15) days of its adoption by publication of a brief notice in a newspaper circulated in the City of Novi stating the date of enactment and effective date, a brief statement as to its regulatory effect and that a complete copy of the Ordinance is available for public purchase, use and inspection at the office of the City Clerk during the hours of 8:00 A.M. to 5:00 P.M., Local Time. The provisions of this Ordinance shall become effective seven (7) days after its publication.

MADE, PASSED, AND ADOPTED BY THE CITY COUNCIL OF THE CITY OF NOVI, OAKLAND COUNTY, MICHIGAN, ON THE ____ DAY OF _____, 2019.

ROBERT J. GATT, MAYOR

CORTNEY HANSON, CITY CLERK

Ayes:
Nays:
Abstentions:
Absent:



PLANNING COMMISSION MINUTES

CITY OF NOVI

Regular Meeting

September 25, 2019 7:00 PM

Council Chambers | Novi Civic Center
45175 W. Ten Mile (248) 347-0475

CALL TO ORDER

The meeting was called to order at 7:00 PM.

ROLL CALL

Present: Member Avdoulos, Member Gronachan, Member Lynch, Member Maday, Member Anthony, Member Ferrell

Absent: Chair Pehrson

Also Present: Barbara McBeth, City Planner; Sri Komaragiri, Planner; Rick Meader, Landscape Architect; Kate Richardson, Staff Engineer; Thomas Schultz, City Attorney

PLEDGE OF ALLEGIANCE

Member Ferrell led the meeting attendees in the recitation of the Pledge of Allegiance.

APPROVAL OF AGENDA

Moved by Member Anthony and seconded by Member Maday.

VOICE VOTE TO APPROVE THE SEPTEMBER 25, 2019 AGENDA MOTION MADE BY MEMBER ANTHONY AND SECONDED BY MEMBER MADAY.

Motion to approve the September 25, 2019 Planning Commission Agenda. Motion carried 6-0.

AUDIENCE PARTICIPATION

Nobody in the audience wished to speak.

CORRESPONDENCE

There was no correspondence.

COMMITTEE REPORTS

There were no Committee Reports.

CITY PLANNER REPORT

Planner McBeth said I just wanted to report a few items, two of which were considered and approved at the City Council Meeting this past Monday. The first one the Planning Commission had recently considered, which was the amendment to the Zoning Ordinance that would allow additional building height in the B-2 District subject to a number of restrictions. That was approved for the first reading. The second item the Council approved was the request of Cambridge of Novi for the first amendment to the previously

approved Planned Rezoning Overlay. That amendment was requested to allow greater flexibility for the construction of the individual homes in the development and alternate pavement for the sidewalks and driveways and a few other minor changes. Also, we placed on your table this evening a flyer providing information about the ITC Corridor Trail grand opening community walk which is this Saturday, September 28, 9-11:00 AM at Wildlife Woods Park. The entire community is invited to attend that. Also, one item the Planning Commission and members of the audience might be interested in hearing about is that there are two Public Hearings on the agenda tonight, Golling Maserati and Alfa Romeo and The Scenic Pines Estates. We did hear from Mr. Golling earlier today that he would not be able to attend the meeting. We will ask the Planning Commission to go ahead with the planner's presentation on this item and the Public Hearing to get comments from the public and if you choose to do so to set the decision date in a month from now which is October 30th. So that will be later in the agenda, but we just wanted to announce that in the beginning.

Chair Avdoulos said just for those that didn't hear we are going to hear the Public Hearing for Golling Maserati and allow the public to speak toward it but we're not going to vote on it. We're going to postpone that vote until October 30th.

CONSENT AGENDA - REMOVALS AND APPROVALS

1. 39500 ORCHARD HILLS PLACE JF19-04 – SECTION 9 WAIVER

Approval of the request of JFK Investment Company, LLC, for 39500 Orchard Hills Place JSP19-04 for a Section 9 waiver related to an exterior remodel. The subject parcel is located in Section 36, west of Haggerty Road north of Eight Mile Road. It is zoned OSC, Office Service Commercial. The existing building was constructed in 1986, prior to the adoption of the Façade Ordinance. The applicant proposes to update the building facades to replace the existing primary material, EIFS, with a Flat Metal Panel system.

2. CROSSPOINTE MEADOWS ACCESSORY BUILDING JSP 19-36

Approval at the request of Crosspointe Meadows Church for Preliminary Site Plan with a Section 9 waiver. The subject property is located on the eastside of Meadowbrook Road, south of Thirteen Mile Road in section 12. The primary building on the property is a Church. The applicant is proposing to build a 480 square feet storage building in the rear yard, adjacent to the existing dumpster. No other changes to the Site Plan are proposed.

Motion made by Member Lynch and seconded by Member Maday.

ROLL CALL VOTE TO APPROVE BOTH ITEMS ON THE CONSENT AGENDA MADE BY MEMBER LYNCH AND SECONDED BY MEMBER MADAY.

Motion to approve both items on the Consent Agenda. Motion carried 6-0.

PUBLIC HEARINGS

1. GOLLING MASERATI & ALFA ROMEO JZ19-28 WITH REZONING 18.728

Public hearing at the request of Dorchen/Martin Associates for Planning Commission's recommendation to City Council for a Planned Rezoning Overlay Concept Plan associated with a Zoning Map amendment, to rezone from NCC (Non-Center Commercial) to B-3 (General Business). The subject property is approximately 5.25 acres and is located on the south side of Grand River Avenue,

west of Joseph Drive (Section 24). The applicant is proposing an automobile dealership, a permitted use in the B-3 District, with outdoor space for exclusive sale of new and used automobiles, which is a Special Land Use in the B-3 District.

Planner Komaragiri said the applicant had previously come before the Planning Commission in March with a traditional rezoning request which would have allowed the parcel to be developed with any of the uses permitted in the B-3 District if the zoning change had been approved. After hearing feedback from staff, Commission Members and the concerns raised by nearby residents, the applicant decided to pursue the PRO option in order to be able to clarify their intended development plans and limit the use that can be developed. The applicant is currently requesting a Planned Rezoning Overlay and associated Zoning Map Amendment for two parcels located South West of Grand River Avenue and Joseph Drive from NCC, Non Center Commercial, to B-3, General Business.

The site as you mentioned is in Section 24 has been the location of Glenda's Garden Center for many years which is a non-conforming use in the NCC District. As shown in the PRO Concept Plan the applicant proposed to redevelop the 5.52 acres of the property for an auto dealership with associated outside storage. The proposed dealership building would have a footprint of approximately 17,000 square feet and the parking area consists of approximately 292 spaces. The property is currently zoned Non Center Commercial which allows uses such as retail business and service uses, professional and medical offices, financial institutions, sit down restaurants, and instructional centers. Special Land Use Permits could also allow for low density multiple family or single family dwellings, day care centers, places of worship, public utility buildings, as others as permitted uses.

The current zoning of the surrounding area is I-1, Light Industrial District to the north, OS-1 Office Service District to the west, Non Center Commercial to the east, R-4 One Family Residential to the south. The Future Land Use Map identifies this property and the parcel to the east as community commercial. The parcels to the west along Grand River Avenue are planned for community office. North of Grand River Avenue is planned for Industrial Research Development and Technology and the south is planned for Single Family Residential uses. In this area for Grand River there are professional offices, small retail strip centers, sit down restaurants, and an office complex. Single family residential homes are located to the south of the property.

The proposed concept plan requires a couple of deviations that were identified in our review letters. The first one is along the southern property line. The ordinance requires a six to eight foot berm or wall as a buffer between residential and commercial uses, as well as landscaping to achieve 80% opacity in winter and 90% opacity in summer. The applicant has proposed to retain the three to five foot buffer and many of the existing trees. The tree survey submitted by the applicant indicates all the trees in this area are in fair or poor condition with significant vine growth. In the response letter, the applicant has indicated they will supplement landscaping with evergreen trees as well as large sub-canopy shrubs to provide the required screening as well as clear the vines. Staff is still concerned that the trees are in poor condition should be removed in order to make way for new healthy trees that will be more effective to providing a visual and noise barrier to the adjacent residential area.

Planner Komaragiri continued to say the applicant requests deviations to allow overhead doors to face the major thoroughfare Grand River Avenue and a residential district to the south with the justification that the doors will only be opened for entering and exiting

vehicles and will otherwise remain closed. Staff does not object to this deviation provided that the buffer requirements are met or exceeded. A deviation to allow a reduction in same side driveway spacing is required. It is supported as it allows efficient circulation around the site without a curb cut onto Joseph Drive, which was a major concern for the nearby residents. The curb cut along Joseph Drive was removed and was relocated to Grand River Avenue which would require the waiver because it is too close to Joseph Drive.

The applicant had requested deviations for absence of twelve raised islands in the parking area. In their response letter, several islands had been modified to be proper raised islands but the deviations are still requested for several missing end islands. As you can see in the landscape plan, the plan proposed for a few islands to be painted, but the applicant provided a revised plan where he indicated raised islands, but staff still has concerns about the areas indicated on the plan and we've asked the applicant to reconsider those locations and provide a revised plan.

There are no designated woodlands or wetlands on the property. Landscape review is currently not recommending approval due to number of deviations required from the landscape ordinance. The applicant has stated some of these will be reduced by providing additional landscape islands.

Engineering review found that there are adequate public utilities to serve the parcel and the impacts from B-3 uses are expected to be the same as potential NCC uses. The engineering design manual permits underground detention facilities for storm water collection to be utilized on developed parcels that are proposed to be redeveloped as is the case with this property. This determination was revised from the original review and the revised letter recommending approval was included with your packet.

Traffic consultants have reviewed the anticipated traffic generation from the proposed use and found the impacts are expected to be less compared to what could be developed under the existing zoning. The applicant has submitted public benefits being offered to meet the objective of the benefits including eliminating a non-conforming use reducing the number of vehicle trips generated and providing additional landscaping areas along the east and south side yards. Staff feels these are fairly minor in nature and could be achieved under alternate development scenarios.

We would encourage the applicant to consider other ways the deviations sought could be offset with the provision of more significant community enhancements. Staff and consultants are mostly recommending approval of the concept plan although staff recommends approval of the plan to move forward, we still have concerns about certain details of the plan as noted which will need to be worked out. This request for postponement by the applicant provides an opportunity for the staff to keep working with the applicant to address those concerns and come back to Planning Commission with more clarity in October. Tonight the Planning Commission is asked to hold the scheduled public hearing and postpone making the recommendation to City Council to the October 30th meeting. Thank you.

Chair Avdoulos said this is a public hearing. The public has an opportunity to make any remarks related to this particular project. You have three minutes per person. If you do have something to say please approach the podium and if you could, address the Planning Commission, not the audience. We'll take in all the comments; everybody will hear everything as indicated. We won't be making any kind of recommendation until the

next Planning Commission Meeting.

Raju Ramaswamy, 24730 Bethany Way, said I am a resident of Willowbrook Farms Subdivision and a member of the home owner's association board. I'm also one of the immediately impacted residents south of the proposed rezoning area. As I mentioned in the last meeting too, I am not really against the progress of development in the Grand River Avenue Corridor and am certainly thankful to the City Planning Commission for recommending the applicant to use a Rezoning Overlay Concept Plan. We as residents of Willowbrook Farms Subdivision expect some sort of landscaping and buffering requirements on the south side of the property. Based on the resubmitted proposal from the dealership, I'm a bit surprised they're not offering any improvements on the buffering. This raises a lot of concerns and contributes to be one of the most important factors that impacts the residents on the south side. My request to the Planning Commission is to reconsider the buffering requirements or make sure the dealership provides adequate buffering. I would like to see at least a minimum of a six to eight foot wall. I did hear that there will be some trees to be planted, taking care of the older trees now, but it will take quite a few years for the trees to grow to sufficient height to cut down the noise and light pollution.

Andrew Phillips, 24710 Bethany Way, said I live right behind the planned development. I agree with my neighbor Raj. I have significant concerns about the buffering of the property between residential and B-3 usage. The trees there now are more than 90% in poor condition. You can easily see through to the existing property. In the last meeting there were a number of improvements I would have loved to see in the plan. The one I really cannot fall back on is the addition of a wall between my property and the property behind us. That's something I personally as a resident could not feel comfortable about with the approving of this particular project not only now but for the future of what could be there after the FCA lease is up. I couldn't help but think that the number of deviations that are being requested on this leads to be the best use for this piece of land.

John Waack, 24841 Joseph Drive, said I'm the first house right behind the proposed dealership on Joseph Drive on the west side. I have a number of concerns. Some improvements have been made since the last meeting, but to me there are too many deviations to let this plan move forward. My concerns regard the lighting and the storm water management plan. I don't understand how it will work going underground. There's a huge retention pond that's used now by Glenda's and I would like to show you a couple pictures, if I could. The pictures show the top of the berm, and the retention pond that's currently there; this was about fifteen years ago. I don't see how the current plan will prevent some water issues that are not limited to just this situation, but I have seen it worse. There are more pictures of Joseph Street and how our street looks after we have a decent rain. We already have plenty of water. Some driveways fill right to the edge of where cars get parked. I don't see how the plan is going to work in this current situation. The other problem with that is when they make changes.

Mr. Waack continued, there are a couple new curb cuts, the one is really close to Joseph Drive and I'm not sure that's such a great idea. There's a future building expansion noted on the plan, we can't comment on that because we really don't know what that means. I would like to say the Master Plan at one point said NCC Zoning was not be any type of automotive buildings, it's changed now, it does not say that, but it used to. Needless to say, when it comes down to it this development is using up four of the five acres and will be cemented over. There's hardly any green space left and it doesn't seem to fit the mold of living here in Novi. I don't think any of the residents have been talked to about

adding a berm or wall to the back of the property. I guess I'll end with, I hate to keep coming here and then it gets postponed, I really don't understand why we're here talking about this when in two weeks or a month maybe some of these things will change and we'll have to go over them again. I would like the process to not be so fractured, but I appreciate your time, thank you.

Nisha Curran, 24801 Joseph Drive, said I'm against the rezoning and I don't think we need another car dealership especially around the residential areas. It's a nice area, I like the greenery. With the car dealership right near the residents there will be 24 hour lights, traffic, and noise by our small street. I don't like the idea of test cars coming down the street when we have a lot of people that are disabled. There are a lot of open properties that they can put a car dealership on, just not at Glenda's. We like the greenery and we like the nature. It seems to be its becoming over developed here in Novi, its pretty sad, that's what I liked about it. I'm hoping this does not go through but thank you for listening and thank you for your time.

Richard Reising, 24750 Joseph, said John sort of stimulated some thought here when he showed the pictures of the water retention pond and the drainage problems we've had along Joseph Drive. Joseph Drive, whether you realize it or not, is a chip seal road and right now it looks beautiful because they just resurfaced it a couple of weeks ago, but typically it is under constant repair. The City says they cannot do anything about it because of the drainage both at the north and at the south ends. I would also like to point out we have wells and septic tanks and I don't know what this underground water retention tank is but it seems like someone better do some sort of environmental study. We don't want our septic tanks and wells interfered with by a structure underground.

Chair Avdoulos said, seeing no one else from the audience wishing to speak, I'm going to ask Member Lynch to review the written responses.

Member Lynch said I'm going to go through the written responses and I will summarize them. We have an objection from Jean Reising, 24750 Joseph, she is primarily concerned about the noise. Another objection from Richard Reising, 24750 Joseph, he is worried about property values. Another objection from Victor Diponio, 24729 Joseph, concerned about rezoning, which will lead to my question after this. An objection from Rekha Hariram, 24826 Joseph, says there's an empty dealership within a mile, they could put it there. Objection from Helen Lear, 24730 Joseph, she has property value concerns. Objection from Jacob Lee, 41033 Scarborough, concerns about noise and greenery, specifically the evergreens on the western border. Lastly, from John Waack, 24841 Joseph, an objection, in summary, concerns about the lighting plan, storm water management, berm, landscaping outside of the property.

Member Lynch confirmed this project is a PRO and not a straight rezoning.

Chair Avdoulos closed the public hearing and asked if we have a motion to postpone this to the October 30th meeting?

Motion made by Member Anthony and seconded by Member Gronachan.

ROLL CALL VOTE TO POSTPONE RECOMMENDATION TO CITY COUNCIL MADE BY MEMBER ANTHONY AND SECONDED BY MEMBER GRONACHAN.

Motion to postpone recommendation to City Council to the October 30th Planning

Commission meeting. Motion carried 6-0.

2. SCENIC PINES ESTATES JSP 18-76

Public hearing at the request of Singh Development for Preliminary Site Plan With One-Family clustering Option, Site Condominium, Special Land Use, Wetland Permit, Woodland Permit and Stormwater Management Plan Approval. The subject properties are approximately 9.44 acres and are located south of South Lake Drive and south side of Pembine Drive (Section 3). The applicant is proposing to utilize the One-family Cluster Option to develop a site condominium with 25 single family detached homes.

Planner Komaragiri said the subject property is currently zoned R-4 and surrounded by R-4 on all sides with RA to the south. The Future Land Use Map identifies this property and the surrounding properties as single family use and a public park to the south for the area shown in green on the map. The applicant is proposing to combine three existing parcels for this development. The site is predominantly undeveloped. However, it does contain two single family homes which are proposed to be demolished. The properties to the West are developed with single family homes and there are some vacant parcels of land to the West. To the North are also single family homes that are within the Lakewood Subdivision. To the Northwest is the Lilley Pond Subdivision. To the East are the South Pointe Condominiums. To the South is vacant land which is part of the City's Lakeshore Park.

The site frontage spans the entire length of Pembine Street. There is no outlet from the side. All existing single family homes to the North are all legal non-conforming with smaller lot frontages and setbacks. Buffington Drive, Henning Drive, and Pembine Street are public roads with a width of eighteen to twenty-one feet with a fifty foot wide right-of-way. They are paved with chip seal pavement and are not planned for asphalt.

The site is surrounded by and has a significant amount of regulated wetlands and woodlands. Our Zoning Ordinance provides a one-family clustering option for similar sites as an alternate development option. The intent of that section is to allow flexibility in single family developments where conventional developments would destroy the unique environmental significance of the site. This option does not allow additional density, but does allow relief in certain developments standards such as setbacks and yard requirements. To be able to use this option, the applicant has to preserve a minimum of 50% natural features to qualify. The current plan proposes to preserve about 53%.

Just a little bit of background, Planning Commission has approved a Preliminary Site Plan for a similar development on this property in 2003, subject to a number of conditions. The current layout is similar and is also using the same option with a slightly different road layout. The applicant has referred to that Site Plan approval and a couple of locations in their response letter. However, the current review is independent from that approval. Staff did recommend some conditions that were a part of their approval which are still applicable at this time. I wanted to share this slide which gives a brief overview of existing site conditions and the proposed impacts before we get into other details. As you can see on the slide, the area highlighted in the blue boundaries are the existing regulated wetlands and everything south of the green line shown on the map is all regulated woodlands on site. The applicant is proposing to clear the woodlands within the shaded area in green shown on the map to propose the twenty-five unit development. The wetland impacts are in the area shown in dark green on the map and the wetland buffer

impacts are in the area shown in red on the map. The impact shown on the map is mainly because of the bridge that is proposed. All the other impacts are because of the grading for the new units. The plan proposes about 0.07 acres of wetland impacts and 0.129 acres of buffer impacts as part of them are temporary. Currently, they are proposing about 219 trees to be removed within the green shaded area on the map. That would require about 438 replacement woodland credits and only seventy-four are proposed to be replaced on site.

The applicant has performed soil boring tests at twelve different locations. The soil type mostly includes clay type with layers of sand. Storm water is proposed to be detained on site with an above ground storm water pond in the North West corner and an underground retention pond south of Pristine Lane (proposed street name). Approximately 2.15 acres of the northern portion of the development will drain to the on-site detention basin and about 1.7 will drain to the underground retention to the south. Engineering staff had some concerns originally that the ground water elevation may be within three feet from the bottom of the underground detention unit however, after reviewing the soil borings and meeting with the applicant and discussing in detail, we are at a comfort level where they are recommending approval with some conditions. The plans that are in the packet are good enough for a level of detail for Preliminary Site Plan, but we will continue monitoring the piezometer readings where requested near the underground detention to be provided with each future Site Plan submittal.

The landscape review notes two landscape waivers that would be required. One of which is completely supported by staff, which is lack of street trees along Pembine Street. The other one is the engineering review requires that the sidewalk to be placed fifteen feet from the edge of the road and then the street trees are expected to be planted between the curb and the sidewalk, however, given the cluster development plan trying to protect the woodlands in the back, the sidewalk is pushed closer to the curb in certain locations and less than fifteen feet in some locations due to which the street trees which are expected to be along the road are pushed farther away. Our landscape review recommends support of the deviation as long as the trees are kept within 15 feet from the road right-of-way.

Traffic review does not note any major concerns, just asks for a few details at the time of Final Site Plan application.

The applicant has provided about twelve different elevations for the proposed units. They are proposing a first floor master bedroom to gear towards older senior citizens. Our façade review notes, they include adequate variations to comply with similar and dissimilar Ordinance requirements which would be reviewed at the time of plot plan review.

The fire review noted some additional comments to be addressed at the time of Final Site Plan which the applicant indicated will be addressed.

Due to the close proximity with the single family homes, the Site Plan has gathered a lot of public interest. The applicant has held two public meetings on their own to gather comments from the surrounding residents. They also noted they met with a few of the immediate neighbors multiple times to address their concerns. There's a resident who lives on the opposite side of Pristine Lane, she was bothered by the headlights shining into her house and the applicant has agreed to provide additional screening on her property to protect her from the shining headlights. Staff also met with the residents and immediate

neighbors multiple times to address their concerns and explain the review and process and a majority of the concerns relate to the site drainage. Engineering review agrees with most of the public comments, but they note the comments can be addressed adequately with detailed engineering drawings at the time of Final Site Plan submittal. Our engineers are available here tonight if you have any questions. The Planning Commission is asked to hold the public hearing today and make a decision on the Site Plan, special land use, and other items. I do want to point out that the motion sheet that was posted online has been revised and the one in front of you is the revised motion sheet. The changes mainly refer to two deviations that the Site Plan would require which refer to the reduction of distance between the clusters and reduction of front building setbacks from the street. There was a little confusion as what the Planning Commission can approve and what would need to go to the Zoning Board of Appeals. The one you have in front of you is the final clarified version of the motion sheet. Planning Commission can approve the reduction of distance between the clusters if they can make a finding that the strict allegation of the distance would destroy a natural amenity such as regulated wetlands and woodlands. This deviation is only requested for one set of clusters, not all. The other one where the Planning Commission can approve a reduction of building setbacks from the streets provided that the applicant met certain conditions that is listed in the Ordinance, which the applicant is meeting with an exception of one which would require a landscape berm on all sides abutting single family districts. Staff would not support a berm because of the existence of the wetlands and woodlands so they have to go to the Zoning Board of Appeals to get a relief from that. But, Planning Commission, if they make a finding that the other conditions are met they can provide a conditional approval subject to ZBA approval. The other two options the Planning Commission needs to make a finding and make a motion is the approval of the one family clustering option. The applicant as noted earlier is proposing to permanently preserve up to 53% of the qualifying area and then a special land use based on Section 6.1.2.C and this is where staff has included certain conditions which were part of the previous approval but are still applicable at this time. We have Todd Rankine from Singh Development with his engineer Mike Noles if you have any questions for them. Thank you.

Chair Avdoulos asked if the applicant would like to address the Planning Commission?

Mike Noles, Diffin-Umlor, said good evening. I'm representing Singh Development tonight. I'm pleased to be back in front of the Novi Planning Commission with another fantastic, luxury development. Scenic Pines is a wonderful opportunity to develop a unique property with significant natural features. As Sri indicated, we are in the R-4 Zoning and we're utilizing the one-family clustering option in Section 3.2A, which provides a framework to allow certain innovations, constraints, and also departures and whose goal is to cluster the homes closely together to preserve and permanently protect the abundant natural resources on this site. The cluster option is not easy to navigate. I'm grateful for Singh Development's patience and understanding while we spent the last year perfecting the plan before you. We are pleased to have secured unanimous recommendations for approval from your diligent staff and consultants. We look forward to discussing the details for our project tonight.

As Sri mentioned, Scenic Pines was Final Site Plan approved in 2003, those approvals have since expired. The City staff at the time, Planning Commission, and residents collaborated on the previously approved plan to identify and resolve many of the conflicts. We carefully examined the record and identified opportunities to further improve the plan. The changes from the previously approved plan include 53% woodlands preservation which was up from 50%, a better storm water management configuration that has been

updated to today's stricter standards including storage for the 100-year event versus the 10-year event, an approved entry configuration, less impact to the natural features, and elimination of a contentious wall. We also engaged the neighbors to listen to and address their comments. I would like to thank the thirty-five neighbors who actively and courteously participated in our informal meetings. I would especially like to express my gratitude to Dorothy and Mike Duchesneau who helped to coordinate communication with the neighbors and help coordinate our informational events. All in all, we held two public meetings at the Novi Public Library which lasted three hours each. We attempted to answer all questions and the input helped guide the plan before you tonight. We've exchanged over thirty emails with residents, held multiple one-on-one meetings, fielded over twenty phone calls, and prepared dozens of specialty exhibits to clarify and communicate our proposal. We didn't always agree on every aspect and I'm sure you'll hear about that tonight, but many concerns were addressed and the process greatly reduced rejections to the development.

I won't belabor my remarks by reiterating Sri's thorough report, but I do wish to highlight a couple of bullet points. The density: the twenty-five units proposed are consistent with the previously approved plan, the Master Plan, and the cluster ordinance restrictions. The 9.45 acre property would technically yield thirty units under strict adherence to R-4 zoning rules. The right of way: the proposed 0.17 acre Pembine Road right-of-way dedication associated with this plan greatly improves the configuration of the City street that was built long before construction standards were enforced. The roads in the Lakewood Subdivision meander in and out of the prescribed right-of-way and this additional property dedication to the City helps resolve that issue. The wetlands: the existing 1.7 acre on-site wetlands are minimally impacted under our proposal and an EGLE/MDEQ draft permit was issued for the site last week. Trees: 53% of the regulated woodlands will be preserved. They will be protected by a conservation easement. Interesting fact, in the last sixteen years since the old tree survey was done, the number of regulated trees has increased, but the number of trees called for removal with this proposed plan has actually decreased. We are going the right way with this and we have some fantastic slides if you want to see how we managed to do that, we can show you that as well. Mitigation is proposed in strict accordance with the City of Novi replacement requirements including a bond for saved but at risk trees. The extension of the water main through the Lakewood Subdivision and looping of the water main is a definite benefit for all the residents of Lakewood. That subdivision previously had a special assessment district that was only partially completed. Singh is bringing the water main through the Lakewood Subdivision to serve Scenic Pines so a future dig will not be required, which eases the burden on residents should they one day want to hook up to the public water supply.

We have spent a considerable amount of time discussing the drainage on this site and the surrounding properties so that our neighbors can see graphic depictions of where their issues really lie and how the Scenic Pines proposal helps them and in no way exacerbates their situation. I can go into further detail if you wish, but the City staff has also looked at this proposal in detail and has also issued a recommendation for approval. Thank you for your time tonight. I hope we can count on your support and I'm available to answer any questions you may have.

Chair Avdoulos said this is a public hearing, if there are those who wish to address the Planning Commission on a Public Hearing, please approach the podium. Please state your name and address and please address the Planning Commission and not the audience. We are here to listen to your concerns and then we'll address it during our discussion. Thank you.

Gary Zack, 359 South Lake Drive, said on September 25, 2002 a previous developer was planning to develop the same sensitive wetland and woodland area and was meeting with the Planning Commission. After significant discussion of citizen input, a final motion was made regarding Scenic Pines Estates SP01-63B to approve the Preliminary Site Plan with several stipulations including but not limited to the following: One, the Planning Commission approval for a brick screen wall in lieu of the required thirty inch tall landscape berm abutting Pembine Road. Two, subject to the understanding that the Site Plan does not carry with it, approval of the lake access lot. Three, conditional on no lots encroaching in the wetland buffer. Final Site Plan requires additional significant detail of the Northwest corner, engineering issues being satisfied as well as DEQ permit being obtained. Four, the Site Plan shall return to the Planning Commission for Final Site Plan approval and be subject to the comments on the attached review letter being addressed at the time of Final Site Plan review. The concerns of the residents and neighbors remain the same today as they did seventeen years ago. I recommend that the Planning Commission incorporate the wording of the final motion from September 25, 2002 regarding SP01-63B and any motion made today regarding the current Scenic Pines Estates JSP18-76. It is also very important that citizens have a chance to review the final plans and provide comment. I do not recommend allowing administrative approval of the final plan. Developers go into these projects eyes wide open and know the restrictions and City ordinances. They should not receive or expect large numbers of deviations and variances for it defeats the intended purposes of the Ordinance. If they cannot work with the guidelines they should not pursue development of the property.

Howard Katz, 1155 South Lake Drive, said I am appearing on behalf of the condominium association to the Northwest. One of the issues nobody has addressed yet is the wetland water table that extends to the northwest into South Pointe Condos. I didn't see any boring samples taken there. The drainage according to the plans is going to the west and going to the north and I believe that the only water that is going to come through that wetland is going to come from those seven houses on the plan and they're going to keep dumping more water. We see the water level right now as pretty high, it's just going to get higher because that water has no place to go. It's a very moist area and to quote the engineer, he says the borings genuinely indicate major problems for installing basements. The builder is looking for trouble because this is not a suitable parcel to build. More importantly if you look at the plan of the development itself, they're going to bring in tons of dirt. They have to build up that whole level five to eight feet tall, where's the water going to run from there? When it runs to the Northwest, it's going to go down into the wetlands and it's going to come to the north and flood our homes. We're a senior citizen development. That water is going to come up and we have no recourse whatsoever.

Mr. Katz continued, if you approve this today and administratively accept them without any input, you're doing us a disservice. You're going to raise the ground level another seven to nine feet and you're going to be driving down South Lake Road and looking at these towers going over the trees and you're going to cut down all those trees anyway. You're going to destroy the whole natural beauty of a piece of property you have here which is one in a million. I would recommend that you see the final plan, and then you give us an opportunity to come back and look at the final plans because the builders going to have to adjust. He's taking advantage of a lot of zoning requirements and ordinances. He's asking you to give him a break because it's to his benefit. He could eliminate a couple of houses off the plan and not have very many variances, he chose not to, he wants to maximize the houses and he's asking you to help him do it. I just don't think it's the right thing to do.

Gerald Montes, 128 Buffington, said the first thing that I want to get into is something that is going to affect the future owners of Scenic Pines and it's the borings that were taken in place by the engineers. The water table is so great they recommended that this is going to be a problem site for quite a few of the units. According to a study, it's in the plans, on page 7, this is a recommendation by the services of McDowell and Associates that they would be engaged for all soil and footing extractions and placements. In order to do tests on each foundation setting which will include a density test after the hole is dug to place the foundations and that the foundations be extra-large to support the soil which is not be given enough time to settle. You're going to bring in that much fill, you need to have soil densities done at different levels as the soil is placed. They're going to have basement problems and cracks. In Texas, it's 25 years before you can build on any type of fill.

Mr. Montes continued, the entrance for the trucks coming in to Buffington and leaving on Henning for the fill is another concern for me. South Lake Drive is considered a B-class road with a weight limitation of 18,000 pounds for all weather conditions. It says single axels are 20 tandems or 36,000. No through traffic because of the bridge on South Lake Drive. Henning and Buffington have very small entrances, there's also a sewer cap to the right for the entrance into Buffington, that's for all the main sewers that connect into South Lake Drive and all the remaining streets. For the amount of trucks and that amount of fill, it's probably going to be one hundred to three hundred semi-trucks. The turning radius for a semi-truck single trailer is forty feet out of the corner. As it stands now that truck approaching South Lake Drive having to turn on to Buffington is going to have to access the opposite side of the road into oncoming traffic. Hopefully people slow down and with the way people drive there now, I doubt it. The trucks are going to tear out our new curbs that we put in this year. It's also going to run over the City sewer which is right at the corner base. I recommend that this be denied and wait until these conditions are corrected. The developer says they are responsible and they will maintain the road, but for two to three years the residents are going to have to live with a torn up road. For that reason I would ask that you would deny their request for approval. Thank you.

Michelle Werner, 135 Henning, said I live about midway down the block from the property. I want to talk to you today about basements and groundwater. We have the only house on Henning Street with a full depth basement. We live in a house that never should have been allowed to be built. Thirty years ago, a different developer came before this Commission and said the same things the developer is saying now. They were wrong thirty years ago and they're still wrong now. I and the previous owners of our property have been fighting a battle that's expensive and unending for thirty years against groundwater encroachment because the water table is just as high as they found it to be in April. We replace our sump every twelve to eighteen months because we pull that much silty water through our basement. We have had to have our foundation resealed twice, it has major problems because fill settles harder when you have groundwater running underneath it and it does not settle as evenly as when you built up on dry ground.

Ms. Werner continued, the developer is selling these houses as low maintenance luxury homes for retirees. They are not signing up to deal with flooding basements and high water tables and flooded backyards and living in the middle of a swamp. Please don't let this developer dig basements on this property. The people that are going to buy these homes, they're not going to know what's underground until they put their life savings into these properties. It's not fair to say we hope that the July numbers were right. It's just not fair to these folks who are going to be retiring and think this is an easy house to live in and finding that they're pumping water constantly. Where will the tens of thousands of gallons

of the sump discharge supposed to go every day? Are those tanks big enough to hold hundreds of thousands of gallons of sump discharge for four months a year? Until you can get a full year's worth of readings to see what's on on that site I don't think it's fair to approve anything, because the water is there and not going away and I don't want to see these folks get hurt the way we were. Thank you.

Robert Harris, 209 Henning, said I'm a lifetime Novi guy. I've lived on the north side for about seventeen years when this project was first brought on so I'm familiar with it. I'm not against the project. What I'm against is that I live three houses in from the project off of Henning on the east side and my garage is sitting in thirteen inches in water three to four months out of the year. As soon as the first thaw hits, it just fills. Mike Noles, who I have spoken to - when he talks about the phone calls and the emails trying to work through things and we have still have not come up with a solution. Todd, my neighbor, deals with the same thing. All of our neighbors have flooding and when Mike tells me it's going to divert the water away and I can't understand how that is. The engineers have no idea what's going on. I don't know who to believe. I've emailed City Council, I've been in front of them and no one can come up with a solution of what's going on at my house and what's going on with South Pointe Condos. It's just concerning that were building another project less than two hundred feet from my house. I'm really concerned about my house and myself and I don't find this to be anything that's conducive to help me out until we find out why the drainage is going on. We sit on wetlands and it's pretty wet back there. I know it better than anybody, we do have to address it, the project is sitting on both sides of wetland preserves and they're talking about putting it up on fill. So it falls on you guys to see where it's at before we approve this. Thank you.

Gerry Cooper, 155 Buffington, said I'm right across the street from where the developer is going to put the pond. They're running the water back towards Pembine. There's no pond there now, there's no water there now, it sheds to the back, it runs to the south so were taking water and bringing it to the road. Across the street I have a pond, if the proposed pond ends up being higher in elevation than what my pond is, that's going to fill my pond with what's going to end up in my basement. There are twenty-five condos being put in and fifty vehicles going down the street. You're going to bring in all these giant trailers full of dirt over capacity. It's going to ruin the roads and the houses are going to get ruined that are on Buffington and Henning, they're going to flood out and the liability lies with the City.

Danielle Fasseel, 1185 South Lake Drive, said I live right at the end of Henning. Mostly I'm here just to say I agree with most of my neighbors. I am very concerned about the water, especially the runoff. If we're going to be building up these houses seven to eight feet higher, were going to get all the rain water and sump pump water, I know they're saying this can be contained, but I know my neighbor's yards flood in the spring almost all the way up to their houses so if this does go in and it does increase the levels, what is that going to do to everybody that already has houses there? I was shocked to learn they were going to put in basements just because I know many of the neighbors with basement problems. Because of how high the water table is, I feel like it's just asking for problems for all these people who are going to be buying these houses. I also agree with the fact that they should have to bring back their final plans so the neighbors can have final comments about what can be done so this isn't just put straight through and approved. With the water problems that are already there and how high quality these wetlands and woodlands are, I really recommend that they decrease the number of houses that they're proposing to put in. I don't know if Buffington and Henning are actually made for an increase in traffic, those streets are crumbling already. I know they

have repaved them already this year, but they're not high quality roads. I'm also concerned, I know you're only increasing maybe fifty cars every day but we're also increasing traffic because of the beautiful park renovation and we renovated the other park and there's so much traffic on South Lake Drive, I would just like this to be a smaller development because all of the people that live on South Lake Drive already know there's a problem with traffic. We all have kids; they cross the streets and people are always speeding. Maybe a stop sign gets put in at Buffington with the way that traffic is going to be coming in and out down those small streets. That's all I had to say, thank you so much for listening to me.

Rachel Sines, 2219 Austin, said I moved to this area for the nature and in the last 5 years it has just been devastating watching all these trees come down. In fact, developments at 12 ½ and Novi Road and 13 Mile Road and Novi Road and now Old Novi Road and even Lakeshore Park, just everything is coming down. There is probably more tree credits that you guys have than places that will ever be able to plant trees. So my question to you is that this development can probably be done without many deviations and variances yet the City tends to bend over backwards and give the developers whatever they want. I would challenge the City to hold the development to the current standards, deviations, and variances and limit those so our community wouldn't be as impacted as it is now.

Xiaoli Xiao, 29785 Lilley Trail, said my concern is about Buffington or Henning Road being pretty narrow. Both sides are private parking so I guess that the people and traffic, at least a portion of the traffic, will travel through Lilley Trail, which I do not like. The second concern is to the south of Lilley Trail is zoned as Residential Acreage so I would like to know if the City of Novi also plans to have that developed because I hope not. Thank you.

Dorothy Duchesneau, 125 Henning, said my home is also one of the homes where the backyard tends to flood. It's like an anniversary tonight. Exactly seventeen years ago, on September 25, 2002 Scenic Pines Version 1 came in front of the Planning Commission as Site Plan 01-63. It was the only item under Public Hearings that evening. According to the minutes, the Planning Commission was here until 12:35 in the morning that night. Many concerns were brought up then by the neighbors, but it was approved that night with certain restrictions in the Motion to Approve.

Ms. Duchesneau continued, Scenic Pines Version 2 now comes before you with a different builder involved. One who it seems has read the issues brought during Scenic Pines Version 1 and has addressed many of them up front. There are still some issues with details that need to be explained and worked out. I believe it is in the City's and the neighboring resident's best interests to still include some of the restrictions put on the development back in 2002 on the 2019 version. As a Preliminary Site Plan there are still unanswered questions that won't come up until further engineering work is done on the project. The actual answers to these questions may dictate a change in the plans of the developer or the scope of the project. For example, the recent soil borings have shown high water levels in several areas of build. Planning basements in these areas even if staying within the two and a half story height, will require substantial grading changes to bring the basements underground to comply with our ordinances for building heights in an R-4 cluster option. At this time, all we know is the amount of fill that will be required to create the bridge, 2,100 cubic yards worth. That by itself will be about twenty-five big semi-truck loads and trips for just that small area. It's approximately twenty-eight cubic yards to one big truck.

Ms. Duchesneau said the previous motion also approved a brick screen wall and I just

want to bring that up because at that time, at the front of the development the motion required in lieu of the required thirty inch tall landscape berm abutting Pembine Road a brick screen wall would be applied. That helped to facilitate the continuing flow of water onto the City owned property of 2.4 acres on the northeast corner that had been bought by the City years back to help with storm water management back when South Pointe Condos were developed. The same for the northwest berm, which helped the flow south towards the proposed retention pond area. Even though berms are required by the City, the creation of berms along Pembine in this case, were deemed to hurt, not help the water run off by staff at that time. There will be more than enough tree credits left over to more than adequately shield Scenic Pines from Pembine by creating a small forest on both sides of their entry road. Additional pines in Scenic Pines would be appropriate. Another important condition stipulated at that time was that no lots encroach into the wetlands buffer. The most important condition added to the motion at that time, the Site Plan shall return to the Commission for Final Site Plan approval and subject to the comments on the attached review letter being addressed at the time of the Final Site Plan review. This is the most important to me. By returning to the Commission for Final Site Plan approval rather than just as administrative approval stamp, the residents and neighbors will have a chance to make final comments on the rest of all those details we don't know about now. If this gets approved tonight please make these conditions as part of the approval. There are too many loose details that are not required to be answered in the preliminary approval process especially with the location of this site. Thank you.

Tod Neff, 217 Henning, said I'm the last house on the left, which I think I will be affected the most because everyone around me has filled in the swamp, the condos behind me, everybody else, and now you're going to push water over onto my side. I have pictures on my phone of how high the water is. I've never seen it this high. My furnace is the crawl space and I've never replaced it since I've built that house. I've been on this property for a long time, haven't seen the water this high since this year. Now we're going to build and push water and affect me more. I can't have that. I hold you guys responsible if my crawl space gets flooded. Please don't allow this.

Mike Duchesneau, 1191 South Lake Drive, said my front door and entrance as well as my mailbox is on Henning Street. You probably have received this morning the summary of my concerns that I have been raising and asking for answers to these questions. The staff has been very helpful as far as reviewing and communicating with the developer to try to answer some of those questions. There are many items that are left unanswered. My letter was written at the staffs request because I have been so concerned and have been identifying things for months. Many of these items have been on the original list back in April when we met with the developer who was very cooperative as I say in my letter and I'd like to make that letter a part of the record. I'd like to see this item tabled so the applicant can address some of the concerns and items listed by the staff. I recognize this is not a PRO, but the proposal seems to have many built in variances and items needing to be addressed. The soiling borings summary should be enough to alert anybody that these are not typical houses with typical basements. Side and rear elevations were not provided and I've been told the front elevations are all that's required for this particular project. The side elevations and rear elevations would have shown where the patios and decks and the drainage as far as the side of the hill. Screened-in patios do not meet the setback requirements. I've been told through staff that the applicant is proposing to not have any screened enclosures. If you're going to hold that to them, that should be part of the motion. But that wasn't my purpose as far as restricting that, my purpose was just to identify things so they wouldn't have to go to the Zoning Board of Appeals. My concern is mainly about drainage. I've suggested, repeatedly, that we have a twelve foot setback

between the edge of road and the edge of sidewalk. I understand in talking with staff, that generally they follow the item that goes with the back of the curb, they use a different setback calculation, but there's also in that same set of standards that there's a twelve foot setback required when there is no curb. So this is an item. Many mentions were made of the previous approval. The applicant fails to mention that the previous approval was for twenty-four houses not twenty-five. The previous approval also had conditions in it. The Preliminary Site Plan that was approved seventeen years ago also said that it was conditional on no lots encroaching into the wetlands buffer. The Ordinances say that the City has to determine that this is in the public's best interest to encroach into a wetlands buffer. Proposals should also come back as was mentioned to the Planning Commission for Final Site Plan approval. This was basically mentioned a few times, it was part of the original Preliminary Site Plan recommendation. They've done a lot of work, don't get me wrong, because they really have come a long way and they've addressed issues. I'm just not sure they're there, and I would like to see, if you choose to push this forward today, that certain amendments be made to the motion. I'm kind of disappointed that this is such a flurry right now and that the package that we are seeing doesn't include the most recent changes as far as what goes to ZBA because what the package to the public says is that it's going to go to ZBA for certain variances, I don't understand. I guess we should just kind of address this thing and get a good package and more answers. Thank you.

Tom Skrobecki, 132 Henning, said I would just like to agree with my neighbors and object to the development on many of the same reasons: construction traffic and construction noise. Our road is unimproved. We currently have sixteen houses on our street, it's a very quiet street, and I don't know why we would more than double that. I also believe it is a very risky development. It's been for sale for twenty years. No one has developed it for twenty years, why would that be? We went through this in 2002, it got rejected, it never got built, tried again in 2006-2007 bought other lots on Buffington and I question the City's value with trying to go forward with it.

Chair Avdoulos said, seeing no one else wishing to speak, Member Lynch could you review the written responses.

Member Lynch said we have a few response forms. I'm just going to summarize these. We have an objection, Marc Kennedy 1201 South Lake Drive, primarily concerned about the traffic. Another objection, Patricia Koonter, 29740 Lilley Trail, concerned with road deterioration, traffic, noise. Ann Smith, 226 Henning, concerned about loading and unloading, construction site traffic, wetland concerns. Mike Duchesneau, he just spoke and pretty much summarized his findings with pictures. We have Virginia Runyon, 1155 South Lake Drive, concerned about the wetlands and water level. Objection from Gwendolyn Martin, 1127 South Lake Drive, concerned about wetlands and traffic. Lois Nugent, 1155 South Lake Drive, concerned about wetlands and drainage issues and traffic. They're all objections. To summarize the concerns: wetlands, drainage, traffic, and wildlife.

Chair Avdoulos said those will be in our public record for anyone who wants to take a look at what the concerns are but I think we will be addressing mainly everything the residents have concerns about too. Chair Avdoulos closed the public hearing, and turned this over for the Planning Commission's discussion.

Member Anthony asked if the houses include basements?

Mike Noles said they will.

Member Anthony said how high will you be building up fill for these homes? Will fill be needed under all the homes or just a few select homes?

Mike Noles said no, we will be filling the entire site, but it varies how much. For example, on the very south end of the site the existing elevation is at 942 which is significantly higher so over there those are going to be standard basements. This came up when one of the folks was trying to see if our basements are considered basements under the definition of basements in the Novi Ordinance. So I picked the worst-case cross section which is a walk out basement and you can see the basement floor is at 936. Sri mentioned we'll have to do additional piezometer readings to show what that level is. The scientists are determined that the ground water is at 931.

Member Anthony said so in that particular case, how much of that is built up with fill?

Mike Noles said so it will be about zero at the back and then at the front it will be about eight feet.

Member Anthony said so you will not be putting fill that will actually elevate the homes so that the bottom of the basements are elevated?

Mike Noles said to a certain extent there will be some of that. It varies on the grading plan, but we have to match the existing condition with the existing grade ten feet away from the house so there will be a slope down from the house but at the back of the property the fill is zero, at the house it's probably a foot and at the front of the house it's eight feet.

Member Anthony said so it's likely every house will have some degree of fill, but towards the front and no fill towards the back?

Mike Noles said that's correct.

Member Anthony said so there won't be really any adjustment to the bottom of the basement versus current elevation?

Mike Noles said yes, but it's tough to generalize twenty-five houses and the grading without actually looking at the grading plan. Mike Noles showed a map where there was a higher elevation, 942, and the road at 942. There won't be any fill in there. That will be a cut. Where there will be walk out elevations there's already a slope throughout there. He showed a line that was highlighted in blue showing a 934 elevation.

Mike Noles continued to say the water does go through all these Lakewoods backyards. We're not even touching that contour line with any of our development. So if you take the water that naturally flows off of this property, in every direction it's going to be seeing less water going into that direction. We will capture nearly everything, not 100%, but a large percentage of water within the limits of disturbance. We will put it into the detention basins which discharge at this location and at this location (shown on the map). We are not pushing any of our water to the east, were not pushing any of our water up to the Lakewoods. One of the problems is that the neighboring condo development, you can also see we took their engineering design and overlaid it onto a plan so people could understand how that was supposed to work. They have several inlets along the property

line, the rear yard property line that was supposed to allow water to enter into their system and discharge it at the south end. All of this water drains to the south and it even has a drainage easement across our property in this corner to allow it to keep going across the property. We're not doing any disturbance in any of that area. We're not adding water to this area. We're taking water out of the area, were sending it to the west and they're going to see a reduction of water going into that but their big problem is that the neighborhood was never graded properly. That 934 elevation is the same from the south lot all the way to the very north lot, that's not how you design a site. This site was designed a long time ago without any slope to their rear yard drainage. There are some maintenance issues with their neighbors with the inlets not picking up water, but this development has nothing to do with that. What this development is going to do is pick water up in our storm system and store it in our detention basin.

Member Anthony said before you go any further, I want to finish where I'm going with this. The outer blue line on your drawing is elevation 934. What is the significance of 934?

Mike Noles said so what I was trying to illustrate to the residents is in the area where the water is backing up, we don't even hit that contour line. Their water problem is below the existing conditions. We're capturing our storm water, were storing it, and were discharging it to the west. We are treating the storm water, and storing it for the one hundred year event so we are not exasperating their initial problem.

Member Anthony said what's the significance of the 934? Is that telling me that's the top of the surface water? Or are you just saying that's the area of where the surface water flows?

Mike Noles said what I was trying to show was that it's flat as a pancake through there. When the water rises up to the level of the 934, that's the shape of it. It's just a huge flat wet area back there with no slope to it.

Member Anthony said okay I got it now. So what is the elevation of the bottom of your deepest basement?

Mike Noles said I just have the one example with me which was 936 as the depth of that basement which is five feet above the ground water table.

Member Anthony said okay and just so you know, I know you guys do very good work. Singh is a very good developer. So where I'm cutting you short is I'm trying to just get through a train of thought without going on too long. So your bottom basement is at 936, so I realize different people within your team look at different parts of the reports that come in, are you familiar with the geotech report? How many wells or piezometers did they use on this site?

Mike Noles said there were six, I believe. They were all read and were reading the 931 elevation. They also did a ground water study.

Member Anthony said and how many episodes over what period of time did they gauge those wells?

Mike Noles said they only read the piezometers twice. One was at initial installation and really is just to make sure that the water is flowing at initial installation. They have only had one additional reading since then.

Member Anthony said and the time span between that?

Mike Noles said so the second one was July 9th, 2019 and the first one was a couple months before that.

Member Anthony said so you're looking at May and July?

Mike Noles said yes. Here are the readings from the piezometers. They had six of them. Piezometer number one is here in the detention pond that turned out to be all sand and that reading was at 93070 and that's a good indicator because it was really sandy material. Piezometer number six was of interest to us because it was right next to the underground storage detention area which I have highlighted in blue that was also 930. Piezometer number twelve was confirmation of the others and you have this all in one nice line all 931. There were three others that were off slightly. Two of them were at 93150 half a foot higher, but they were in stiff clay so it's harder to get a good ground water reading when it's in stiff clay because the groundwater doesn't move as well. The geotech scientist with McDowell and Associates determined that number eleven, which is over here on the high mound - that it was actually perched water.

Member Anthony said did McDowell state that they were concerned about basements and what the elevations of the basement would be because of the groundwater?

Mike Noles said no they didn't say that but what they did say is that it's a challenging and difficult site and they recommend that their services are engaged so that we can make sure that we don't have any problems. They wanted us to be up and above the ground water with the basements so the sump pumps were not constantly running. The tests that were mentioned earlier are standard practice, every time you dig a foundation you go down and test the compaction at the bottom of the hole.

Member Anthony said just for clarification, there's two separate things, one is compaction, it's pretty standard that when you're bringing in fill material you're compacting in six inch lifts by achieving 95% compaction or more and you have testing on site. Everyone does that. I've seen Singh projects and they hit that nail square on the head. The other issue though is the groundwater and that you can't determine by a field observation during construction it's something that has to occur over time. Just so we don't blend the two because I've noticed some of the public comment had blended the two together and those are distinctly separate.

Member Anthony said when I first looked at this I thought this is not really intensive, I drove the roads and I drove the area and I thought wow this would be nice, I like the Singh product. I thought the Site Plan was really quite well done in preserving all the wetlands, but once I got to that point I saw that the roads and the sidewalks went over one of the wetland areas and I'm sure that's where the bridge is, but that immediately triggered my concern with shallow groundwater because beyond the bridge when we have shallow groundwater. As groundwater comes up we hit the freeze/thaw cycle, we get liquefaction with soil. Then you begin to get failure under your big surfaces, your driveways will shift, your sidewalk will crack so you do run into those problems on infrastructure. Some of the concerns here were basements so that's why I just put you through these questions on depth and groundwater. One of the solutions I have seen, is to raise the elevation of the base of the homes, bringing it above the water table.

Member Anthony said July is our dry season; five foot fluctuation in Michigan is pretty easy, so it is important knowing how that fluctuates throughout the interior. The sump pump is not designed or intended to run 24-7. It's not just in old homes that are struggling with this, there are two brand new developments in South Lyon where this is happening in every home. They didn't have Singh or McDowell, but they are struggling with it. The industry standard of checking a couple of wells or just having one episode checked in geotech is common, but this is a really complex groundwater site. You can see that when you go through the wetlands pictures, you see the surface, you don't know if that's held up from the clay or if that's truly the potentiometric surface. Once you get that potentiometric surface, what's our fluctuation? And if you're a homeowner that ends up with a basement flooding and your sump pump is running constantly there are all sorts of consequential problems. Trying to find someone that's liable it becomes pass the hot potato. I've been in the middle of those. I started thinking through some mechanisms like, I know Singh does a one year warranty on their homes, is there a way to do a three to five year warranty. But as a City we have no legal authority to require that and it really does take that long to sort these things out. Then I went and I saw the regulated woodland and how much of the regulated woodland came up. Then I had to ask myself is this really a developable site? Rick, help me with where else in the City or how frequently have we seen a site that is nearly 100% regulated woodland become redeveloped?

Landscape Architect Rick Meader said it's happened and there have been commercial sites and residential sites. When you develop a site that's wooded you're going to end up ripping out more than half of the woods, that's a fact of life.

Member Anthony said but I've seen the difference between old growth that's not the regulated woodland where we designate the woodland area versus that's just an old tree we want to keep. What I haven't seen before out of the seven years of sitting here of this density of a regulated woodland where they have come in and removed half of it. This I really the first one I've seen and I kind of cringe at that. I start to go through the reasons, and I like the product and I'm confident in the builder but I'm not confident in the site. I have a really difficult time supporting the site when I look at we don't know enough about shallow groundwater, and the shallow groundwater was measured in July which is our traditional dry season when the groundwater is at its lowest. The construction over the wetland where they do the bridge will work because that's a much deeper foundation but it's going to struggle where the driveways and the sidewalks are. I worry about how frequently the concrete slabs will fail, and I worry about in the flat areas of the road, will we end up with pockets where you get sinkholes or potholes much easier. When I add all of that up, that's where I struggle with the site and having it developed. I guess with that I'm going to turn it over to the rest of the commissioners.

Member Gronachan said I have a question for the experts. Could you help with the clarification of the ZBA variances/ no variances and what we have the right to approve, I'm a little confused. In our packet it said that to allow absence of a required berm, but then there was this eighty-five foot for the driveways, so do they need a variance, what variances is it that they are going to need?

Planner Komaragiri said I have on the screen the section of the Zoning Ordinance that was compared for compliance against the Site Plan. This is section 3.28 it talks about required conditions for one family clustering option. This item talks about a minimum distance required between two-cluster homes. If it's a cluster of four homes against a cluster of two you would apply the minimum distance required based on the total number of homes. That section says Planning Commission can approve the reduction in the

distance. One of the proposed clusters does not meet the required distance of separation of eighty-five. But Planning Commission can approve the reduction if they feel like the deviation is to protect woodlands or wetlands.

Planner Komaragiri said the other item is the reduction of setbacks from the front façade of the home to the back of curb which needs to be thirty feet. The Planning Commission may approve reduction in setbacks if the Site Plan meets all the conditions. The plan meets all the conditions except the one shown in green and they would have to go to ZBA to get relief from that item. So the Planning Commission can go ahead and approve reduction in setbacks or can do it as a conditional approval at this time.

Member Gronachan said thank you for that explanation. So for clarification, somewhere in all of this, I read that if one house was removed and they were down to twenty-four then they would meet the eighty-five foot requirement, is that correct? That would be on the 22-25 cluster and the 1-3.

Planner Komaragiri said I think that they are opposite clusters so that the distance between the clusters is here. For them to meet the eighty-five feet they would have to be pushed further back into the woodlands.

Member Gronachan said so having one more or one less house would not solve that problem?

Planner Komaragiri said no, it would push them farther away from each other.

Member Gronachan said that's another thing that we will have to address for the resident that brought that up in one of the letters we received. Overall, I concur with our first speaker who asked so many wonderful questions. His experience shows at this table. I am concerned about the amount of water. I too think it's a wonderful plan. I feel at this point that more work needs to be done. I will add to this that I have not sat at this table for a long time, but I have been well versed in the development arena. My concern is that I think the developer has a great plan and I think he is doing his due diligence. Lord knows there's been enough time and experts looking at this but as previously spoken, I don't know if we know enough about the water flow and what it's going to do about those basements. Now I will say, I am an insurance agent so when it comes to flooding basements, it's not my favorite time of year. I live in a subdivision where they couldn't build basements, I'm not saying I recommend that for this project, but we don't have basements in my subdivision and we have a ton of water problems. It's a very old subdivision, I don't know what my subdivision looked like thirty to forty years ago and if it was sitting on wetlands or not but I know what the current drainage problems are. I wouldn't want to be a part of something that could create a problem for all these new wonderful home owners. I'm going to reserve any further comments at this time and wait to hear from the rest of my fellow Commissioners.

City Attorney, Tom Schultz said that because there have been a couple of comments about the water table I guess I just want to make sure that were on the same page with what the Planning Commission's role is in reviewing that question. At the Preliminary Site Plan stage, what the developer is obligated to do is to essentially establish for your engineer, engineering feasibility, but not detailed engineering plans. The developer comes to you with a Preliminary Site Plan that shows compliance with your Zoning Ordinance requirements: how big the lots are, how far setback they are from the roads, things like that, and as part of that they are authorized under your ordinance to ask you

for a couple things for that you're here holding the public hearing on tonight: clustering the units together instead of having them separate single family homes and in that process asking you for some relief from things that Sri just went through. The engineering part, the water part, is really an inquiry on part of the Planning Commission at this point to say what does our engineer say about the likelihood that this development is going to be able to be built. Your engineer at this point is essentially saying it looks like we're going to be able to deal with the engineering issues and the storm drainage issues.

Attorney Schultz continued, our Ordinance doesn't really say whether or not they have basements, you are not the building official, and you're not in charge of grading plans. You're looking at: does the lot layout work, should we allow them to do the cluster to save additional natural features, does it look like they're going to be able to deal with engineering issues, and are there any giant red flags about not being able to build a basement that somebody should know about. So the developer has stood up and said were familiar with this. We are going to have deal with your professional staff as we go on with the development process, but the Planning Commission doesn't really have enough information -- and more importantly -- doesn't have a standard in its ordinance to say you can't have this development because you might not be able to have basements. That's just not your role here tonight. You are detail oriented, but not every detail is yours.

Attorney Schultz said, to address one Planning Commissioner's comment, on a regular basis we actually see more than 53% of trees taken down from a development site, because if the plan meets all the setbacks and can create a buildable parcel that fits the Zoning Ordinance, it's probably going to impact trees. We do see a more significant number of trees taken down in other plans. The ironic thing about that is the developer is here in front of you saying I'm going to cluster these homes so I don't have to come to you on this piece of property with just a subdivision that has big lots and impacts even more trees and more wetlands. That's the question you're really here for, and technically holding the Public Hearing on: do we like this plan better because it saves more trees than it might otherwise and maybe impacts less wetlands? That's the fundamental question. It's your decision but I just want to make sure we stay focused on what that question is.

Member Lynch said I do like the cluster option. I like how it saves most of or a large percentage of woodlands that wouldn't normally be saved. I have a follow up question on that, for all the trees you cut down on the property, you have to put money into a tree credit? Is there any way possible, I'm not a big supporter of this tree fund, instead of donating to the tree credit fund, you can put more trees in areas away from the homes to still give it that kind of rural feeling, but I don't want too many close together that they die.

Mike Noles said yes, that would be a problem, but we are using the tightest spacing that we could possibly use. We would love to plant them on here because it would be cheaper for us to plant a tree rather than for us to pay into the fund.

Member Lynch said okay I'm going to take your word for it. I wanted you to keep the issue in mind, if this gets approved. Another thing, the property that's located by the condos, that's a low area. I didn't want this property causing any damage to an existing problem, number one. Number two is I was looking for opportunities where maybe we can alleviate some of the existing problem and based on what you were saying, it looks like the property or the way you set up the flow plus now you put the retention basins in, the water doesn't actually flow in that direction. You mentioned you were going to have some impact, a reduction in the amount of water that's going to flow into the existing areas and also you mentioned there was a maintenance issue with water flowing out of

this property, this condo. There has to be an ordinance that requires maintenance of this drainage, we approve these drainage systems, somebody has to maintain it. It's not the City, it has to be the property owner. What recourse do we or the home owners have to ensure that these drains are maintained?

Staff Engineer, Kate Richardson, said I know that an ordinance officer and the engineering department have been involved reviewing the swale that's back there that's been clogged. An ordinance officer recently went back there to verify what's going on. South Pointe Condo ended up clearing out that swale. I believe they ended up clearing everything out and hopefully when we get a big rainstorm again they'll see some benefits from that work, but right now it has been cleared.

City Attorney, Tom Schultz, said one of the things since 2002 that the City has more standards on is for each development that's approved that has a retention or detention basin, there's an agreement the property owner is obligated to enter into with the City that says if the owner doesn't maintain the system, the City will.

Member Lynch said so that's one of the benefits of approving this now. If they were to build this in 2003 they were under a whole different set of rules.

City Attorney, Tom Schultz, said I think they still had that obligation. I just like to think over seventeen years maybe the forms have become a little more detailed just as you become more developed as a City and you've improved a little bit. It all helps engineering and helps code enforcement.

Member Lynch said so I do like the cluster option and I do like the idea that you're preserving as much as you possibly can. My primary concern is if I was going to reject this was drainage flow. Correct me if I'm wrong, you're going to come in and do all these drawings, guarantee that the storm water performance as designed and flowing away from this area to somehow alleviate some of the problems there and you're going to put a 120% cost in escrow. You're going to post a performance guarantee and what you're telling us here today is that all this storm water is going to flow in these areas away from that area in the blue with the arrows that you're showing on the map.

Mike Noles said so the arrows that you see, if you notice none of them are inside the development area, that's the existing drainage. Those are areas that we are not touching so if that's what it's doing right now and I'm not going to touch it.

Member Lynch said but what I'm getting at with the number of comments that people are worried about, and I would be worried too, is that here's this development going in and I know I have a bad situation now and this development it's going to make it a lot worse. But we're saying here and what we're guaranteeing is you're going to put in a storm water management system in order to accommodate this subdivision that's not going to create a negative impact on the existing sites.

Mike Noles said that's right, it won't have a negative impact. Now I don't want to broaden that out, performance guarantee is not how the system performs it's for the contractor to perform to install the improvements on the plan and once you've complied with the plan you get your performance guarantee at the end.

Member Lynch said okay but you won't get your approval until the City engineer approves the plans.

Mike Noles said so there's a phasing in there but it's not really performance in the concept of what you're talking about. The other thing that you should know there's multiple drainage areas around the development that go into different areas. There are some areas at the back of the lot by the walkouts that will continue the current drainage pattern so not everything within the perimeter of the development area is going to end up in the basin. Some of them are going to continue on these existing drainage patterns. But what I'm saying is that a great bulk of that water that's currently going that way will be stored, discharged at a controlled rate, and discharged on the other side of the problem area, and that has to help.

Member Lynch said okay that was my concern. At some point this property is going to get developed. I just want to make sure with whatever we do we're not exacerbating an already dicey situation. Based on what I have heard and what I've read in the letters here, I'm comfortable with our engineering department and with the performance guarantees that are in here. They won't approve a storm water management plan that's basically going to exacerbate the situation and I'm confident in that. I guess my overall feeling of the site is that I like the idea of saving as many woodlands as you can, I don't mind the cluster option I think it's more efficient, I do like the idea of the storm water management where there is none right now. Right now there's no directed water flow, at least were going to have directed water flow now with the performance guarantees. That's really a Singh issue with your guarantees and with your homeowner guarantees. Certainly you are cognizant of that because you don't want the blow back. Does the cluster option preserve more area, yes it does. I would like you to save as many trees as you can. Are we causing any more harm in doing what we're doing? I don't believe so, I think there's some checks and balances with it, you won't even get approval from engineering to build it if it doesn't meet ordinance. The last thing that I have written down is that I do share a concern with that narrow road and getting the trucks in and out. My understanding is that if you cause damage the developer will have to take responsibility.

Mike Noles said we have to videotape it at the beginning so we show what the condition is before we start. Ted Meadows is a pretty tough guy when he goes out and does his final inspections and we have to put it back equal or better condition.

Member Lynch said as far as traffic goes, what they do is they shut down the road so the developer can bring in the heavy equipment. It will be backed up, and it doesn't last forever. I'm leaning more towards approving this, I think it's a decent plan. I do like the idea based on what happened with the other project we're working on. Things turned out better than what we initially thought. That was another tough parcel and it was all water drainage issues. I think this project has some potential. Not only do I think it fits into the neighborhood, I think it has the potential for reducing some of the water problems.

Member Maday said I'm not as concerned about the woodlands, the clustering is going to help tremendously with that but I am more concerned about the water. I'm assuming and I'm confident that as this project evolves that there will be work with the engineer back and forth to make sure you're developing a site that's feasible for the people that are potentially going to purchase the property. How long is this taking to do the next phase? Will it be in the spring when you can take some more samples?

Mike Noles said if we move forward tonight we should be starting development in the spring and be fully permitted. One of the conditions that the engineering review made is with each submittal, and we have multiple submittals to make between now and then,

they wanted another set of piezometer readings because they wanted to see those in different seasons. It's a condition of the Ordinance. If you were to support it and approve the plan, that's already written in the Ordinance that we have to do that.

Member Maday said that's what I mean by evolving with the way that these projects work and evolve to work with the City and the developer to make sure it's properly developed.

Chair Avdoulos said before we have a motion, I wanted to respond to Ms. Duchesneau, I don't know but I think I was on that Planning Commission. I was relatively new and we had many late nights and I can't think that far back to remember what was discussed, but you know this property obviously has a lot of concerns and is very sensitive. There are issues with water and with trees and I think we're looking at a vehicle that is offered by the City to create an option where we do cluster more of these homes so that we can save more woodlands and make it more natural. There's a development near my house that has basically done the same thing. I know there are concerns related to construction traffic but I've been very impressed with the City and how they review those projects and if there are concerns from the residents, they do answer them and make sure everything is running the way it should be running. I think based on what I've seen and I appreciate Member Anthony's expertise on a lot of the environmental impacts to the site, but I have one question to Kate. Based on this diagram where the outline is indicating what the developed area is, outside of that is basically left as natural as possible. So this particular development is containing all the storm water within its own footprint?

Staff Engineer, Kate Richardson, said for the most part, like what Mike Noles said, there are still some spots where it is sheet flowing out past its boundaries, but they're not increasing the concentration, or the rate at which the storm water is discharged, or the volume. They are allowed to do that under Michigan Law.

Chair Avdoulos said as the project progresses and we have engineering documentation and obviously that will be reviewed as every project is, I trust the City Engineers and City Landscape Architect to monitor this. I think our due diligence is to listen to make sure that the project follows the process and our engineering teams, our site teams, our staff reviews will be looking to address all the concerns. I think up to this point and from what I've seen and read, I think we're heading in that direction.

Member Anthony said can I just get a point of clarification? The engineering report we're looking at is really just for information and that it's done and submitted and has been approved. Our vote really has no opinion on the engineering side.

City Attorney, Tom Schultz, said so there are communities at the Planning Commission that do not ask for engineering stuff; you do want to see it, obviously. Under the Site Plan section of your Zoning Ordinance says that you want to see engineering feasibility. So engineering gets a copy of the plan and additional details that are shared by the developer, they write an initial review which is what you have in your packet and the planning staff's summary of it. For Final Site Plan, that letter is usually quite a bit more detailed. You go from fifteen things to pay attention to thirty things and detailed engineering plans that you have to get before we give you our final stamp of approval. You get detail that the engineers looked at and everything seems to flow the right way.

Member Anthony said so if I'm hearing you correctly then my vote simply is that the process of submitting to engineering and the review has been done correctly.

Planner Komaragiri said that would be after the Planning Commission approves Preliminary Site Plan and the applicant will start working on the construction drawings.

Member Anthony said I recognize that I'm just trying to clarify my vote.

City Attorney Schultz said so they have submitted the plans that your ordinance requires, and that the engineer typically reviews and comments on for a Site Plan. They have done that.

Member Anthony said okay and that's what my vote reflects.

City Attorney Schultz said can I just say one more thing just because I know a number of people have brought it up. I didn't want it to look like the Planning Commission didn't say anything about it. There was a long motion that was written at the table back in 2002 and a number of speakers have said you should require all these things that you as a Commission did seventeen years ago. I think Sri wanted an opportunity to generally say, in the way your motions are set up now, you're generally doing that because you're referring back to your detailed staff reports which pick most of those things up. There are a couple of things that you should know are not in the motion that were in the motion before.

Planner Komaragiri said so there were two items which we did not carry forward from the last motion. The one is the condition that no lots would encroach into the wetland buffer. They are proposing buffer impacts in three locations. They are very minor. Only one of the impacts is permanent and the rest are temporary. They're going to seed and put them back so we did not carry that forward because it's only happening with one unit, unit 16, where the impact is permanent and because the applicant noted they were trying to make a choice between moving a tree as opposed to impacting the buffer. The other one is the Site Plan shall return to the Commission for the Final Site Plan approval and subject to the comments on the attached review letters being addressed. At that time, after going through the minutes at the moment I think that discussion was brought forward because there were many other Preliminary level concerns that were not addressed so the Planning Commission wanted an opportunity to review it one more time. They have to deal with some additional ZBA variances which are no longer needed because they are proposing to demolish those buildings and some retaining walls and a few other items which were within the scope of Planning Commissions review which was not addressed at that time. Because that didn't happen this time we didn't recommend that as part of the motion.

City Attorney Schultz said but just to be clear, in your Ordinance you are allowed to ask for a Final Site Plan.

Member Anthony said so we would have to amend this motion to see a Final Site Plan?

Planner Komaragiri said yes if you choose to do so.

Member Lynch said before you do that, Mr. Noles there's something I want to address. There's something on here about the future use of a parcel, what is that?

Mike Noles said that is a parcel that is out on the lake that is not subject to this Site Plan request. There was a lot of concern from the residents in Lakewood that somehow this

property would get rights to be able to use that parcel because one of the parcels where the detention basin is located had a right to use it. That is by the parcel number and that parcel number will go away once the property is combined. We have assured them that in no way does authorization of this plan have anything to do with lake access.

Member Lynch said okay, before you made the motion I wanted to make sure that was clear.

City Attorney Schultz said, through the Chair, the motion that is in front of you tonight, because of the questions that were raised over the last few days it is specifically called out in what you have in front of you for the first motion the Special Land Use where you have the most discretion. You are essentially making a finding that that parcel on the lake is not a part of this development and isn't going to become a part of it. I assume that the developer is fine with that from what he just said.

Mike Noles said we are aware, and it was never part of it. It was a legitimate concern that the residents had because it could be a possible connection to it.

Member Anthony said I'll make a motion.

Motion made by Member Anthony and seconded by Member Lynch.

ROLL CALL VOTE TO APPROVE SPECIAL LAND USE PERMIT MADE BY MEMBER ANTHONY AND SECONDED BY MEMBER LYNCH.

In the matter of Scenic Pines Estates, JSP 18-76, motion to approve the Special Land Use Permit based on and subject to the following:

- 1. The proposed use will not cause any detrimental impact on existing thoroughfares *(based on the Traffic review)*;**
- 2. The proposed use will not cause any detrimental impact on the capabilities of public services and facilities;**
- 3. The proposed use is compatible with the natural features and characteristics of the land *(because the applicant is proposing to preserve 53% of qualifying area that includes regulated woodlands and wetlands)*;**
- 4. The proposed use is compatible with adjacent uses of land *(because the subject property is surrounded by single family residential uses. Façade review notes that the proposed elevations portray an overall architectural standard equal or higher than the existing homes in the surrounding neighborhood)*;**
- 5. The proposed use is consistent with the goals, objectives, and recommendations of the City's Master Plan for Land Use *(because the development is age-targeted. The proposed floor plans indicate first-floor master)*;**
- 6. The proposed use will promote the use of land in a socially and economically desirable manner;**
- 7. The proposed use is (1) listed among the provision of uses requiring special land use review as set forth in the various zoning districts of this Ordinance, and (2) is in harmony with the purposes and conforms to the applicable site design regulations of the zoning district in which it is located;**
- 8. The approval shall be subject to the following conditions at that time:**
 - a. The Planning Commission finding that Parcel ID No. 22-03-327-004, mentioned in the notes to the Site Plan, located on the north side of South Lake Drive, is not part of this development and shall not become or be made part of this**

development, as it does not comply with Sec. 36-62, Lakefront use standards, of the City Code of Ordinances as relates to lakefront recreational parks;

- b. Maintenance and reconstruction of the roads during and after construction, dust maintenance control and the stipulation that the roads be videotaped before and after construction to determine reconstruction requirements;
- c. Limit Construction times with respect to elementary school bus schedule;
- d. Construction traffic to comply with the City load limits; and

9. Final Site Plan shall come back to Planning Commission for Final Approval.

Motion Carried 6-0.

Motion made by Member Anthony and seconded by Member Gronachan.

ROLL CALL VOTE TO APPROVE THE PRELIMINARY SITE PLAN WITH ONE-FAMILY CLUSTERING OPTION AND THE SITE CONDOMINIUM MADE MY MEBER ANTHONY AND SECONDED BY MEMBER GRONACHAN.

In the matter of Scenic Pines Estates, JSP 18-76, motion to approve the Preliminary Site Plan with One-family clustering option and the Site Condominium based on and subject to the following:

1. Planning Commission's finding per Section 3.28.1.B, that in all one-family residential districts, the clustering of one-family dwellings may be permitted, provided that the land consists of an unsubdivided area and the proposed Site Plan and, that the conventional approach to residential development would destroy the unique environmental significance of the site, and that the use of the cluster option is a desirable course of action to follow based on the following condition.
 - a. The majority (fifty (50) percent) of the net site area (defined as the area which is delineated by parcel lines, exclusive of rights-of-way as shown on the adopted master plan) is composed of lands that are within jurisdiction of Woodland Protection Ordinance, as amended, Chapter 37 of the Code of Ordinances, or within the jurisdiction of the Wetland and Watercourse Protection Ordinance, as amended, Chapter 12, Article V of the Code of Ordinances, or any combination of such lands. The applicant is proposing to permanently preserve up to 53% of qualifying area on site.
2. Planning Commission approval for reduction of minimum distance between the clusters, based on the finding, subject to conditions listed in Section 3.28.5., that the strict application of the distance in this instance would destroy a natural amenity such as regulated wetlands and woodlands. This is required for the Units 1-2-3 cluster and the Units 22-23-24-25 cluster. A minimum of 85 feet is required, approximately 78 feet is proposed;
3. Planning Commission approval of reduction of front building setbacks from the streets as listed in Section 3.28.4.D. A minimum of 30 feet is required from the edge of Private drive, the plans currently propose 25 feet in order to protect regulated woodlands in the back yards; this is based on the following findings listed in Section 3.28.6.C:
 - a. All the conditions listed in Section 3.28.6.C. from i thru iv are met with the exception as noted below;
 - b. A Zoning Board of Appeals variance from Section 3.28.6.C.iv.a to allow absence of required berm along the east, west and south property boundaries adjacent to other single-family residential districts;
4. Planning Commission waiver for reduction of the minimum distance for opposite-side spacing requirement, Design and Construction Standards Section 11-216(d), for the roadway spacing between Pristine Lane and Henning Street(A minimum of

200 feet is required, 117 feet is proposed, due to estimated low volume of vehicles expected from the proposed development, which is hereby granted;

5. A landscape waiver for absence of three required street trees along Pembine Street Frontage, as listed in 5.5.3.E.i.c and LDM 1.d., due to lack of space between the edge of pavement and the future Right-of-way and conflicts with other required proposed utilities and swales, which is hereby granted;
6. A landscape waiver from Section 2.1 of Landscape Design Manual to allow some of the proposed trees to be located outside of the space between the sidewalk and the curb due to conflicts with proposed utilities, which is hereby granted. This waiver is supported as most of the proposed trees are located within 15 feet from the curb, with an exception of three trees;
7. Administrative approval from Engineering for variance from Engineering Design Manual Section 7.4.2.C.1 for not meeting the minimum distance of 15 feet from back of curb to outside edge of sidewalk;
8. The applicant shall revise the woodland replacement plan at the time of Final Site Plan to avoid the conflict between the proposed tree replacement locations and the existing overhead electric line along the western property boundary;
9. The applicant shall obtain necessary approvals from all related outside agencies for the proposed location of storm water pond and related landscape under the existing overhead lines prior to approval of Final Site Plan;
10. Assurance of the permanence of the open space and its continued maintenance shall be submitted for review and approval by the City Attorney at the time of Final Site Plan approval.
11. The findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters, as well as all of the terms and conditions of the PRO Agreement as approved, with these items being addressed on the Final Site Plan; and
12. The Final Site Plan shall come back to Planning Commission for Final Approval.

Motion Carried 6-0.

Motion made by Member Anthony and seconded by Member Gronachan.

ROLL CALL VOTE TO APPROVE THE WETLAND PERMIT MADE MY MEMBER ANTHONY AND SECONDED BY MEMBER GRONACHAN.

In the matter of Scenic Pines Estates, JSP 18-76, motion to approve the Wetland Permit based on and subject to the findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan. *Motion Carried 6-0.*

Motion made by Member Anthony and seconded by Member Gronachan.

ROLL CALL VOTE TO APPROVE THE WOODLAND PERMIT MADE MY MEMBER ANTHONY AND SECONDED BY MEMBER GRONACHAN.

In the matter of Scenic Pines Estates, JSP 18-76, motion to approve the Woodland Permit based on and subject to the findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan. *Motion Carried 6-0.*

Motion made by Member Anthony and seconded by Member Gronachan.

ROLL CALL VOTE TO APPROVE THE STORM WATER MANAGEMENT PLAN PERMIT MADE MY MEMBER ANTHONY AND SECONDED BY MEMBER GRONACHAN.

In the matter of Scenic Pines Estates, JSP 18-76, motion to approve the Storm water Management Plan, based on and subject to the findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan; and the Final Site Plan must come back to Planning Commission for Final Approval. *Motion Carried 6-0.*

MATTERS FOR CONSIDERATION

1. APPROVAL OF THE AUGUST 28, 2019 PLANNING COMMISSION MINUTES

Motion made by Member Anthony and seconded by Member Gronachan.

ROLL CALL VOTE TO APPROVE THE AUGUST 28, 2019 PLANNING COMMISSION MINUTES MADE MY MEMBER ANTHONY AND SECONDED MY MEMBER GRONACHAN.

Motion to approve the August 28, 2019 Planning Commission Meeting Minutes. Motion carried 6-0.

SUPPLEMENTAL ISSUES

There were no supplemental issues.

AUDIENCE PARTICIPATION

Mike Duchesneau, 1191 South Lake Drive, said I'm really appreciative of you adding the coming back for Final Site Plan approval. I am disappointed in the response from staff as far as the cluster spacing. If one of the houses was taken out between clusters 1, 2, and 3 and 22, 23, and 24 the required setback would change from eighty-five feet to seventy-five feet and then it would be up to the applicant to decide whether to eliminate a house or whether to redesign a lot. I thought that answer was off base. The other question that I had is we looked at a packet and in that packet, Scenic Pines was going to the ZBA. Now, the ZBA is going to be looking at some other things that were listed but not the things in the packet. I find it a little disappointing that the motion went forward without the information available to the public as to what it is we're looking at today. I do thank you for what you're doing and your process and I do respect that you are all residents and are very concerned about what happens in Novi and how it happens. Thank you.

Howard Katz, 1155 South Lake Drive, said there are only two more things I want to say. I don't know what you hired an engineer for because McDowell came back and said you're going to have problems with this site, he says it right here in your letter and he says they're going to do a lot of things that the builder's not doing, but that's going to fall upon you. The other thing I have an issue with when you look at the Site Plan is that no one seemed to address when they talked about the water dispersal system. There are seven houses on the east side, those houses are not connected to that system, their sump pumps are going to run 24/7 and they're going to shoot water into that pond which is wet enough as it is and when the builder says the water is going to the south, it's not, it's just going to fill up that pond and by definition it's not a part of the water retention, at least according to the plan unless they've changed it. The sump pump will be running 24/7 and you didn't address that. You've got all this water going underground to the west but that's to the houses on the west side, that's a problem you're going to have to deal with and nobody said that but I thought you should know. Thank you.

Gerry Cooper, 155 Buffington, said I just find something confusing. Mr. Schultz is saying the only reason that you guys are here is to say that the engineering did the paperwork. Why do you have all of us come down here and actually think that we have input? You said all you people are here to look at the documentation and say if it's put together correctly per the ordinance. If that's all you guys get to look at and that's all you judge the approval by, we don't have any input. You gave them that direction, you guys don't get to make any decisions, you look at the engineering package and you approve it based upon does it meet the ordinance? But you brought all the people down here and say come down here we really want to hear from you and that's not true according to the way that your process works. Think about that because that's the way it happened here today and you know I'm right.

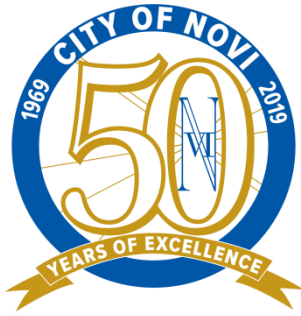
ADJOURNMENT

Moved by Member Lynch and seconded by Member Anthony.

VOICE VOTE ON THE MOTION TO ADJOURN MADE BY MEMBER LYNCH AND SECONDED BY MEMBER ANTHONY.

Motion to adjourn the September 25, 2019 Planning Commission Meeting. Motion carried 6-0.

The meeting was adjourned at 9:41 PM.



PLANNING COMMISSION MINUTES

CITY OF NOVI

Regular Meeting

October 16, 2019 7:00 PM

Council Chambers | Novi Civic Center
45175 W. Ten Mile (248) 347-0475

CALL TO ORDER

The meeting was called to order at 7:00 PM.

ROLL CALL

Present: Member Avdoulos, Member Ferrell, Member Gronachan, Member Lynch, Member Maday, Chair Pehrson

Absent: Member Anthony

Also Present: Barbara McBeth, City Planner; Lindsay Bell, Planner; Rick Meader, Landscape Architect; Victor Boron, Staff Engineer; Beth Saarela, City Attorney

PLEDGE OF ALLEGIANCE

Member Lynch led the meeting attendees in the recitation of the Pledge of Allegiance.

APPROVAL OF AGENDA

Moved by Member Avdoulos and seconded by Member Gronachan.

VOICE VOTE TO APPROVE THE OCTOBER 16, 2019 AGENDA MOTION MADE BY MEMBER AVDOULOS AND SECONDED BY MEMBER GRONACHAN.

Motion to approve the October 16, 2019 Planning Commission Agenda. Motion carried 6-0.

AUDIENCE PARTICIPATION

Mike Duchesneau, 1191 South Lake Drive, said I see for today's agenda under Matters for Consideration you have The Goddard School. I would like to make a few comments on that. The Goddard School is already in Novi and I think it's very nice that they want to add a second facility. I do have one recommendation for consideration by the applicant and the City. Cabot Drive is basically a three lane road with one lane that goes north, one lane that goes south and a left turn lane in the middle. Cabot Drive is very nicely done at 13 Mile Road, but it basically dies out to the center left turn lane and what I would like to ask the applicant and the City to consider is to take the parking lot and make that a one way as opposed to a two way access to the parking lot. If they do that perhaps they can angle the parking spaces and it would be similar to what we see in a lot of school systems as well as other similar facilities like the Montessori Academy that is down the street on 13 Mile Road. If I were to do it I would probably suggest the one way in from the north because shortly after that the left turn lane dies. Thank you.

CORRESPONDENCE

There was no correspondence.

COMMITTEE REPORTS

There were no Committee Reports.

CITY PLANNER REPORT

There was no City Planner Report.

CONSENT AGENDA - REMOVALS AND APPROVALS

1. JSP 17-37 ARMENIAN CHURCH AND CULTURAL CENTER: PRELIMINARY SITE PLAN EXTENSION

Approval of the request of Armenian Cultural Center for a one-year extension of the Preliminary Site Plan with Land bank Parking, Phasing Plan, Shared Parking, Wetland Permit, Woodland Permit, and Storm water Management plan approval. The subject property is located in section 12, on the north side of Twelve Mile Road and east of Meadowbrook Road, in residential acreage (RA) zoning district. The applicant is proposing a church and a cultural center with accessory uses such as multipurpose hall, café, religious educational rooms, Armenian genocide memorial and recreational facility. The applicant is proposing to construct in three phases. The project area is approximately 19.30 acres. A Special Land Use Permit is required to permit Places of Worship and a daycare in residential districts.

ROLL CALL VOTE TO APPROVE THE EXTENSION OF THE PRELIMINARY SITE PLAN WITH LAND BANKING, PHASING PLAN, SHARED PARKING, WETLAND PERMIT, WOODLAND PERMIT, AND STORM WATER MANAGEMENT PLAN MADE BY MEMBER MADAY AND SECONDED BY MEMBER LYNCH.

Motion to approve the extension. Motion carried 6-0.

PUBLIC HEARINGS

There were no Public Hearings.

MATTERS FOR CONSIDERATION

1. THE GODDARD SCHOOL JSP19-33

Consideration at the request of PEA, Inc., for Preliminary Site Plan and Storm water Management Plan approval. The subject property is located in Section 12 south of Thirteen Mile Road and west of Cabot Drive, and is zoned OST, Office Service Technology. The applicant is proposing to construct a 10,957 square foot building, fenced outdoor recreation areas and associated parking for a child day care center.

Planner Bell said the subject property is in Section 12 on the southwest corner of Thirteen Mile and Cabot Drive, west of Haggarty Road and the parcel is just over 2 acres. It is zoned OST, Office Service Technology and is surrounded by the same zoning on all sides.

The future land use map indicates Office Research Development & Technology for this property and surrounding properties.

There is a wetland area that extends on to the northwestern portion of the site, which will not be impacted by the proposed development. However there will be temporary impacts to the natural features setback area surrounding the wetland.

The site is currently vacant. The applicant is proposing a new 10,957 square foot child day care center building, outdoor recreation areas and associated parking. Two access drives off of Cabot Drive are proposed. The applicant has requested and received approval for seven variances from the Zoning Board of Appeals, which were granted on October 8.

Two landscape waivers are also requested, which require approval from the Planning Commission. The first is for the location of building foundation landscaping – because most of the building is surrounded by outdoor play areas, the foundation landscaping needed to be located further away from the building and the total area exceeds the requirement. The second is for a deficiency of 2 large frontage trees provided on Cabot Drive. The applicant states the lack of space between the sidewalk and the curb prohibits them from providing these 2 trees. A third waiver is not required as the applicant has agreed to provide additional parking lot island area in the Final Site Plan.

Planner Bell said, the City's staff engineers had originally not recommended approval of the storm water management plan – however the applicant has worked with them to provide additional information and the required maintenance access to the detention pond. Engineering now recommends approval.

The site plan meets all other Zoning Ordinance requirements, and all reviewers are recommending approval with additional comments to be addressed in the Final Site Plan.

Tonight the Planning Commission is asked to approve or deny the Preliminary Site Plan and the storm water management plan. Representing the project tonight and here to answer any questions you may have is Kevin Campbell, the owner, and his team.

Chair Pehrson said would the applicant wish to address the Planning Commission at this time?

Kevin Campbell said we're excited to be a part of the community. The sellers of the site, Northern Equities, who own a number of office buildings in that area, have voiced their support for our project.

Member Avdoulos said I think based on reviewing what the staff has pulled together, basically every department has recommended approval. Rick, there was the one comment that was related to the frontage trees provided on Cabot. It indicated it's not supported by staff, is there anything we can do with that?

Rick Meader, Landscape Architect, said I think I'm actually okay with it because they do have the street trees along the road and they really couldn't have counted that in their numbers, so I think they're okay.

Member Avdoulos said and the fact that the ZBA has provided the variances. I think in all honesty this is a project that is pretty clean so I'm just going to start by making a motion.

Motion made by Member Avdoulos and seconded by Member Ferrell.

In the matter of The Goddard School, JSP19-33, motion to approve the Preliminary Site Plan based on and subject to the following:

- a. On October 8, 2019, the Zoning Board of Appeals granted variances as follows:**
 - 1. Proposed 19,033 square feet of outdoor recreation area, 23,400 square feet required;**
 - 2. Proposed recreation area occupying 63.86% of the exterior side yard, up to 25% of the distance between the building and the property line allowed;**
 - 3. Absence of a loading zone;**
 - 4. Deficiency of parking spaces, 40 spaces proposed, 41 required;**
 - 5. Proposed dumpster located in the front yard, dumpsters are required to be located in the rear yard Landscape waiver of the greenbelt berms along Grand River and Twelve Mile Road, as acceptable hedge substitutes are proposed, which is hereby granted;**
 - 6. Location of play/shade structures in the exterior and interior side yards, and less than 10 feet from the main building, by code accessory structures must be located in the rear yard and must be located greater than 10 feet from the main building;**
 - 7. Location of a fence in the exterior side yard, by code no fence shall extend into a front or exterior side yard.**
- b. Landscape waiver for location of building foundation landscaping away from building, due to a conflict with recreation areas and total required area is exceeded, which is hereby granted;**
- c. Landscape waiver to allow deficiency of 2 large frontage trees on Cabot Drive, due to lack of space between the sidewalk and the curb, which is hereby granted;**
- d. The findings of compliance with Ordinance standards in the staff and consultant review letters and the conditions and the items listed in those letters being addressed on the Final Site Plan.**

Member Maday said I wanted to know if it was possible to take into consideration our resident's comment about the one-way parking lot.

Jason Sutton, Civil Engineer of PEA, said we looked at several parking layouts on this site. It is a very unusually shaped parcel. It actually has a lot of topography across it as well. Typically angled spaces are more cumbersome to a parking layout. You gain a narrower driveway, but the spaces themselves end up being longer because the vehicle is on an angle and so you don't get as many spaces. We asked for a variance for 1 parking space and we received that.

ROLL CALL VOTE TO APPROVE THE GODDARD SCHOOL PRELIMINARY SITE PLAN MADE BY MEMBER AVDOULOS AND SECONDED BY MEMBER FERRELL.

In the matter of The Goddard School, JSP19-33, motion to approve the Preliminary Site Plan based on and subject to the following:

- a. On October 8, 2019, the Zoning Board of Appeals granted variances as follows:**
 - 1. Proposed 19,033 square feet of outdoor recreation area, 23,400 square feet required;**
 - 2. Proposed recreation area occupying 63.86% of the exterior side yard, up to 25% of the distance between the building and the property line allowed;**
 - 3. Absence of a loading zone;**
 - 4. Deficiency of parking spaces, 40 spaces proposed, 41 required;**

5. Proposed dumpster located in the front yard, dumpsters are required to be located in the rear yard Landscape waiver of the greenbelt berms along Grand River and Twelve Mile Road, as acceptable hedge substitutes are proposed, which is hereby granted;
 6. Location of play/shade structures in the exterior and interior side yards, and less than 10 feet from the main building, by code accessory structures must be located in the rear yard and must be located greater than 10 feet from the main building;
 7. Location of a fence in the exterior side yard, by code no fence shall extend into a front or exterior side yard.
- b. Landscape waiver for location of building foundation landscaping away from building, due to a conflict with recreation areas and total required area is exceeded, which is hereby granted;
 - c. Landscape waiver to allow deficiency of 2 large frontage trees on Cabot Drive, due to lack of space between the sidewalk and the curb, which is hereby granted;
 - d. The findings of compliance with Ordinance standards in the staff and consultant review letters and the conditions and the items listed in those letters being addressed on the Final Site Plan.

Motion Carried 6-0.

Motion made by Member Avdoulos and seconded by Member Gronachan.

ROLL CALL VOTE TO APPROVE THE GODDARD SCHOOL STORM WATER MANAGEMENT PLAN MADE BY MEMBER AVDOULOS AND SECONDED BY MEMBER GRONACHAN.

In the matter of The Goddard School, JSP19-33, motion to approve the Storm water Management Plan, subject to the findings of compliance with Ordinance standards in the staff and consultant review letters and the conditions and items listed in those letters being addressed on the Final Site Plan. This motion is made because the plan is otherwise in compliance with Chapter 11 of the Code of Ordinances and all other applicable provisions of the Ordinance. *Motion carried 6-0.*

CONSENT AGENDA REMOVALS FOR COMMISSION ACTION

SUPPLEMENTAL ISSUES

There were no supplemental issues.

AUDIENCE PARTICIPATION

There was no audience participation.

ADJOURNMENT

Moved by Member Lynch.

VOICE VOTE ON THE MOTION TO ADJOURN MADE BY MEMBER LYNCH.

Motion to adjourn the October 16, 2019 Planning Commission meeting. Motion carried 6-0.

The meeting was adjourned at 7:13 PM.