



## HARMAN BECKER AUTOMOTIVE JSP14-37

### Harman Becker Automotive, JSP14-37

Consideration of the request of Northern Equities Group for approval of Preliminary Site Plan, Woodland Permit, Wetland Permit and Stormwater Management Plan. The subject property is 16.5 acres in Section 1 of the City of Novi and located north of Thirteen Mile Road, between M-5 and Cabot Drive, in the OST, Planned Office, Service, Technology district. The applicant is proposing an 188,042 square foot facility for Harman Becker Automotive offices.

### Required Action

Approval/Denial of the Preliminary Site Plan, Woodland Permit, Wetland Permit and Stormwater Management Plan.

REVIEW	RESULT	DATE	COMMENTS
Planning	Approval recommended	07-28-14	-The construction of Phase II driveway and pathway relocation is required; the timing for the construction shall be at the sole discretion of the Director of Community Development; and -Applicant shall provide phasing plan delineation on the Final Site Plan submittal
Engineering	Approval recommended	08-07-14	-Applicant shall provide an 8 foot wide path along the extent of the east side of the proposed parking lot, consistent with the City Council's motion of October 8, 2007; and -Applicant to resolve retaining wall conflict with existing sanitary sewer for Phase II driveway construction to Thirteen Mile Road; -Applicant shall incorporate on the Final Site Plan a sidewalk stub along Thirteen Mile Road to connect the north/south path to future sidewalk to the west
Traffic	Approval recommended	08-07-14	Items to be addressed on the Final Site Plan submittal
Landscaping	Approval recommended	07-29-14	<b>Planning Commission waiver for excess contiguous parking spaces required (Not supported by staff) or modification needed on the Final Site Plan (Staff recommendation)</b>
Woodlands	Approval recommended	07-23-14	Items to be addressed on the Final Site Plan submittal
Facade	Approval Recommended	07-22-14	<b>Section 9 waiver required for underage of Brick and overage of Flat Metal Panels (Staff supports)</b>
Fire	Approval recommended	07-23-14	Items to be addressed on the Final Site Plan submittal

## **Motion Sheet**

### **Approval –Preliminary Site Plan**

In the matter of Harman Becker Automotive, JSP14-37, motion to **approve** the Preliminary Site Plan based on and subject to the following:

- a) As part of Phase I of the development, the applicant shall provide an additional path on the Final Site Plan connecting the proposed path along the east side of the parking lot, and continuing along the east side of the parking lot to connect to the proposed path extending south to Thirteen Mile Road, as recommended in the Engineering Review memo dated August 7, 2014 (see attached Exhibit A with possible location), further provided that the width of the entire length of the path be increased to 8 feet, public easement being provided, and wayfinding signs being shown, as this is consistent with the intent for, and location of, the "nature path" as approved at the October 8, 2007 City Council meeting; and
- b) The applicant shall provide with the Final Site Plan submittal for Phase I a phasing plan showing the construction of an access drive to Thirteen Mile along with all associated improvements (including maintaining or relocating proposed pathway to be built as part of Phase I) as a conceptual Phase II, and future parking as a conceptual Phase III. The construction of the Phase II access drive, pathway, and landscape improvements shall be required. However, in order to accommodate the applicant's desire to begin construction on Phase I as soon as practicable, the timing of the construction of the access drive and related improvements shall be deferred until after occupancy of the Phase I improvements has been granted. The determination as to when such construction of the Phase II improvements shall occur will be at the sole discretion of the Director of the Department of Community Development. Upon notification by the Director that the construction of the Phase II improvements is required, the applicant shall submit the required site plan and other information to the City within 60 days and diligently pursue approval for the improvements. Failure of the applicant to comply with this requirement shall be a violation of the site plan approval and, in addition to any other remedies available to the City to correct such violation, the applicant agrees that no further approvals with respect to the site shall be granted and occupancy of the Phase I improvements shall cease. The applicant may be required by the City to sign an appropriate acknowledgement to such effect prior to Final Site Plan approval for Phase I.
- c) In order to eliminate conflict with retaining wall for the Phase II access drive, the applicant shall relocate the access drive and retaining walls, or relocate the sanitary sewer line and public easement; alternatively, the applicant may choose to seek a License Agreement from the City Council to allow for the construction of the retaining wall within the sanitary sewer easement as shown on the plans; and
- d) The applicant shall incorporate on the Final Site Plan for Phase I a sidewalk stub along Thirteen Mile Road to connect the north/south path to a future sidewalk to the west; and
- e) Final Site Plan approval for Phase II, when completed, shall require the submittal of a Stormwater Management Plan for Phase II access drive connection to Thirteen Mile Road, possible wetland permit approval for the stormwater outfall, and resolution of sanitary sewer easement conflicts, which will come back to Planning Commission for approval once more detail is provided; and
- f) Applicant to modify the Final Site Plan for Phase I to insure that no more than 15 contiguous parking spaces are provided without a landscaped island; and
- g) Section 9 Waiver for underage of brick and overage of metal panels on the south and north facades, which is hereby granted; and

- h) The findings of compliance with Ordinance standards in the staff and consultant review letters and the conditions and the items listed in those letters being addressed on the Final Site Plan; and
- i) *(additional conditions here if any)*

*This motion is made because the plan is otherwise in compliance with Article 23A, Article 24 and Article 25 of the Zoning Ordinance and all other applicable provisions of the Ordinance.*

**-AND-**

**Approval –Woodlands Permit Phase I**

In the matter of Harman Becker Automotive, JSP14-37, motion to **approve** the Woodland Permit for Phase I based on and subject to the following:

- a) The findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan for Phase I; and
- b) *(additional conditions here if any)*

*This motion is made because the plan is otherwise in compliance with Chapter 37 of the Code of Ordinances and all other applicable provisions of the Ordinance.*

**-AND-**

**Postpone –Wetlands Permit**

In the matter Harman Becker Automotive, JSP14-37, motion to **delay** consideration of the Wetlands Permit until consideration of the Stormwater Management plan for Phase II driveway construction.

**-AND-**

**Approval –Phase I Stormwater Management**

In the matter of Harman Becker Automotive, JSP14-37, motion to **approve** the Phase I Stormwater Management Plan based on and subject to the following:

- a) Stormwater Management Plan for Phase II access drive will come back to Planning Commission for approval once more detail is provided.
- b) The findings of compliance with Ordinance standards in the staff and consultant review letters, and the conditions and items listed in those letters being addressed on the Final Site Plan; and
- c) *(additional conditions here if any)*

*This motion is made because the plan is otherwise in compliance with Chapter 11 of the Code of Ordinances and all other applicable provisions of the Ordinance.*

**-OR-**

**Denial –Preliminary Site Plan**

In the matter of Harman Becker Automotive, JSP14-37, motion to **deny** the Preliminary Site Plan with Site Condominium, for the following reasons... (because the plan is not in compliance with Article 23A, Article 24 and Article 25 of the Zoning Ordinance and all other applicable provisions of the Ordinance.)

**-AND-**

**Denial –Woodlands Permit**

In the matter of Harman Becker Automotive, JSP14-37, motion to **deny** the Woodlands Permit, for the following reasons... (because the plan is not in compliance with Chapter 37 of the Code of Ordinances and all other applicable provisions of the Ordinance.)

**-AND-**

**Denial –Wetlands Permit**

In the matter of Harman Becker Automotive, JSP14-37, motion to **deny** the Wetlands Permit, for the following reasons...(because the plan is not in compliance with Chapter 12, Article V of the Code of Ordinances and all other applicable provisions of the Ordinance.)

**-AND-**

**Denial –Stormwater Management Plan**

In the matter of Harman Becker Automotive, JSP14-37, motion to **deny** the Stormwater Management Plan, for the following reasons... (because the plan is not in compliance with Chapter 11 of the Code of Ordinances and all other applicable provisions of the Ordinance.)

**-OR-**

**Postpone –Preliminary Site Plan, Woodland Permit, Wetlands Permit, and Stormwater Management Plan**

In the matter of Harman Becker Automotive, JSP14-37, motion to **postpone** the Preliminary Site Plan, Woodland Permit, Wetlands Permit, and Stormwater Management Plan, for the following reasons... (further information is required regarding the project phasing, Thirteen Mile Road pedestrian connections, and traffic issues.)



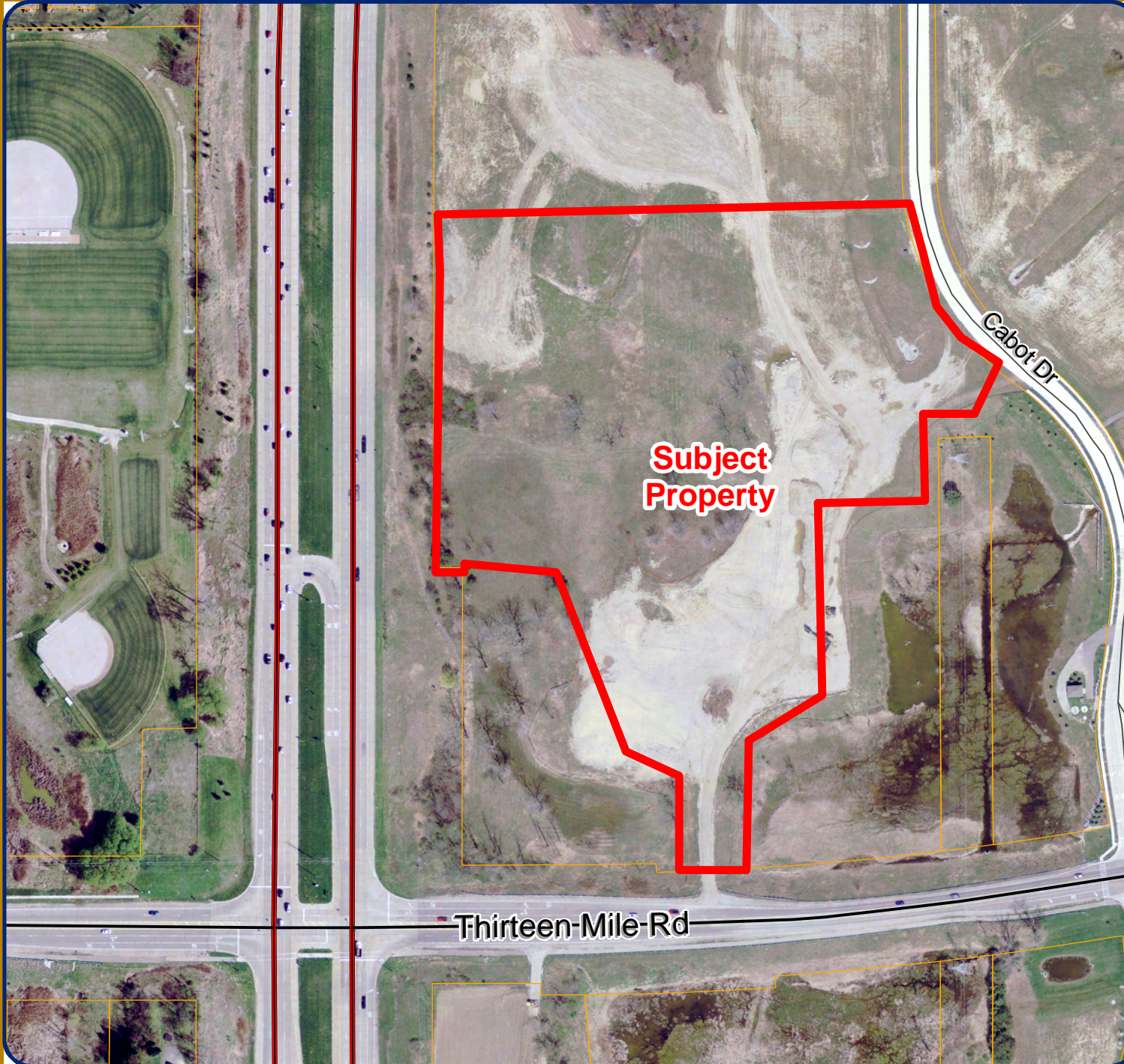
EXHIBIT "A"



MAPS  
Location  
Zoning  
Future Land Use  
Natural Features

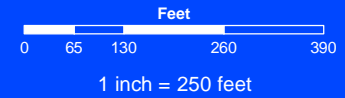


# JSP14-37 Harman Becker Automotive Location



## Map Legend

 Subject Property



**City of Novi**  
Planning Division  
Community Development Dept.  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

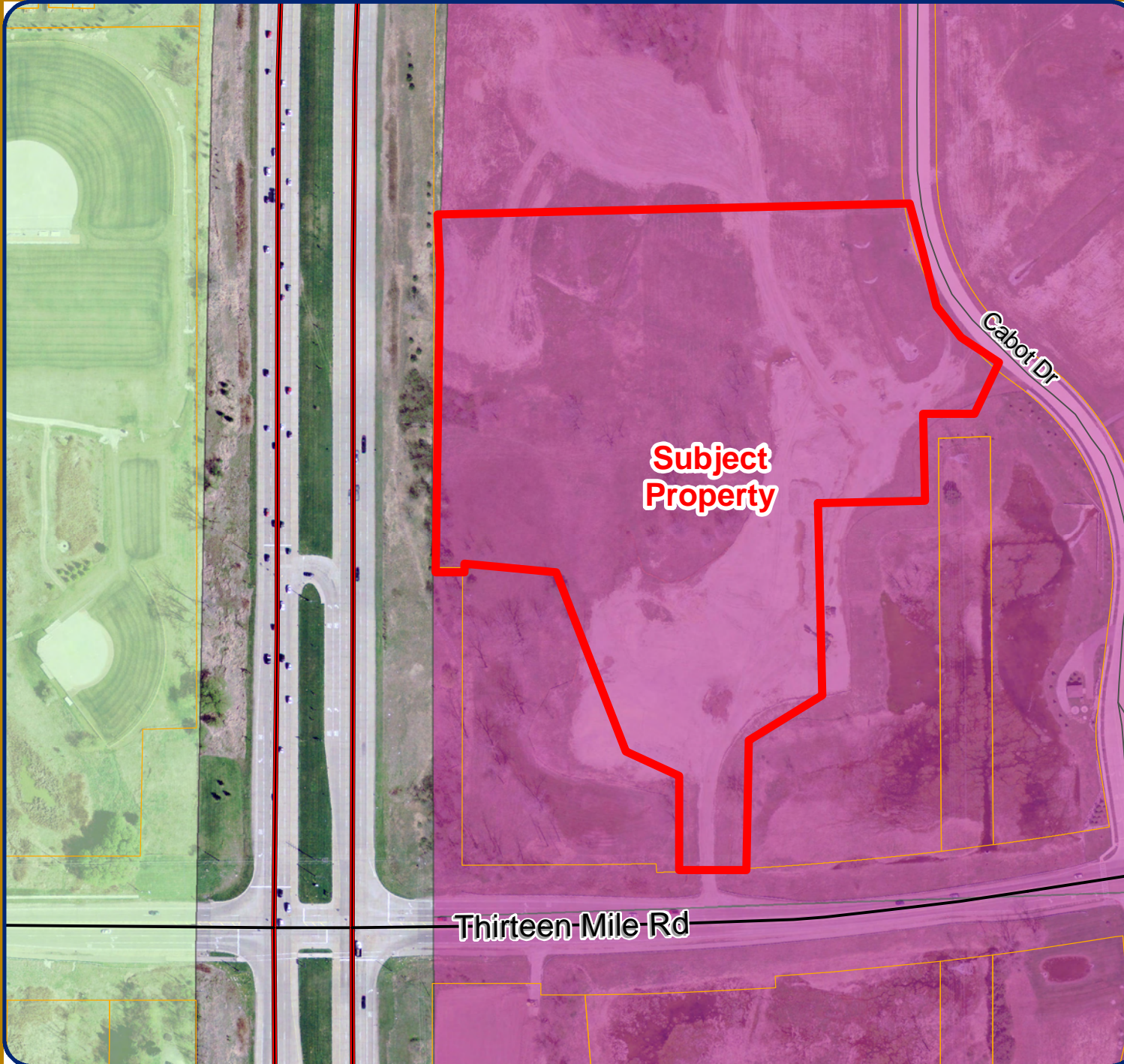
Map Author: Sara White  
Date: 08-05-14

### MAP INTERPRETATION NOTICE





Map information depicted is not intended to replace or substitute for any official or primary source. This map was intended to meet National Map Accuracy Standards and use the most recent, accurate sources available to the people of the City of Novi. Boundary measurements and area calculations are approximate and should not be construed as survey measurements performed by a licensed Michigan Surveyor as defined in Michigan Public Act 132 of 1970 as amended. Please contact the City GIS Manager to confirm source and accuracy information related to this map.



# JSP14-37 Harman Becker Automotive Zoning



## Map Legend

-  Subject Property
-  R-A: Residential Acreage
-  RM-1: Low-Density Multiple Family
-  OST: Office Service Technology

Feet  
0 65 130 260 390  
1 inch = 250 feet



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Planning Division  
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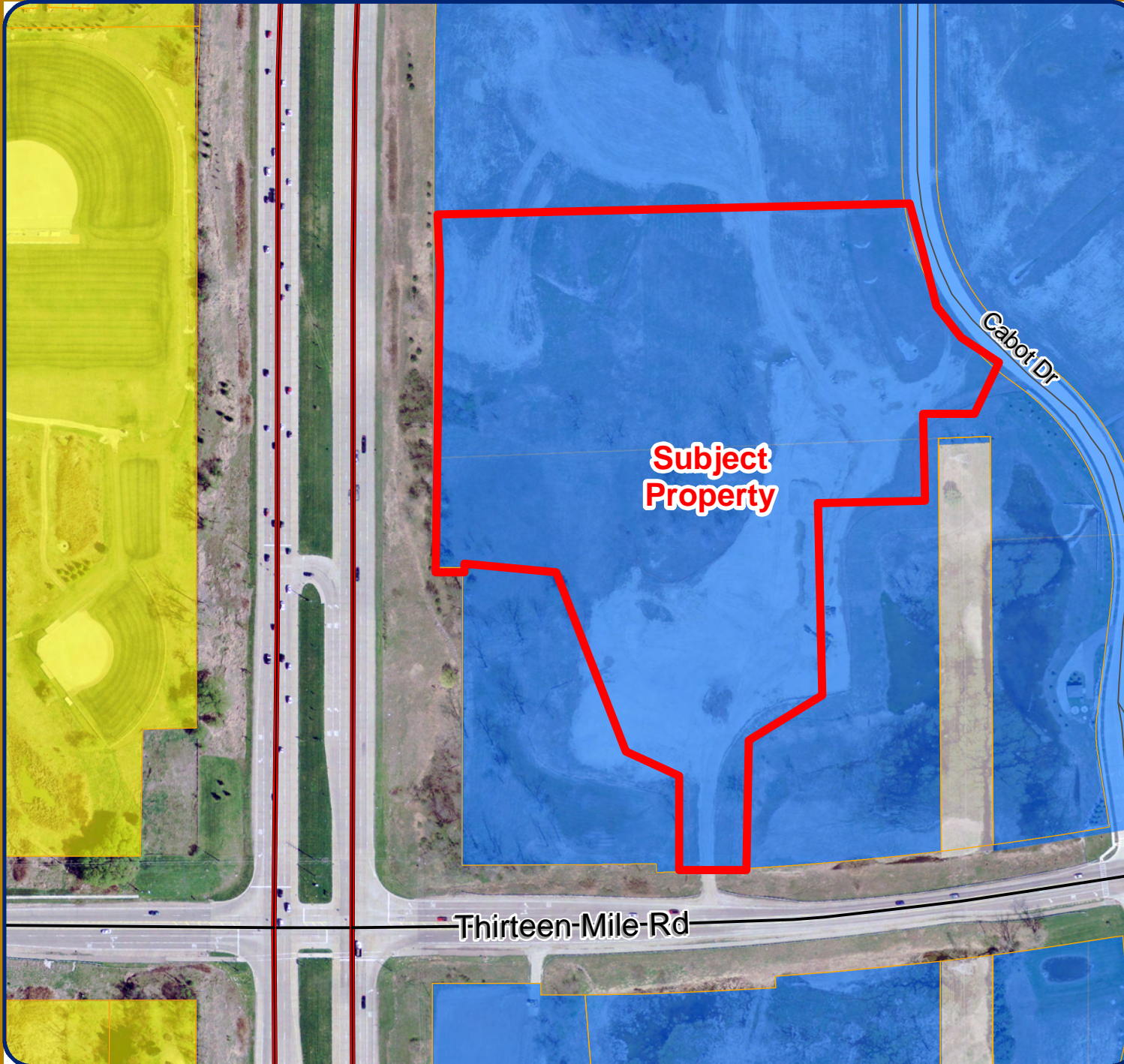
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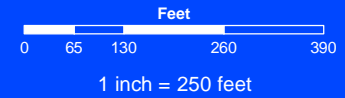


# JSP14-37 Harman Becker Automotive Zoning



## Map Legend

-  Subject Property
-  Single Family
-  Multiple Family
-  Office R&D Technology
-  Utility



**City of Novi**  
Planning Division  
Community Development Dept.  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

Map Author: Sara White  
Date: 08-05-14

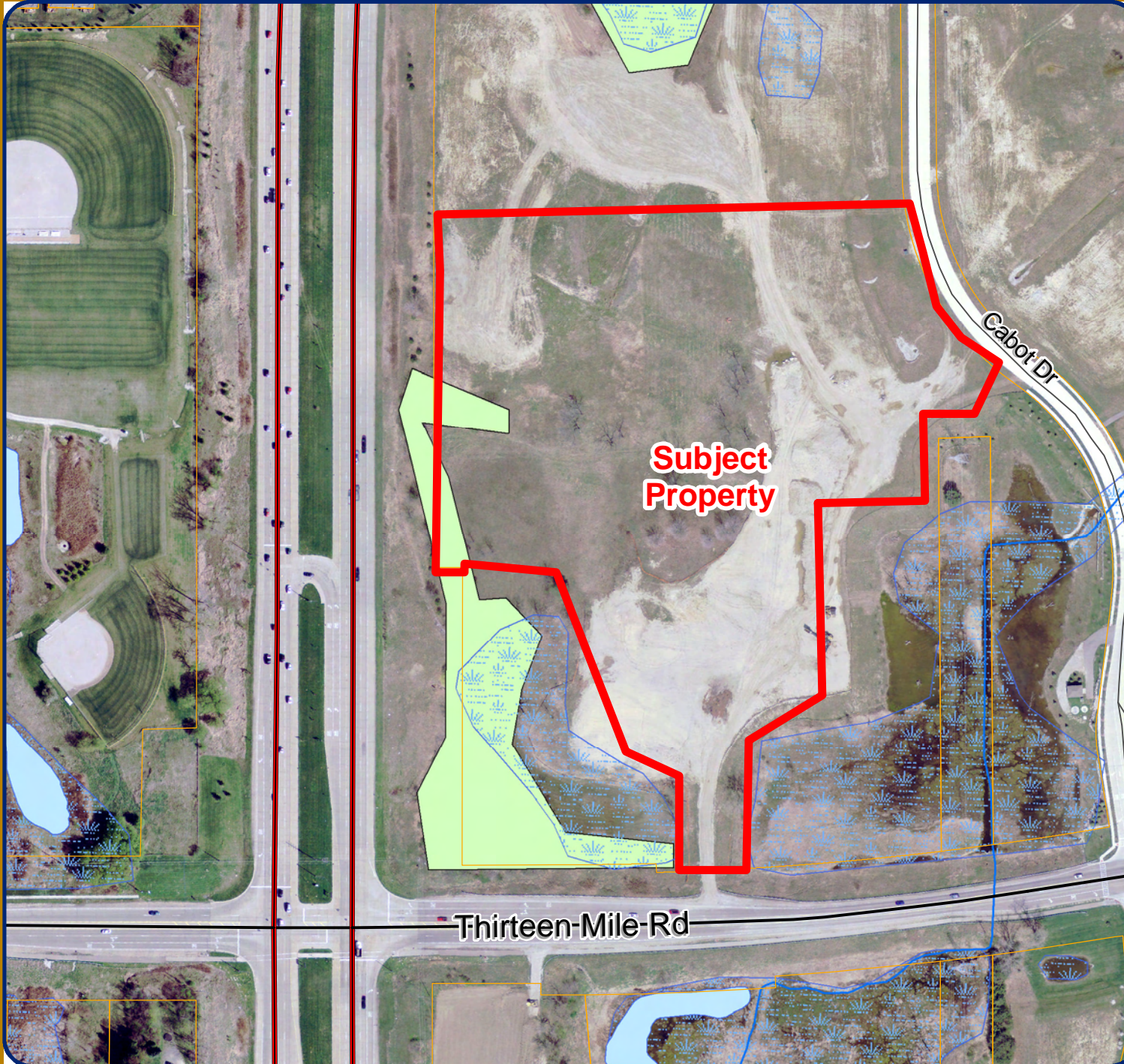
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


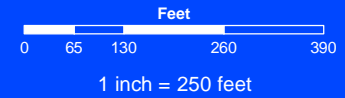
# JSP14-37 Harman Becker Automotive

## Natural Features



### Map Legend

-  Subject Property
-  Waterways
-  Pond
-  Wetlands
-  Woodlands



**City of Novi**  
Planning Division  
Community Development Dept.  
45175 W Ten Mile Rd  
Novi, MI 48375  
cityofnovi.org

Map Author: Sara White  
Date: 08-05-14

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**SITE PLAN**

**(Full plan set available for viewing at the Community Development Department.)**





## PLANNING REVIEW



# PLAN REVIEW CENTER REPORT

June 26, 2014

## Planning Review

Harman Becker Automotive

**JSP14-37**

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### Petitioner

Northern Equities Group

### Review Type

Preliminary Site Plan

### Property Characteristics

- Site Location: North of 13 Mile, east of M-5 (Section 1)
- Site Zoning: OST, Planned Office Service Technology
- Adjoining Zoning: North, South and East: OST, West: Highway, Further West: RA
- Current Site Use: Vacant
- Adjoining Uses: North, South and East: Vacant; West: Highway
- School District: Walled Lake District
- Site Size: 16.5 acres
- Plan Date: 07-09-2014

### Project Summary

The applicant is proposing to construct a 188,042 square foot, 3 story office building with associated parking and landscaping at the northeast corner of Thirteen Mile and Cabot Drive in the Haggerty Corridor Corporate Park for Harman Becker Automotive. Previous correspondence indicated that Harman Becker intends to consolidate six of their seven facilities in a new, state-of-the-art facility in Novi, which would have capacity for over 900 employees. As the site plan indicates, the development will occupy a prominent and visible location near the corner of Thirteen Mile and M-5, with access off of both Cabot Drive and Thirteen Mile Road.

### Recommendation

**Staff recommends approval of the Preliminary Site Plan.** There are minor planning related items to be addressed on the Final Site Plan submittal. Planning Commission approval of the Preliminary Site Plan is still required.

### Ordinance Requirements

This project was reviewed for conformance with the Zoning Ordinance with respect to Article 23A (Planned Office Service Technology District), Article 24 (Schedule of Regulations), Article 25 (General Provisions) and any other applicable provisions of the Zoning Ordinance. Items in **bold** below must be addressed by the applicant.

1. **Building Height:** Building heights are limited to 46 feet or three stories for most of the OST, Planned Office Service Technology District. However, on any OST zoned property located east of M-5 and north of Thirteen Mile Road, buildings may be constructed up to 115 feet in height, provided additional building setback is provided, and provided certain building design elements are used to minimize impact on surround uses. Such design elements might include: variation in building materials, mitigation of exterior and interior building lighting, and utilization of building relief, such as step backs in higher stories. The plans provide the additional building setback that is required for the building height of 55 feet as shown. Please see the façade review for additional comment on the building elevations.

2. Parking Spaces: Office buildings exceeding 100,000 square feet the parking requirements are one space for every 286 square feet of Gross Leasable Area. The parking requirements would therefore be 657 parking spaces, while a total of 895 spaces are provided. The applicant further shows an additional 103 spaces as future parking, to allow for a potential future demand. **The applicant is asked to provide detailed floor plans with the Final Site Plan in order for a detailed parking calculation to be provided. Further, staff suggests that the future parking be labeled "Phase 2" to better clarify the intent of the future parking area that is shown.**
3. Loading Space: The building's loading dock is located at the north end of the building. The ordinance indicates that loading areas must be fully screened from rights of way and adjacent properties. It appears as though a screening wall may be proposed near the loading dock, but it is not notated nor is a detail provided. **Provide detail of loading space screening wall.**
4. Overhead doors: Eight overhead doors shown on the west side of the building, which are proposed to be screened from view from M-5 by a three-foot tall landscaped berm. **The applicant is asked to provide more detail about the purpose and appearance of these doors with the response letter and on the submitted façade board.**
5. Non-Motorized Facilities: The applicant has provided interior connections to the sidewalk along Cabot drive. **Sidewalk/sidewalk stubs for a 6 foot path are required along 13 Mile frontage in order to connect to future sidewalk that will be required with the development of surrounding parcels. There should be interior site connections to this sidewalk as well.** Failure to provide required sidewalk and connections will require a variance from City Council, which staff will not be able to support.
6. Bicycle Parking: The applicant has provided the required number of bicycle spaces and distributed them among multiple entrances of the building. Further detail of the bike parking areas will need to be reviewed at the time of Final Site Plan.
7. Lighting Plan: **Provide additional detail on fixtures and mounting.** See Lighting Review Chart for more information on requirements.
8. Parcel Split: At this time, no property combination or split has been submitted and the Community Development Department has not received a request for condominium approval that would affect the subject property. The applicant has indicated that they have begun the process of a parcel split with assessing. **The applicant must create this parcel prior to Stamping Set approval. Plans will not be stamped until the parcel is created.**
9. Signage: Exterior Signage is not regulated by the Planning Division or Planning Commission. Please contact Jeannie Niland (248.347.0438) for information regarding sign permits.

### **Site Addressing**

The applicant should contact the Building Division for an address prior to applying for a building permit. Building permit applications cannot be processed without a correct address. The address application can be found on the Internet at [www.cityofnovi.org](http://www.cityofnovi.org) under the forms page of the Community Development Department.

Please contact Jeannie Niland [248.347.0438] in the Community Development Department with any specific questions regarding addressing of sites.

### **Pre-Construction Meeting**

Prior to the start of any work on the site, Pre-Construction (Pre-Con) meetings must be held with the applicant's contractor and the City's consulting engineer. Pre-Con meetings are generally held after Stamping Sets have been issued and prior to the start of any work on the site. There are a variety of requirements, fees and permits that must be issued before a Pre-Con can be scheduled. If you have questions regarding the checklist or the Pre-Con itself, please contact Sarah Marchioni [248.347.0430 or [smarchioni@cityofnovi.org](mailto:smarchioni@cityofnovi.org)] in the Community Development Department.

**Planning Review**

Harman Becker  
JSP14-37

July 28, 2014

Page 3 of 3

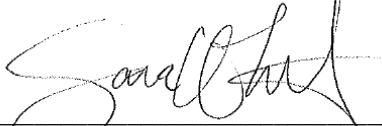
**Chapter 26.5**

Chapter 26.5 of the City of Novi Code of Ordinances generally requires all projects be completed within two years of the issuance of any starting permit. Please contact Sarah Marchioni at 248-347-0430 for additional information on starting permits. The applicant should review and be aware of the requirements of Chapter 26.5 before starting construction.

**Response Letter**

A letter from either the applicant or the applicant's representative addressing comments in this and other review letters is required prior to consideration by the Planning Commission and with the next plan submittal.

If the applicant has any questions concerning the above review or the process in general, do not hesitate to contact me at 248.347.0484 or [swhite@cityofnovi.org](mailto:swhite@cityofnovi.org).



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Sara White, Planner, 248.347.0484 or [swhite@cityofnovi.org](mailto:swhite@cityofnovi.org)

**REVISED ENGINEERING MEMO**

# MEMORANDUM



**TO:** BARBARA MCBETH; COMMUNITY DEVELOPMENT  
**FROM:** BRIAN COBURN, P.E.; ENGINEERING SENIOR MANAGER *BTC*  
**SUBJECT:** JSP14-0037 HARMAN BECKER PRELIMINARY SITE PLAN UPDATES  
**DATE:** AUGUST 7, 2014

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This memo is to provide an updated review of the preliminary site plan for Harman Becker. Engineering issued a preliminary site plan review letter on July 30, 2014 that reviewed the original plan that was submitted for this site and did not recommend approval of the site plan. There were five comments in the letter that identify the reason for staff's recommendation for denial. Over the past week, the applicant has provided additional information to address those comments. We are issuing this memo to update our recommendation as detailed below.

### **Comment 1—13 Mile driveway approach geometrics**

The applicant has revised the plan to show that a one-way entrance to the site from 13 Mile Road will be provided as a future phase of the project. The revised plan P-2.0E (attached) shows the revised driveway configuration and includes the required tapers from Table IX.10 of the Design and Construction Standards. **The updated plan shows that the applicant has complied with this requirement, subject to final review by the traffic consultant.** Staff continues to support the eventual construction of the 13 Mile access to alleviate the congestion that is anticipated at the proposed 13 Mile and Cabot signal at the time of full build-out. It is our understanding that the motion language will address the eventual construction of the 13 Mile driveway.

### **Comment 2—Pedestrian Safety Path on 13 Mile**

The applicant received a variance from City Council on October 8, 2007 to not construct 550 feet of pathway along 13 Mile Road between Cabot Drive to just east of the proposed Harman Becker frontage on 13 Mile Road. The remaining frontage along 13 Mile from Harman Becker to M-5 was not part of the variance that was granted and must be constructed. Additionally, the City Council motion required construction of an 8-foot wide, asphalt pathway for use by the public between Cabot Drive and 13 Mile. The attached figure was included in the City Council motion packet and demonstrates the alignment that was considered at that time. The revised plan P-2.0D (attached) shows a sidewalk that indirectly connects Cabot with 13 Mile (some segments are 7-foot wide and some are 5-foot wide). We do not believe that this proposed alignment meets the intent of the City Council motion and would not approve this plan as provided. However, **if the applicant provides a stub to the west property line along the 13 Mile frontage (as required by ordinance) AND provides an 8-foot wide pathway along the easterly property line to provide a direct connection between 13 Mile Road and Cabot Drive (as shown on the attached sketch), we would be able to recommend approval.** The applicant should address the timing for construction of the remaining portion of the 13 Mile sidewalk between this site and M-5.

**Comment 3—Storm Water Management Plan**

The applicant has provided revised plan sheet D-1 showing the proposed site plan on the drainage area plan for the entire Haggerty Corridor Corporate Park (attached). **This updated plan shows that the applicant has complied with this requirement.**

**Comment 4—Retaining Wall Conflict with Sanitary Sewer**

The proposed phasing of the site to eliminate the 13 Mile drive approach in the initial phase as shown on revised plan P-2.0D (attached) attempted to resolve this comment, however the plan still shows a retaining wall for the proposed sidewalk within the sanitary sewer easement. The applicant has requested a license agreement to place fill and the retaining wall within the existing easement. Staff would not recommend approval of a license agreement. Instead staff recommends the relocation of the existing sanitary sewer out of the influence of the proposed sidewalk/driveway OR relocation of the sidewalk/driveway. Because the applicant owns the land to the east of the subject parcel, relocation of the sanitary sewer is a feasible option, therefore **this comment can be addressed at the time of Final Site Plan.**

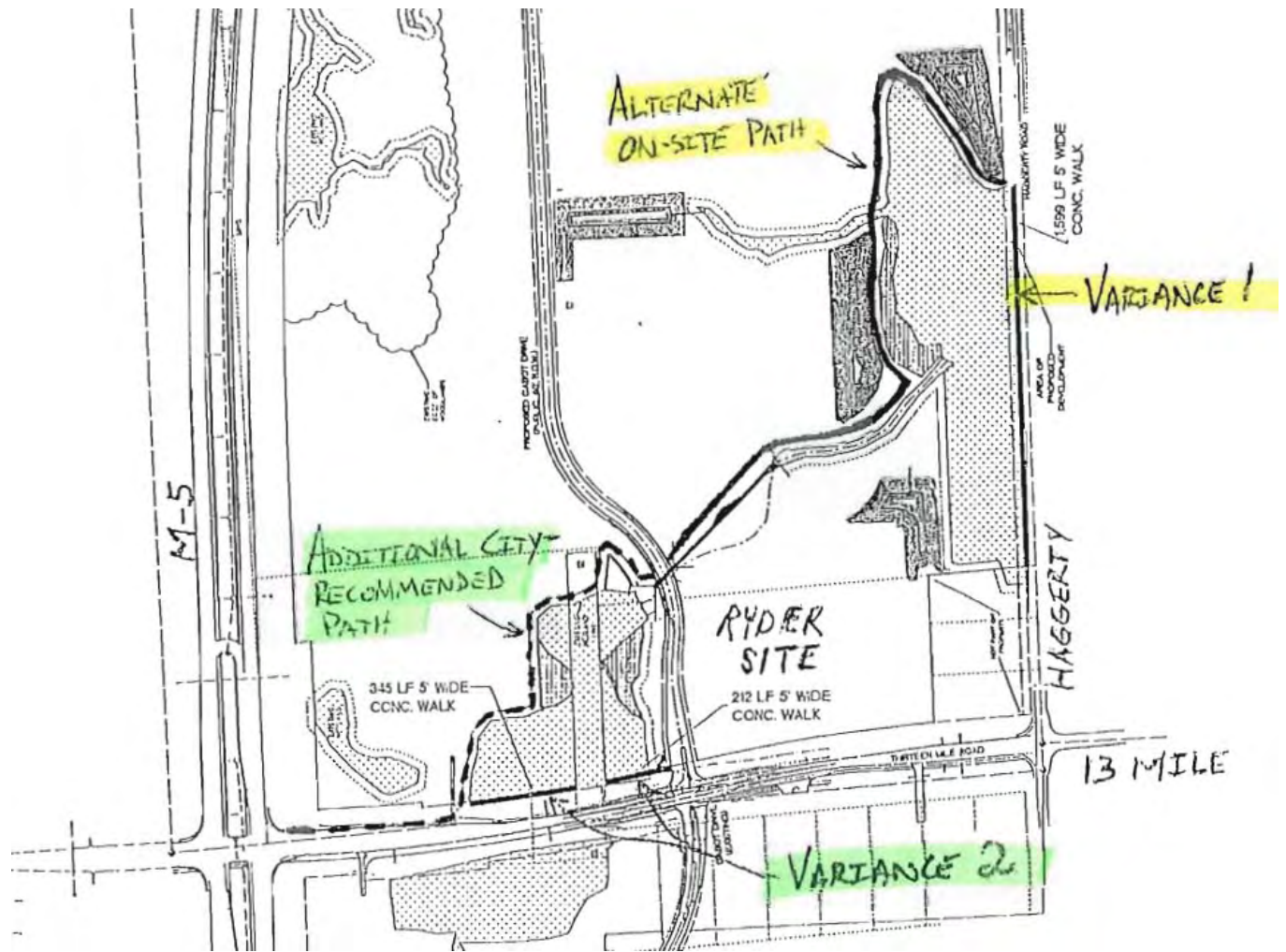
**Comment 5—Wetland Storage**

The proposed phasing of the site to eliminate the 13 Mile drive approach in the initial phase as shown on revised plan P-2.0D (attached) has addressed this comment for the proposed preliminary site plan approval. However, we are unable to recommend approval of the Storm Management Plan for Phase 2 until the use of wetland for storm water has been investigated and recommended for approval. The applicant will need to address this issue prior to final site plan for the driveway and may also need a City wetland permit at that time.

The revised plans have satisfactorily addressed items 1, 3 and 5. Item 4 can be addressed at the time of Final Site Plan. Item 2 can be addressed with motion language and agreement from the applicant to provide the public sidewalk as required by the 2007 City Council motion and as required by the ordinance for the 13 Mile Road frontage. **With these issues resolved, Engineering can recommend approval of the revised preliminary site plan.**

cc: Rob Hayes, Director of Public Services/City Engineer  
Tim Kuhns, Water & Sewer Senior Manager  
Jeremy Miller, Staff Engineer  
Sara White, Planner





Sketch included in motion packet for the sidewalk variance



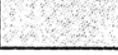


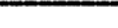









**LEGEND**

-  = EXISTING WETLAND TO REMAIN
-  = EXISTING WETLAND TO BE FILLED
-  = PROPOSED WETLANDS
-  = EXISTING CONTOUR
-  = PROPOSED CONTOUR
-  = EXISTING BUFFER
-  = PROPOSED BUFFER



**A R DECKER & ASSOCIATES, INC.**  
 Consulting Engineers  
 Civil-Structural  
 920 E. Long Lake Rd., Suite 200  
 Troy, Michigan 48065  
 (248) 528-3779 P  
 (248) 528-3548 F

**CLIENT**  
 NORTHERN EQUITIES GROUP  
 39000 COUNTRY CLUB DRIVE  
 FARMINGTON HILLS, MI 48331  
 PH: (248) 848-6400  
 FAX: (248) 848-6700

**PROJECT TITLE**  
 HAGGERTY CORRIDOR  
 CORPORATE PARK PHASE 2  
 SEELEY DRAIN RELOCATION  
 SECTION 1, TOWN 1 NORTH, RANGE 8 EAST  
 CITY OF NOVI, MICHIGAN

**SHEET TITLE**  
 OVERALL PROPOSED PLAN  
 (PROPOSED STORM WATER  
 MANAGEMENT PLAN)

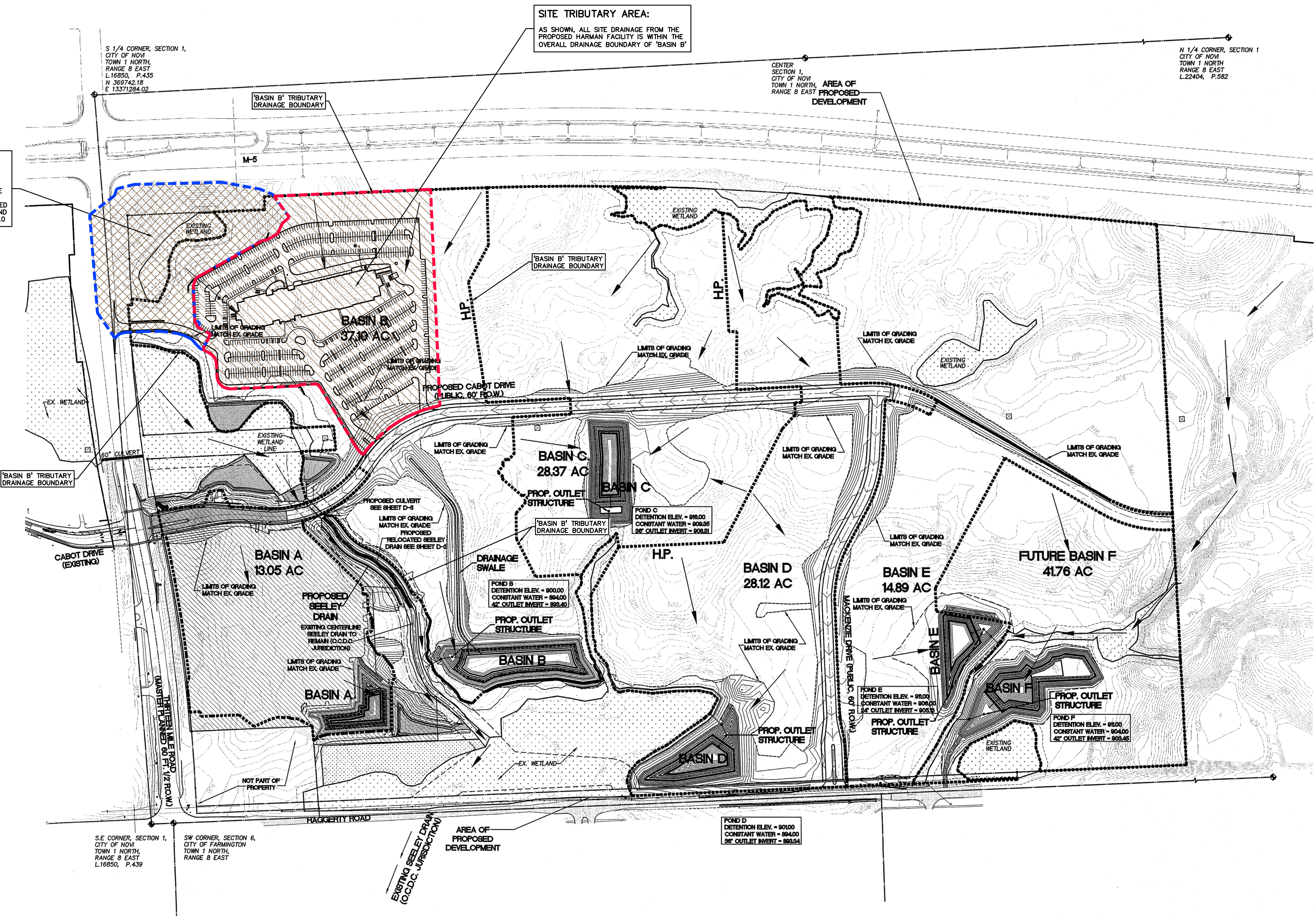
DATE	DRWN/CHK	ISSUED FOR
11/23/06	MAB	PER CLIENT REVIEW
11/29/06	MAB	FOR REG. REVIEW
12/15/06	MAB	FOR REG. REVIEW
1/23/07	MAB	PER ADED REVIEW
3/27/07	MAB	CLIENT REVIEW
4/20/07	MAB	PER OODC REVIEW
5/17/07	MAB	PER COUNTY REVIEW
7/13/07	MAB	PER OODC REVIEW
7/27/07	MAB	PER OODC REVIEW

SEAL

SHEET NO.  
**D-1**  
 PROJECT NO.  
 06-0335

**WETLAND TRIBUTARY AREA:**  
 PORTIONS OF THE SITE BOTH WITHIN THE 'BASIN B' DESIGN TRIBUTARY AREA AND OUTSIDE OF THE 'BASIN B' TRIBUTARY AREA FLOW TO THE EXISTING WETLAND, ALONG WITH A SIZEABLE AREA OF THE 13 MILE ROAD AND M-5 RIGHT-OF-WAYS. THE AREA PROPOSED TO DRAIN TO THE WETLAND HAS BEEN DESIGNED TO MATCH THE EXISTING AREA VERY CLOSELY. THIS EXISTING AND PROPOSED DRAINAGE INFORMATION IS PROVIDED ON SHEET P-4.0

**SITE TRIBUTARY AREA:**  
 AS SHOWN, ALL SITE DRAINAGE FROM THE PROPOSED HARMAN FACILITY IS WITHIN THE OVERALL DRAINAGE BOUNDARY OF 'BASIN B'



S 1/4 CORNER, SECTION 1,  
 CITY OF NOVI,  
 TOWN 1 NORTH,  
 RANGE 8 EAST  
 L.16850, P.435  
 W 369742.18  
 E 13371284.02

N 1/4 CORNER, SECTION 1,  
 CITY OF NOVI,  
 TOWN 1 NORTH,  
 RANGE 8 EAST  
 L.22404, P.582

S.E. CORNER, SECTION 1,  
 CITY OF NOVI,  
 TOWN 1 NORTH,  
 RANGE 8 EAST  
 L.16850, P.439

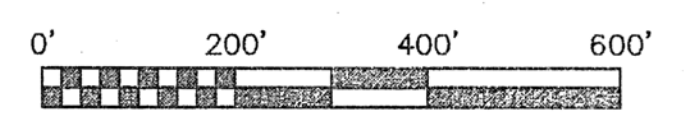
SW CORNER, SECTION 6,  
 CITY OF FARMINGTON  
 TOWN 1 NORTH,  
 RANGE 8 EAST

POND D  
 DETENTION ELEV. = 90100  
 CONSTANT WATER = 90400  
 48" OUTLET INVERT = 90334

POND E  
 DETENTION ELEV. = 91800  
 CONSTANT WATER = 90600  
 48" OUTLET INVERT = 90534

POND C  
 DETENTION ELEV. = 91800  
 CONSTANT WATER = 90635  
 36" OUTLET INVERT = 90534

POND B  
 DETENTION ELEV. = 90400  
 CONSTANT WATER = 90400  
 48" OUTLET INVERT = 90330



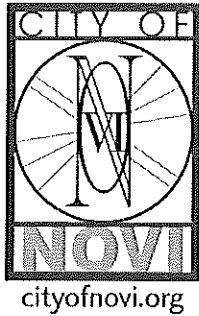
**CAUTION !!**  
 THE LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS SHEET ARE AN INDICATION ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO THE START OF CONSTRUCTION.

3 WORKING DAYS  
**BEFORE YOU DIG  
 CALL MISS DIG**  
 1-800-482-7171  
 (TOLL FREE)

SCALE: 1" = 200'



**ORIGINAL ENGINEERING REVIEW**



## PLAN REVIEW CENTER REPORT

07/30/2014

### Engineering Review

Harman Becker Automotive  
JSP14-0037

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#### Applicant

NORTHERN EQUITIES GROUP

#### Review Type

Preliminary Site Plan

#### Property Characteristics

- Site Location: N. of 13 Mile Road and E. Haggerty Connector (M-5)
- Site Size: 16.50 acres
- Plan Date: July 9, 2014

#### Project Summary

- Construction of an approximately 188,042 square-foot building and associated parking. Site access would be provided from 13 Mile Road and Cabot Drive.
- Water service would be provided by an 8-inch loop from the existing 8-inch water main along the east side of Cabot Drive. A 4-inch domestic lead and a 8-inch fire lead would be provided to serve the building, along with 6 additional hydrants
- Sanitary sewer service would be provided an 8-inch extension from the existing 15-inch sanitary sewer running along north from 13 Mile Road to Cabot Drive. Three 6-inch leads would be provided to serve the building.
- Storm water would be collected by a storm sewer collection system and detained in an off-site basin in the overall commercial park. A smaller portion of the site will be discharged through a storm water treatment chamber into an existing wetland.

#### Recommendation

**Approval of the Preliminary Site Plan and Preliminary Storm Water Management Plan is NOT recommended.**

**Comments:**

The Preliminary Site Plan does not meet the general requirements of Chapter 11, the Storm Water Management Ordinance and the Engineering Design Manual for the following reasons:

1. The review by the traffic consultant indicates that the conditions in Table IX.10 are met to warrant a right turn deceleration taper. Additionally, the corresponding acceleration taper is also required. The current plan must be revised to incorporate the traffic consultant's comments in this regard or a City Council variance of this requirement must be approved prior to Final Site Plan approval. Staff will not support this variance request.
2. A pedestrian safety path is required along the 13 Mile Road frontage of the site. The current plan must be revised to incorporate this requirement or a City Council variance of this requirement must be approved prior to Final Site Plan approval. Staff will not support this variance request, but would support an administrative variance to relocate the pathway within the right-of-way based on the topography that currently exists.
3. The applicant must provide a more detailed storm water management plan that identifies the drainage areas for the site, calculates the flow discharged from this site, confirms that the basin was designed to accept this flow and area, and provides calculations as required by the Engineering Design Manual. Further, the drainage to the wetland located at the southwest corner of this site must be addressed on the Storm Water Management Plan per the Engineering Design Manual.
4. The plan shows a proposed retaining wall crossing the existing sanitary sewer in two locations with approximately 9-17 feet of fill. The applicant must relocate the retaining wall so that it is located outside of the sanitary sewer easement or relocate the sanitary sewer away from the proposed retaining wall.
5. The applicant has proposed the use of off-site wetland storage for a portion of this site, but has not provided information to show that all of the requirements on page 5-5 of the Engineering Design Manual are satisfied to allow the use of the wetland for storm water detention.

**Additional Comments** The following items to be addressed at the time of Final Site Plan submittal:

**General**

6. Provide a utility crossing table indicating that at least 18-inch vertical clearance will be provided, or that additional bedding measures will be utilized at points of conflict where adequate clearance cannot be maintained.
7. Show the locations of all light poles on the utility plan and indicate the typical foundation depth for the pole to verify that no conflicts with utilities will occur. Light poles in a utility easement will require a License Agreement.

8. Label and number all water main, sanitary sewer, and storm sewer structures on Utility Plan.
9. Show the proposed easement boundary for water main and sanitary sewer on the utility plan.
10. Soil borings shall be provided for a preliminary review of the constructability of the proposed development (roads, basin, etc.). Borings identifying soil types, and groundwater elevation should be provided at the time of Preliminary Site plan.
11. The City standard detail sheets are not required for the Final Site Plan submittal. They will be required with the Stamping Set submittal.

#### Water Main

12. Note that a tapping sleeve, valve and well will be provided at the connection to the existing water main.
13. The applicant indicated in the pre-application meeting that the remaining outlots that front on 13 Mile Road will be developed in the future, therefore; a water main stub should be provided to each of the remaining outlots.
14. Provide a profile for all proposed water main 8-inch and larger.
15. A water main stub to the north near the northwest corner of the site should be provided.
16. Three (3) sealed sets of revised utility plans along with the MDEQ permit application (1/07 rev.) for water main construction and the Streamlined Water Main Permit Checklist should be submitted to the Engineering Department for review, assuming no further design changes are anticipated. Utility plan sets shall include only the cover sheet, any applicable utility sheets and the standard detail sheets.

#### Sanitary Sewer

17. Provide a sanitary sewer basis of design for the development on the utility plan sheet.
18. Provide a note on the Utility Plan stating the sanitary lead will be buried at least 5 feet deep where under the influence of pavement.
19. The Oakland County Water Resource Commission IWC form for non-domestic sites must be submitted prior to Final Stamping Set approval.
20. Five (5) sealed sets of revised utility plans along with the MDEQ permit application (11/07 rev.) for sanitary sewer construction and the Streamlined Sanitary Sewer Permit Certification Checklist should be submitted to the Engineering Department for review, assuming no further design changes are anticipated. Utility plan sets shall include only the cover sheet, any applicable utility sheets and the standard detail sheets. Also, the MDEQ can be contacted for an expedited review by their office.

#### Storm Sewer

21. Provide details on the Pre-Treatment structure on the storm sewer across the drive to 13 Mile Road.



22. Provide a 0.1-foot drop in the downstream invert of all storm structures where a change in direction of 30 degrees or greater occurs.
23. Match the 0.80 diameter depth above invert for pipe size increases.
24. Storm manholes with differences in invert elevations exceeding two feet shall contain a 2-foot deep plunge pool.
25. Provide a four-foot deep sump and an oil/gas separator in the last storm structure prior to discharge off- site/to the storm water basin.
26. Provide a schedule listing the casting type and other relevant information for each proposed storm structure on the utility plan. Round castings shall be provided on all catch basins except curb inlet structures.
27. A runoff coefficient of 0.35 shall be used for all turf grass lawns (mowed lawns) and 0.95 shall be used for all paved areas and roof areas. Recalculate the average runoff coefficient based on these 'C' factors.

#### Storm Water Management Plan

28. Provide a drainage area map for the proposed site.
29. Provide bank full volume for the site and show that detention basin 'B' has storage volume for proposed site.
30. Calculations for the runoff and storm water storage into the wetland along 13 Mile Road are required and an easement over the wetland must be provided.
31. Wetland storage is discouraged for the purpose of providing storm water detention. In order to use wetlands for storm water detention, the nine criteria on page 5-5 of the Engineering Design Manual must be met.
32. The Storm Water Management Plan for this development shall be designed in accordance with the Storm Water Ordinance and Chapter 5 of the new Engineering Design Manual.

#### Paving & Grading

33. The proposed driveway access at 13 Mile Road is proposed be only 27 feet wide (back-to-back), less than the City's 30-ft standard but within the acceptable range. This width of driveway will only permit a single exiting lane, which means that longer-delayed exiting left turns will unnecessarily hold up the expected large volume of exiting right turns (per current traffic patterns, the latter could be as high as 173 vehicles in the PM peak hour). Since most of the parking is located south of the proposed Cabot Drive entrance, we believe that there will be more usage of the 13 Mile entrance than what may have been anticipated by the design engineer. We strongly recommend the applicant consider widening this drive access to separate outbound left turns and right turns to improve the operation of the 13 Mile driveway.
34. Provide detailed grading to show ADA parking spaces are in compliance with standards.
35. Revise sheet P-2.0 to better reflect that curb and gutter are provided along the driveway from 13 Mile Road as shown on sheet P-3.3. The curb ends should be extended per Figure IX.11 in the ordinance.

36. Provide a sheet with more detail regarding the work in the 13 Mile Road right-of-way including any pavement work and pavement striping.

Flood Plain

37. Application for a City floodplain permit shall be submitted as soon as possible to begin the review process. The City's floodplain consultant will review the submittal and provide initial comments regarding the review process.

Off-Site Easements

38. Any off-site utility easements anticipated must be executed by both parties **prior to final approval of the plans**. Drafts of the easement shall be submitted at the time of the Preliminary Site Plan submittal for review, and shall be approved by the City prior to final signatures.
39. Approval from the neighboring property owner for the work associated with the off-site water main shall be forwarded to the Engineering Division **prior to site plan approval**.

**The following must be submitted at the time of Final Site Plan submittal:**

40. A letter from either the applicant or the applicant's engineer must be submitted with the Final Site Plan highlighting the changes made to the plans addressing each of the comments listed above and indicating the revised sheets involved.
41. An itemized construction cost estimate must be submitted to the Community Development Department at the time of Final Site Plan submittal for the determination of plan review and construction inspection fees. This estimate should only include the civil site work and not any costs associated with construction of the building or any demolition work. **The cost estimate must be itemized** for each utility (water, sanitary, storm sewer), on-site paving, right-of-way paving (including proposed right-of-way), grading, and the storm water basin (basin construction, control structure, pretreatment structure and restoration).
42. A copy of any off-site easements.

**The following must be submitted at the time of Stamping Set submittal:**

43. A draft copy of the maintenance agreement for the storm water facilities, as outlined in the Storm Water Management Ordinance, must be submitted to the Community Development Department with the Final Site Plan. Once the form of the agreement is approved, this agreement must be approved by City Council and shall be recorded in the office of the Oakland County Register of Deeds.
44. A draft copy of the 20-foot wide easement for the water main to be constructed on the site must be submitted to the Community Development Department.
45. A draft copy of the 20-foot wide easement for the sanitary sewer to be constructed on the site must be submitted to the Community Development Department.

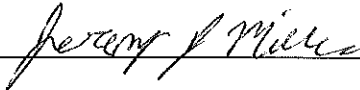
46. Executed copies of any required off-site utility easements must be submitted to the Community Development Department.

**The following must be addressed prior to construction:**

47. A pre-construction meeting shall be required prior to any site work being started. Please contact Sarah Marchioni in the Community Development Department to setup a meeting (248-347-0430).
48. A City of Novi Grading Permit will be required prior to any grading on the site. This permit will be issued at the pre-construction meeting. Once determined, a grading permit fee must be paid to the City Treasurer's Office.
49. An NPDES permit must be obtained from the MDEQ because the site is over 5 acres in size. The MDEQ requires an approved plan to be submitted with the Notice of Coverage.
50. A Soil Erosion Control Permit must be obtained from the City of Novi. Contact Sarah Marchioni in the Community Development Department (248-347-0430) for forms and information.
51. A permit for work within the right-of-way of 13 Mile Road and Cabot Drive must be obtained from the City of Novi. The application is available from the City Engineering Department and should be filed at the time of Final Site Plan submittal. Please contact the Engineering Department at 248-347-0454 for further information.
52. A permit for water main construction must be obtained from the MDEQ. This permit application must be submitted through the City Engineer after the water main plans have been approved.
53. A permit for sanitary sewer construction must be obtained from the MDEQ. This permit application must be submitted through the City Engineer after the sanitary sewer plans have been approved.
54. Construction Inspection Fees to be determined once the construction cost estimate is submitted must be paid prior to the pre-construction meeting.
55. A storm water performance guarantee, equal to 1.5 times the amount required to complete storm water management and facilities as specified in the Storm Water Management Ordinance, must be posted at the Treasurer's Office.
56. An incomplete site work performance guarantee for this development will be calculated (equal to 1.5 times the amount required to complete the site improvements, excluding the storm water facilities) as specified in the Performance Guarantee Ordinance. This guarantee will be posted prior to TCO, at which time it may be reduced based on percentage of construction completed.
57. A street sign financial guarantee in an amount to be determined (\$400 per traffic control sign proposed) must be posted at the Treasurer's Office.

58. Permits for the construction of each retaining wall must be obtained from the Community Development Department (248-347-0415).

Please contact Jeremy Miller at (248) 735-5694 with any questions.

  
\_\_\_\_\_

cc: Ben Croy, Engineering  
Brian Coburn, Engineering  
Sara White, Community Development Department  
Michael Andrews, Water & Sewer Dept.

**TRAFFIC REVIEW**

August 7, 2014

Barbara McBeth, AICP  
Deputy Director of Community Development  
City of Novi  
45175 W. Ten Mile Rd.  
Novi, MI 48375

**SUBJECT: Harman Becker Automotive, JSP14-0037, Revised Traffic Review of Preliminary Site Plan, PSP14-0123**

Dear Ms. McBeth:

At your request, we have reviewed the above and offer the following recommendation and supporting comments.

**Recommendation**

Based on the applicant's response letter of 8-05-14 and the informally revised site plan provided us on 8-06-14, we can now recommend approval, subject to the issues shown below in **bold** being satisfactorily addressed by the final site plan.

**Site Description**

What is the applicant proposing, and what are the surrounding land uses and road network?

1. The applicant is proposing a three-story, 188,042-s.f. single-tenant office building north of 13 Mile Road and west of Cabot Drive (see first attached aerial photo). Surrounding properties, with the exception of the M-5 right-of-way to the west, is or will be developed in a similar (OST) manner.
2. Thirteen Mile Road is a 45-mph minor arterial under the jurisdiction of the City of Novi.

**Trip Generation**

How much new traffic would be generated?

3. A building of the proposed type and size can be expected to generate about 2,205 one-way vehicle trips per day, 338 in the AM peak hour (301 entering and 37 exiting) and 322 in the PM peak hour (48 entering and 274 exiting). While this trip generation forecast obviously exceeds the City's thresholds for a full traffic impact study, such a study was 15 years ago for the overall Haggerty Corridor Corporate Park of which this site is a part.

**Vehicular Access Locations**

Do the proposed "driveway" locations meet City spacing standards?

4. Yes.

## Vehicular Access Improvements

Will there be any improvements to the abutting road(s) at the proposed access point(s)?

5. Our analysis indicates that entering left turns from eastbound 13 Mile could exceed 200 in the AM peak hour. Given the 2012 westbound opposing volume of approximately 500 that hour (determined in our 13 Mile/Cabot signal warrant study for the City), ITE guidelines indicate that at least 175 ft of eastbound left-turn storage would be required west of the potential future entrance-only access drive on 13 Mile. Per Google Earth, there appears to be over 400 ft of existing two-way left-turn lane available between the crosshatching near M-5 and the point of turn for the potential future driveway (see our second attached aerial photo). Hence, there would be adequate space for both left-turn storage and a gentle (unmarked) entering taper.
6. Based on current traffic patterns on 13 Mile Road in the AM peak hour, 32% of the traffic entering the site that hour can be expected to approach from the east (with 68% from the west and a negligible amount from the north via Cabot). Assuming that about half of that 32% enters via Cabot and half enters via the direct drive on 13 Mile, there would be  $(301 \times 0.32 \times 0.5 =)$  48 entering right turns from 13 Mile. The above-cited signal warrant study found that this section of 13 Mile had an average daily traffic (ADT) volume of 12,245 vehicles in June 2012. Per Design and Construction Standards (DCS) Figure IX.10, for this road ADT and peak-hour entering right-turn volume, a deceleration taper is warranted. A deceleration taper meeting City standards (DCS Figure IX.11) has been shown on the plan revision of 8-06-14.
7. Since the potential site drive on 13 Mile Road would operate one-way northbound only (entering the site), an acceleration taper would not be needed. Per DCS Figure IX.2, **the exiting curb radius should be 5-10 ft, not 35 ft as shown on the plan revision of 8-06-14.**

## Access Drive Design and Control

Are the proposed design, pavement markings, and signage satisfactory?

8. Per the plan revision of 8-06-14, the potential south driveway would be 21 ft wide (back-to-back) at the internal ends of the curb returns, narrowing to 18 ft further north. The predominant 18-ft width would be within the City's permissible width range (of 16-20 ft, per DCS Figure IX.2); however, to better assess the surface width actually available for driving, **the plan should detail a specific curb-and-gutter type for each side of the potential drive.**
9. **The final site plan should show appropriate regulatory signage at each end of the potential 13 Mile Road access drive. At a minimum, this should include back-to-back ONE WAY (R6-1) signs on the east side of the drive near 13 Mile Road; DO NOT ENTER (R5-1) signs on both sides of the drive near its north end; and a supplemental WRONG WAY (R5-1a) sign on the west side of the drive about 100 ft south of the same-side DO NOT ENTER sign.**
10. The design and control of the two access drives on Cabot appear generally satisfactory.

## Pedestrian Access

Are pedestrians safely and reasonably accommodated?

11. We understand that the path to/from 13 Mile Road, which would closely parallel the potential future access drive, will have to be 8 ft wide rather than 5 ft wide as shown on the plan revision of 8-06-14; however, we defer to City Engineering staff on this issue as well as the question of how this path will end when it reaches the 13 Mile right-of-way.
12. Per the *Michigan Manual on Uniform Traffic Control Devices* (MMUTCD), all crosswalks must be white. However, where a crosswalk meets an access aisle crosshatched for an adjacent barrier-free parking space, the hatching must (by standard) be blue. **The final site plan should label the four crosswalks to be marked across the parking lots as white per the above specification. The isolated zebra-bar crosswalk west of the building also needs to be labeled as white.**

### Circulation and Parking

Can vehicles safely and conveniently maneuver through the site?

13. **All back-of-curb radii should be dimensioned to facilitate a proper review of the site plan. Where obviously repetitive radii are used, only one such radius need be dimensioned, as "typical."**
14. Given that only the north access drive would have heavy-duty pavement, **the north drive should be signed as serving the (only permissible) truck route to the loading dock at the building's northwest corner. Also, the truck maneuvering plan now shown on sheet P-5.2 should label the design truck (a WB-67). For the record, we note that proposed truck maneuvering plan will require backing truck drivers to use their passenger-side mirrors, a practice that they normally dislike.** (An alternative would be to extend the heavy-duty pavement further west, thus allowing an approaching truck to proceed past the dock and back into it using driver-side mirrors.)
15. A few other comments regarding traffic control devices are as follows:
  - a. **Every designated barrier-free parking space abutting an 8-ft-wide access aisle qualifies as van-accessible and must be signed according, even if the resulting number of such spaces exceeds the ADA minimum. The Sign Quantities table should therefore list eight R7-8P signs, not four.**
  - b. **The Sign Quantities table should list five stop (R1-1) signs (as proposed elsewhere on the plan), not three.**
  - c. **The number of NO PARKING FIRE LANE signs appears excessive (at 50). We see no need for such signs where there is inadequate curb space for potentially obstructive parallel parking. Examples of signs we would question in this regard are those adjacent to a 10-ft-wide end island with semicircular rounding (six along the east side of the building; six along the west side of the building; and two along the north side of the north parking lot). On the other hand, no such signs are proposed along the east end aisle of the southeast parking lot. The City Fire Marshal should offer an opinion on these matters.**

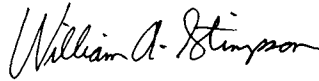


- d. **A diagrammatic Keep Right (R4-7) sign is needed on each end of the boulevard island in the site's south drive on Cabot.**
16. While we realize the layout shown for the land-banked parking would have to be resubmitted for an independent review if and when it is ever proposed to be built, **the potential future end island at the north end of that parking – extending into the site development now proposed – should be revised now to match the City of Novi standard (with a 15-ft main radius, 2-ft minor radius, and 3-ft setback from the aisle).**

Sincerely,  
CLEARZONING, INC.



Rodney L. Arroyo, AICP  
President



William A. Stimpson, P.E.  
Director of Traffic Engineering

Attachments:  
Two aerial photos of site vicinity



**Vicinity Aerial – Proposed Northern Equities Building at 13 Mile and Cabot**





Lane Configuration of 13 Mile Road, from East of Proposed Driveway to M-5

## LANDSCAPE REVIEW



# PLAN REVIEW CENTER REPORT

August 7, 2014

## Preliminary Landscape Plan

Harmon Becker Automotive

JSP 14-37

### Petitioner

Northern Equities Group

### Review Type

Preliminary Site Plan

### Property Characteristics

Site Location:	Mackenzie Drive / Cabot Drive
Site Zoning:	OST – Office Service Technology
Adjoining Zoning:	North, South, West and East: OST.
Site Use(s):	Vacant
Site Size:	4.33 acres
Plan Date:	7/9/2014

### Recommendation

**Approval of the Preliminary Site Plan for Harmon Becker Automotive JSP 14-37 is recommended.**

### Ordinance Considerations

#### **Adjacent to Public Rights-of-Way – Berm (Wall) & Buffer (Sec. 2509.3.b.)**

1. A 20' wide greenbelt is required along both road frontages. The Applicant has met this requirement.
2. One deciduous or large evergreen tree is required per 35 L.F. of road frontage. The Applicant has met this requirement.
3. One sub-canopy tree is required per 20 L.F. of road frontage. The Applicant has met this requirement.
4. Please also note that the Applicant has proposed a landscape berm directly adjacent to the vehicular access doors to the west. Although there is no direct frontage on any roadway and a buffer is not technically required, this will be an effective screen for the series of doors on this side of the building.

#### **Street Tree Requirements (Sec. 2509.3.b.)**

1. A street tree is required per 35 L.F. of road frontage. The Applicant has met this requirement by quantity. Please note that no sidewalk has been proposed along the Thirteen Mile frontage.

#### **Parking Lot Landscape (Sec. 2509.3.c.)**

1. A total of 17,072 SF of interior parking landscape area is required. The Applicant has met this requirement.
2. A total of 228 Parking Lot Canopy Trees are required and have been provided.

3. Snow storage areas have been shown as required.
4. No more than 15 contiguous parking spaces are allowed. Please add landscape islands to meet requirements.

**Parking Lot Perimeter Canopy Trees (Sec. 2509.3.c.(3))**

1. Perimeter Parking Lot Canopy Trees are required per 35 LF surrounding parking and access areas. A total of 78 trees are required. The Applicant has met this requirement.

**Building Foundation Landscape (Sec. 2509.3.d.)**

1. A minimum 4' wide landscape bed is required around the entire building foundation with the exception of access areas. The Applicant has met this requirement.
2. A total of 12,584 SF of building foundation landscape area is required; a total of 44,750 SF has been provided. The Applicant has met the requirement.

**Plant List (LDM)**

1. The Plant List as provided meets the requirements of the Ordinance and the Landscape Design Manual. Standard costs for materials have been provided.

**Planting Notations and Details (LDM)**

1. The Planting Details and Notations as provided meets the requirements of the Ordinance and the Landscape Design Manual.
2. Staking of most plant material is at the option of the Applicant. Plant materials must be staked in areas of steep slopes or in areas of high winds.

**Irrigation (Sec. 2509 3.f.(6)(b))**

1. An Irrigation Plan must be provided upon Stamping Set submittal. An estimated cost has been provided for irrigation. Staff believes true costs will likely be higher and must be reviewed upon submittal of the Stamping Set.

**Storm Basin Landscape (LDM)**

1. Please provide additional information on the proposed retaining walls. Include maximum height.

Please follow guidelines of the Zoning Ordinance, Landscape Design Manual Guidelines and the appropriate items in the applicable zoning classification. This review is a summary and not intended to substitute for any Ordinance.

Reviewed by: David R. Beschke, RLA

## Landscape Review Summary Chart

**Date: August 5, 2014**

Project Name: Harman Becker  
 Project Location: Thirteen Mile / Cabot Drive  
 Sp #: JSP14-37  
 Plan Date: August 5, 2014  
 Review Type: Preliminary Site Plan

Item	Required	Proposed	Meets Requirement	Comments
Name, address and telephone number of the owner and developer or association. (LDM 2.a.)	Yes	Yes	Yes	
Name, Address and telephone number of RLA (LDM 2.b.)	Yes	Yes	Yes	
Legal description or boundary line survey. (LDM 2.c.)	Yes	Yes	Yes	
Project Name and Address (LDM 2.d.)	Yes	Yes	Yes	
A landscape plan <b>1"-20' minimum</b> Proper north (LDM 2.e.)	Yes	Yes	Yes	LA may approve larger scale.
Proposed <b>topography. 2'</b> contour minimum (LDM 2.e.(1))	Yes	Yes	Yes	Show at a minimum <b>2' contour interval</b>
Existing plant material (LDM 2.e.(2))	Yes	Yes	Yes	Show location type and size. Label to be saved or removed. Plan shall state if none exists.
Proposed plant material. (LDM 2.e.(3))	Yes	Yes	Yes	Show location, type and size.
Existing and proposed overhead and underground utilities, including hydrants. (LDM 2.e.(4))	Yes	Yes	Yes	
Clear Vision Zone (LDM 2.3.(5) - 2513)	Yes	Yes	Yes	Measurements are to be taken from R.O.W.

Item	Required	Proposed	Meets Requirement	Comments
Zoning (LDM 2.f.)	OST	OST	Yes	
Sealed by LA. (LDM 2.g.)	Yes	Yes	Yes	Stamping Set must provide an original signature.
Plant List (LDM 2.h.)	Yes	Yes	Yes	Plant schedule that includes key, quantity, botanical name, common name, size, root, comments and cost estimate
Quantities	Yes	Yes	Yes	
Sizes	Yes	Yes	Yes	<b>Canopy trees must be 3" in caliper</b> <b>Sub-Canopy trees must be 2.5" in caliper</b>
Type and amount of mulch	Yes	Yes	Yes	
Turf	Yes	Yes	Yes	Must provide type and quantity of all ground cover.
Acceptable species	Yes	Yes	Yes	There are no prohibited plantings.
Diversity	Yes	Yes	Yes	
Planting Details/Info (LDM 2.i.)	Yes	Yes	Yes	
Deciduous Tree	Yes	Yes	Yes	
Evergreen Tree	Yes	Yes	Yes	
Shrub	Yes	Yes	Yes	
Perennial/ Ground Cover	Yes	Yes	Yes	
Transformers (LDM 1.e.5.)	Yes	Yes	Yes	Show locations and screening.
Berm Plantings (LDM 1)	Yes	Yes	Yes	
Walls (LDM 2.k.)	NA			Show materials, height and type of construction including footings.
Landscape Notes	Yes	Yes	Yes	
Miss Dig Note	Yes	Yes	Yes	
Mulch	Yes	Yes	Yes	Natural color, finely shredded hardwood bark required for all plantings. <b>4" thick bark mulch for trees in 4-foot diameter circle with 3" pulled away from trunk.</b> <b>3" thick bark for shrubs and 2" thick bark for perennials.</b>



Item	Required	Proposed	Meets Requirement	Comments
2 yr. Guarantee	Yes	Yes	Yes	Indicate 2 year guarantee on plant material. Replace failing material within one year, or the next appropriate planting period.
Approval of substitutions.	Yes	Yes	Yes	All substitutions or deviations from the landscape plan must be approved by the city prior to installation.
Tree stakes	Yes	Yes	Yes	Remove after one winter season.
Parking Area Landscape Calculations (LDM 2.0.)	Yes	Yes	Yes	Islands a minimum 300 square feet to qualify.
A. For : OS-1, OS-2, OSC, OST, B-1, B-2, B-3, NCC, EXPO, FS, TC, TC-1, RC, Special Land Use or non-residential use in any R district	Yes	Yes	Yes	A - Total square footage of parking spaces not including access aisles X 10% (parking space square footage x .10)  148,793 x 10% = 14,879 SF
B. For : OS-1, OS-2, OSC, OST, B-1, B-2, B-3, NCC, EXPO, FS, TC, TC-1, RC, Special Land Use or non-residential use in any R district	NA			B - Square footage of all additional paved vehicular use areas under 50,000 sq. ft. x 5% (square footage x .05)  0 x 5% = 0
C. For : OS-1, OS-2, OSC, OST, B-1, B-2, B-3, NCC, EXPO, FS, TC, TC-1, RC, Special Land Use or non-residential use in any R district	Yes	Yes	Yes	C - square footage of all additional paved vehicular use areas over 50,000 sq. ft. x 1% (square footage x .01)  219,293 x .01 = 2,193
A. For: I-1 and I-2 1. Landscape area required due to # of parking spaces	NA			A - Total square footage of parking spaces not including access aisles X 7% (parking space square footage x .07)
B. For: I-1 and I-2	NA			B - Square footage of all additional paved Vehicular use areas under 50,000 sq. ft. x 2%

Item	Required	Proposed	Meets Requirement	Comments
2. Landscape area required due to vehicular use area				(square footage x .02)
C. For: I-1 and I-2 2. Landscape area required due to vehicular use area	NA			C – square footage of all additional paved vehicular use areas over 50,000 sq. ft. x 1% (square footage x .01)
Total A, B and C above = Total interior parking lot landscaping requirement	Yes	Yes	Yes	A+B+C =  14,879 + 2,193 = 17,072 square feet required 21,689 SF provided
Parking lot tree requirement	Yes	Yes	Yes	Total square footage requirement / 75 228 required 228 provided
Perimeter Canopy Tree Plantings	Yes	Yes	Yes	Minimum 1 per 35 linear feet as a minimum.
Parking Lot Plants	Yes	Yes	Yes	Maintain shrubs at <b>max. 24"</b> in height within lot. <b>No plants over 12" within 10 feet of fire hydrant.</b> No evergreen trees in islands.
15 parking space limit	Yes	No	No	Only 15 permitted without island
Parking Land Banked	NA			
Foundation Landscape calculation (LDM.2.p.)	Yes	Yes	Yes	Square footage equal in quantity to the building <b>perimeter x 8'. Minimum 4' required</b>  12,584 required 44,570 provided
Snow Deposit (LDM.2.q.)	Yes	Yes	Yes	Location(s) shown.
Irrigation plan (LDM 2.s.)	Yes	No	No	Provide with final landscape plan.
Cost Estimate (LDM 2.t.)	Yes	Yes	Yes	Provide as a column on plant schedule consistent <b>with the City's current fee calculation chart.</b>
Plant Placement (LDM 3.a.(4))	Yes	Yes	Yes	All plants except creeping vine type plantings, <b>shall not be located within 4' of a property line</b>
Residential adjacent to non-residential	NA			
Berm (2509.3.a.)	NA			

Item	Required	Proposed	Meets Requirement	Comments
Planting (LDM 1.a.)	NA			
Adjacent to Public Rights-of-Way	Yes	Yes	Yes	
Berm (2509.3.b.)	Yes	Yes	Yes	Call out any requested waivers if berm is not provided.
Street trees	Yes	Yes	Yes	
Detention Basin Plantings (LDM 1.d.(3))	NA			70-75% of basin rim planted.
Transformer Screening (LDM 1.d.(3))	Yes	Yes	Yes	Provide 8 to 10 feet of clear space in front of the doors. <b>24" clear on sides.</b>
R.O.W. Trees (2509.3.f - LDM 1.d))	NA			
Single Family	NA			
40 wide non-access greenbelt	NA			
Street Trees	NA			
Islands and boulevards	NA			Irrigated
Multi family	NA			
Condo Trees	NA			3 canopy of deciduous for each first floor unit
Street trees	NA			1 per 35 linear feet
Interior street trees	NA			1 per 35 linear feet Evergreens no closer than 20 feet.
Subcanopy trees	NA			3 per 40 linear feet
Basin plantings	NA			
Loading Zone (2507)	Yes	Yes	Yes	Placed at rear of building / screened.

**NOTES:**

1. Please follow guidelines of the Zoning Ordinance and Landscape Design Guidelines. This table is a summary chart and not intended to substitute for any Ordinance. The appropriate section of the applicable ordinance is indicated in parenthesis.  
For the landscape requirements, see the Zoning Ordinance landscape section on 2509, Landscape Design Manual and the appropriate items in the applicable zoning classification.
2. NA means not applicable.
3. Critical items that need to be addressed are in bold italics.
4. For any further questions, please contact:  
David R. Beschke  
City of Novi Landscape Architect  
45175 W. Ten Mile Road  
Novi, Michigan 48375-3024  
(248) 735-5621  
(248) 735-5600 fax  
City web site [www.cityofnovi.org](http://www.cityofnovi.org)

## FACADE REVIEW



July 22, 2014 (REV)

City of Novi Planning Department  
 45175 W. 10 Mile Rd.  
 Novi, MI 48375-3024

Re: **FACADE ORDINANCE - Facade Review, Preliminary Site Plan**  
**Harman Becker Automotive, PSP14-0123**  
 Façade Region: 1, Zoning District: OST

Dear Ms. McBeth;

The following is the Façade Review for the above referenced project based on the drawings prepared by Faudie Architecture, dated 7/11/14. The percentages of materials proposed for each façade are as shown in the table below. Materials in non-compliance are highlighted in bold. The façade material sample board was not provided at the time of this review.

	East (Front)	South	North	West	Façade Ordinance Section 2520 Maximum (Minimum)
Brick	36%	<b>28%</b>	<b>29%</b>	39%	100% (30% Min)
Spanderal Glass	38%	18%	19%	39%	50%
Flat Metal Panels (Including Roof Screens)	26%	<b>54%</b>	<b>52%</b>	22%	50%

**Ordinance Section 2520** - As shown above a Section 9 Waiver would be required for the underage of Brick and overage of Flat Metal Panels on the south and north facades. A colored rendering of the proposed design was provided by the applicant. The design employs an outward tilting façade element that defines the front entrance on the east façade. We believe this interesting feature will give the building a unique identity within the development. It is noted that the east and west facades are virtually identical, both having main entrances serving a significant number of parking spaces. It is noted that the east entrance (facing Cabot Drive) employs the aforementioned outward tilting elements whereas the same elements on the west façade (facing the M-5 Connector) are vertical. Given the fact that the west façade will be highly visible from the M-5 connector, we believe that equal emphasis of the west entrance would be justified. The applicant may wish to provide a further explanation of this aspect of the design during the Planning Commission meeting.

It is our recommendation that the design is consistent with the intent and purpose of the Façade Ordinance and that a Section 9 Waiver be granted for the proposed minor deviations from the Façade Chart; the underage of Brick and overage of Flat Metal Panels on the south and north facades. This recommendation is contingent upon the applicant providing a sample board indicating carefully coordinated earth tone colors not less than 5 days before the Planning Commission meeting. The sample board should include all materials including paint colors for the overhead doors on the west façade.

**Ordinance Section 2400** – Section 2400 of the Ordinance requires that buildings greater than 46’ in height be designed to minimize their impact on surrounding existing uses, including, but not limited to building design elements such as variation in building materials, mitigation of exterior and interior building lighting, and utilization of building relief (including step backs of higher stories). The design employs a variation in materials and features a strong visual differentiation of the upper story that I believe meets the intent of this section. With respect to “mitigation of exterior and interior lighting”, the applicant should clarify the type and extent of building lighting and confirm that the overall effect of such lighting will be consistent with the adjacent commercial developments.

**Dumpster Enclosure** – Section 2520 requires that dumpster enclosures meet the same standards as the building, in this case matching brick. The detail of the dumpster enclosure on sheet P-5.2 indicates “ARCHITECTURAL MASONRY TO MATCH BUILDING”. The applicant should revise the note to read “brick to match building” to more clearly indicate the design intent.

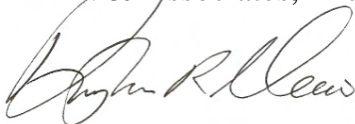
**Notes to the Applicant:**

1. It should be noted that the height of the roof equipment screens must be sufficient to fully conceal all RTU’s from all on-site and off-site vantage points.
2. Façade Ordinance requires inspection(s) for all projects. Materials displayed on the approved sample board will be compared to materials delivered to the site. It is the applicant’s responsibility to request the inspection of each façade material at the appropriate time. Inspections may be requested using the Novi Building Department’s Online Inspection Portal with the following link. Please click on “Click here to Request an Inspection” under “Contractors”, then click “Façade”.

<http://www.cityofnovi.org/Services/CommDev/OnlineInspectionPortal.asp>.

If you have any questions regarding this project please do not hesitate to call.

Sincerely,  
DRN & Associates, Architects PC



Douglas R. Necci, AIA

**FIRE REVIEW**





June 15, 2014

**July 23, 2014**

TO: Barbara McBeth- Deputy Director of Community Development  
Sara White- Plan Review Center  
Sara Roediger- Plan Review Center

**CITY COUNCIL**

**Mayor**  
Bob Gatt

**Mayor Pro Tem**  
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David E. Molloy

**Director of EMS/Fire Operations**  
Jeffery R. Johnson

**Assistant Chief of Police**  
Victor C.M. Lauria

**Assistant Chief of Police**  
Jerrod S. Hart

RE: Harman Becker

PSP#14-0105

**PSP#14-0123**

**Project Description:** A Three Story 189,200sq. ft. building located on the Northeast corner of Thirteen Mile and M-5.

**Comments:**

- 1) Proximity to hydrant: In any building or structure required to be equipped with a fire department connection, the connection shall be located within one hundred (100) feet of a fire hydrant. *(Fire Prevention Ord. Sec. 15-17)* **Item Corrected 7/23/14**

**Recommendation: Approved**

Sincerely,

Joseph Shelton- Fire Marshal  
City of Novi – Fire Dept.

cc: file

**Novi Public Safety Administration**  
45125 W. Ten Mile Road  
Novi, Michigan 48375  
248.348.7100  
248.347.0590 fax

cityofnovi.org

**APPLICANT RESPONSE LETTER**

**VIA HAND DELIVERY**

August 5, 2014

Planning Commission  
City of Novi  
45175 W. Ten Mile Rd.  
Novi, Michigan 48375-3024



Re: Harman Becker Automotive Systems, Inc.  
Site Plan #JSP14-37

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Dear Commissioner:

As you most certainly have heard, the Harman Becker Automotive Systems, Inc. project has thus far been a great example of the private sector working with the public sector towards a common goal. We are very excited to be bringing Harman to our City and the Haggerty Corridor Corporate Park. It will be a great gateway to the City and our Park.

Many of the comments and concerns raised by both Engineering and Traffic related to the driveway connecting the site to 13 Mile Road. In lieu of building that driveway today, we will be constructing a sidewalk connecting the site to 13 Mile Road. As a condition of not constructing the road, we will be agreeing to build the driveway if and when there are any abnormal or unintended traffic backups at 13 Mile Road and Cabot. We believe there will be none, especially with the completion of the traffic signal (Note: The City and Northern Equities Group have partnered to bring the traffic light on-line in early October of this year. The costs to construct the light will be shared 50/50). This compromise, reached earlier today, will now remove from consideration many of the concerns raised, foremost: (a) we will no longer require a MDEQ or Novi wetland permit which would have delayed the project and put our delivery date in jeopardy, (b) we will not be building a wall over the sanitary sewer line. While this has been done before and we feel this can be worked out, this is a positive by-product of not building the driveway now. In the future, we will have time to work both the MDEQ and sanitary issues out together with the City. I believe this idea, worked out between myself and Rod Arroyo is a great benefit to the project and the City and will allow us to start construction in early September.

After reviewing the various review letters received from the City and its consultants, we have the following responses (please note the item numbers used correspond to the item numbers of each original comment):

**Planning Review**

- Landbanked parking will be re-labeled "Phase II";
- Loading space screening will be further detailed;
- The lab area overhead doors will be sufficiently screened.  
Please see attached for more detail about the types of plantings that will be used. The doors are for car labs which

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REAL ESTATE DEVELOPMENT,  
CONSTRUCTION AND INVESTMENT

provide research and development areas for Harman. They are the same labs located at LaSalle South and face the same direction (but with much less screening);

- The sidewalk issue was addressed by City Council when the Park was originally developed. A Sidewalk Waiver was granted for the sidewalks on 13 Mile Road, west of Cabot Drive, and along Haggerty Road, south of McKenzie Drive (safety and wetland impacts were the primary concerns). In lieu of installing the sidewalks, we agreed to provide a connection from the Nature Path to 13 Mile Road through this site, which we will do (see attached revised drawings) and fill gaps in the first phase of the Park, which is completed;
- Bike Parking details have been provided and more can be added;
- Lighting plan will be further detailed.
- Parcel Creation: A property split plan and application will be submitted to the City.

### **Facade Review**

Site lighting details will be provided. The dumpster enclosure will be matching brick. RTU screening will comply with ordinance.

### **Woodland Review**

1. The number of trees to be planted will be corrected.

### **Landscaping**

#### **General**

No comments need to be addressed

#### **Irrigation**

An irrigation plan and cost estimate will be submitted at the time of final site plan approval.

### **Fire Review**

No comments.

### **Engineering Review**

1. The taper will be added if we choose to build the driveway.
2. Comment is moot, see above response to Planning Letter regarding Sidewalk Waiver. Sidewalk/Trail will be taken to 13 Mile Road.
3. All stormwater plans will be provided at final site plan per discussions with Engineering Department.

4. City of Novi has allowed walls to be built over Sanitary Sewer for other projects. Currently, the drive is in the optimum location. We will execute an indemnity letter. If we do not construct the driveway to 13 Mile Road, this issue becomes moot as the walls will not be required.
5. While we believe an MDEQ wetland permit is not required, we have three alternatives to solve this issue. The only water being diverted to the wetland is due to the driveway. We have three alternatives, all of which will allow us to move forward towards a grading permit by September 2<sup>nd</sup>: (a) we can drain the driveway to a catch basin and pump the small amount of water to the required elevation, (b) we can apply for a minor use permit from the MDEQ, or (c) not build the driveway which would alleviate the need for any drainage into the wetland. The only water we are sending to the wetland is water that is currently draining there. All of the water will be pre-treated.

#### General, Water Main, Sanitary Sewer and Storm Sewer

6. Utility Table will be included.
7. Light pole locations will be shown.
8. Structures will be labeled.
9. Easement boundaries will be shown.
10. Soil borings are in process.
11. Detail sheets will be provided.
12. Taping sleeve, valve and well will be provided.
13. There are no other lots on 13 Mile.
14. Profile will be provided.
15. Stub will be added.
16. DEQ permit will be made as soon as possible.
17. Basis of design will be provided.
18. Note will be added.
19. Permit will be submitted.
20. MDEQ water main permit will be submitted as soon as possible
21. Details will be provided if we utilize the wetland as an outlet and if driveway is built.
22. Structures will be modified.
23. Noted.
24. Noted.
25. Noted.
26. Noted.
27. Noted.
28. Map area will be provided.
29. Bank volume will be provided.
30. Will be provided if required.
31. While no longer an issue today, we believe we meet the 9 criteria.
  - a. Wetland was highly altered by construction of M5 and 13 Mile Road. It now lies 30 feet below the road grade.
  - b. There are no unique features of the wetland.
  - c. The only water being sent is water that already goes there. The health will be maintained by our plan.
  - d. We will provide the water balance plan.
  - e. An easement can be provided for this small area.

- f. Pretreatment will be performed.
  - g. If any, we will spray for invasive species.
  - h. If any permit is required, it will be a minor use permit. This area is not on register of wetlands the state compiles, is not over 5 acres, and is not within 500 feet of the “drain”.
  - i. A wetland permit will be obtained if required.
32. Noted.
  33. Due to grades and wetland impacts we cannot widen the driveway.
  34. ADA details will be provided.
  35. Sheet will be revised.
  36. Sheet will be provided.
  37. A CLOMAR revising the flood plan has been awarded and the final LOMAR is in process. The flood plain will not encroach the site.

The remaining items are all related to final submittals after engineering approval of the construction plans and will be submitted to the City at the time each item is required.

### **Traffic Review**

Earlier today, I had a very productive conversation with Rod Arroyo and we came up with a solution which I believe addresses most of the concerns raised in his Response Letter.

We will provide the following in response to the left turn traffic issues:

1. We will modify the design of the driveway to be an entrance only drive, one lane, 18ft wide throat at 13, 15 foot wide drive, appropriately signed.
2. We will not be required to build today, thus alleviating the following issues: (a) no need for a DEQ permit, (b) no more issue re wall at sanitary sewer.
3. We will build and landscape the sidewalk going to 13 Mile as part of the initial development.
4. If there are issues at 13 Mile/Cabot in the future regarding abnormal back-ups in the left turn lanes, the City can require the developer to build the driveway as per (1) above. We will then have the time to get our deq permit or pump waiver, and Harman’s start of construction date will not be impacted.

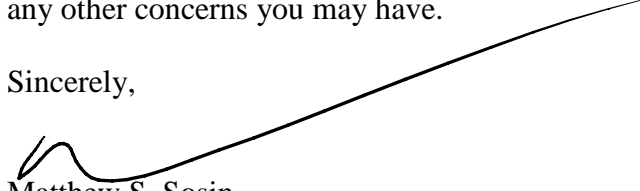
The following are responses to the letter:

5. Noted.
6. See solution above.
7. See solution above.
8. See solution above.
9. Noted.
10. See note above regarding Sidewalk Waiver.
11. Noted.

12. The requested dimensional information will be depicted on the plans submitted for Final Site Plan Submittal.
13. The truck maneuvering issue will be addressed on the plans submitted for Final Site Plan Submittal.
- 14a. The additional sign will be added to the plans submitted for Final Site Plan Submittal.
- 14b. The additional signs will be added to the plans submitted for Final Site Plan Submittal.
- 14c. The Fire Department has reviewed the Site Plan and has not requested any modifications to the No Parking Fire Lane Signs.
- 14d. The requested sign will be added to the plans submitted for Final Site Plan Submittal.
15. Noted.

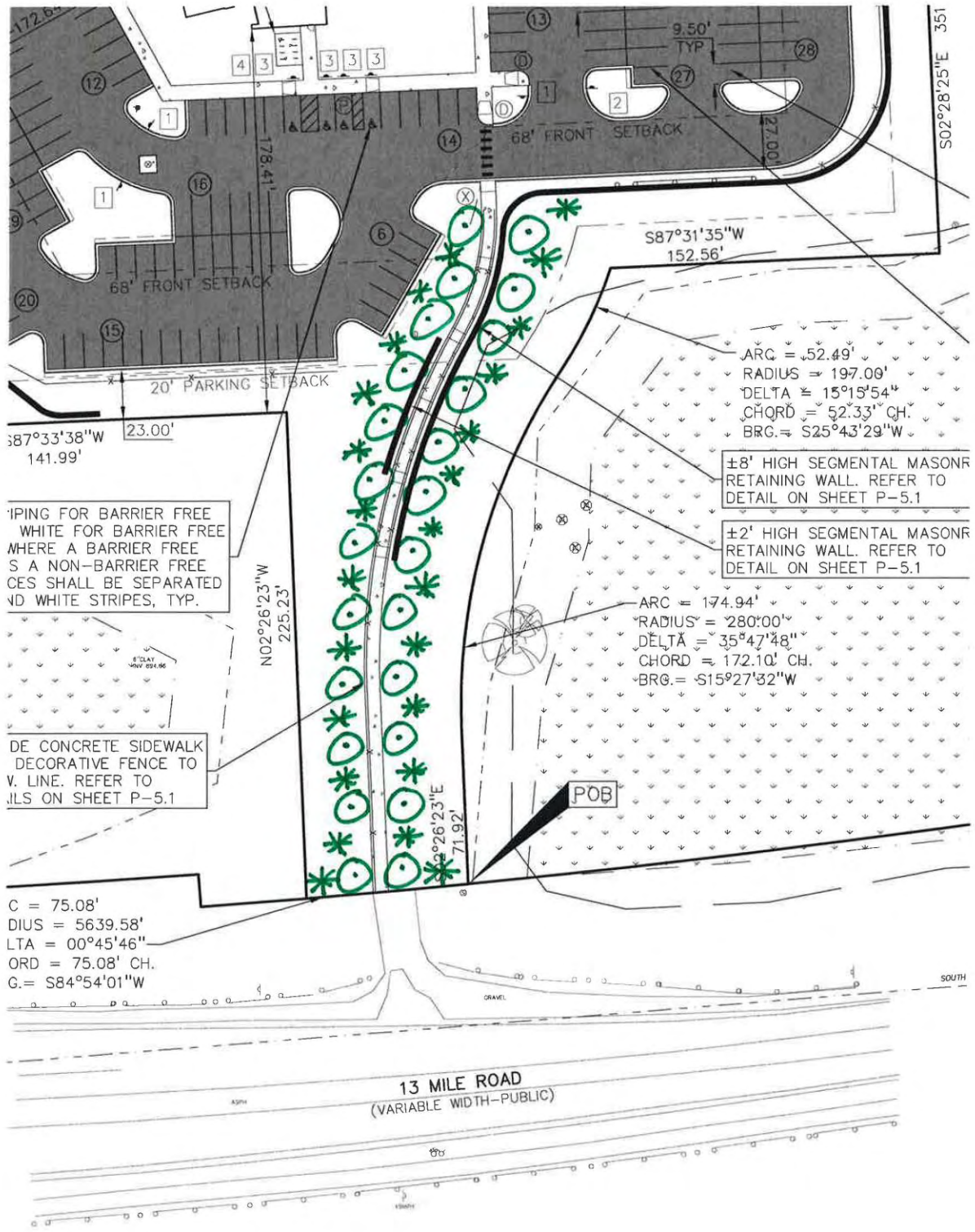
The remaining items pertain to permits and fees therefore do not require changes. We look forward to meeting with you to discuss these items in more detail and/or consider any other concerns you may have.

Sincerely,



Matthew S. Sosin  
President

# EXHIBIT A HARMAN-BECKER AUTOMOTIVE LANDSCAPE – KEY AND FACT SHEET





**TO BE PLACED ALONG WEST BERM AND M-5**

Key Symbol: CB3 (48 Total)  
Columnar European Hornbeam  
*"Carpinus betulus"*  
Height: 30' – 40'  
Width: 20' – 30'  
Perfect for showing  
symmetrical and architectural  
value, an excellent choice for  
linking lawns, streets,  
screening out sun.



Key Symbol: AR3 (14 total)  
Red Maple  
*"Acer rubrum"*  
Height: 59' – 115'  
Width: 39'  
Over most of its range, red  
maple is adaptable to a very  
wide range of site conditions,  
perhaps more so than any other  
tree in eastern North America.



Key Symbol: QR3 (47 Total)  
Red Oak  
*"Quercus rubra"*  
Height: 90'  
Width: 60'  
Straight and tall, has stout  
branches growing at right  
angles to stem, forming  
narrow, round-topped head.



**TO BE PLACED ALONG WEST BERM**

Key Symbol: PC3 (27 Total)  
Cleveland Select Pear  
“*Pyrus calleryana*”  
Height: 25’ – 35’  
Width: 15’  
Early spring bloom, dense  
flowers, very tolerable to heat  
and drought, needs little  
attention.



Key Symbol: AS3 (54 total)  
Sugar Maple  
“*Acer saccharum*”  
Height: 82’ – 148’  
Width: 30’ – 45’  
Known for its spectacular  
range of fall colors, is easy to  
propagate and transplant, as  
well as being fast-growing.



Key Symbol: PG8 (15 Total)  
Black Hills Spruce  
“*Picea glauca*”  
Height: 30’ – 60’  
Width: 15’ – 25’  
Large, very dense tree, fairly  
drought-resistant and excellent  
for wind breakage.





**TO BE PLACED ALONG M-5**

Key Symbol: CA3 (14 Total)  
Shagbark Hickory  
“*Carya ovata*”  
Height: 100’  
Width: 30’  
One of the hardiest of the hickory species, however, and has successfully adapted to a wide range of climatic conditions.



Key Symbol: AP3 (5 total)  
Bottlebrush Buckeye  
“*Aesculus parviflora*”  
Height: 6’ – 12’  
Width: 15’ – 20’  
Small, white flowers in 8-12” clusters in summer, autumn foliage yellow-green



Key Symbol: CC2.5 (32 Total)  
Eastern Redbud  
“*Cercis Canadensis*”  
Height: 15’ – 30’  
Width: 15’ – 25’  
One of the first trees to bloom in spring, rosy pink, reddish-purple, to dark green. Provides shade.

